



WOKINGHAM BOROUGH COUNCIL

A Meeting of the **PLANNING COMMITTEE** will be held in the Council Chamber - Civic Offices, Shute End, Wokingham RG40 1BN on **WEDNESDAY 9 MARCH 2022 AT 7.00 PM**

Susan Parsonage
Chief Executive
Published on 1 March 2022

Note: Although non-Committee Members and members of the public are entitled to attend the meeting in person, space is very limited due to the ongoing Coronavirus pandemic. You can however participate in this meeting virtually, in line with the Council's Constitution. If you wish to participate either in person or virtually via Microsoft Teams please contact Democratic Services. The meeting can also be watched live using the following link:

<https://youtu.be/uy47O66PYOM>

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WOKINGHAM BOROUGH COUNCIL

Our Vision

A great place to live, learn, work and grow and a great place to do business

Enriching Lives

- Champion outstanding education and enable our children and young people to achieve their full potential, regardless of their background.
- Support our residents to lead happy, healthy lives and provide access to good leisure facilities to complement an active lifestyle.
- Engage and involve our communities through arts and culture and create a sense of identity which people feel part of.
- Support growth in our local economy and help to build business.

Safe, Strong, Communities

- Protect and safeguard our children, young and vulnerable people.
- Offer quality care and support, at the right time, to prevent the need for long term care.
- Nurture communities and help them to thrive.
- Ensure our borough and communities remain safe for all.

A Clean and Green Borough

- Do all we can to become carbon neutral and sustainable for the future.
- Protect our borough, keep it clean and enhance our green areas.
- Reduce our waste, improve biodiversity and increase recycling.
- Connect our parks and open spaces with green cycleways.

Right Homes, Right Places

- Offer quality, affordable, sustainable homes fit for the future.
- Build our fair share of housing with the right infrastructure to support and enable our borough to grow.
- Protect our unique places and preserve our natural environment.
- Help with your housing needs and support people to live independently in their own homes.

Keeping the Borough Moving

- Maintain and improve our roads, footpaths and cycleways.
- Tackle traffic congestion, minimise delays and disruptions.
- Enable safe and sustainable travel around the borough with good transport infrastructure.
- Promote healthy alternative travel options and support our partners to offer affordable, accessible public transport with good network links.

Changing the Way We Work for You

- Be relentlessly customer focussed.
- Work with our partners to provide efficient, effective, joined up services which are focussed around you.
- Communicate better with you, owning issues, updating on progress and responding appropriately as well as promoting what is happening in our Borough.
- Drive innovative digital ways of working that will connect our communities, businesses and customers to our services in a way that suits their needs.

MEMBERSHIP OF THE PLANNING COMMITTEE

Councillors

Chris Bowring (Chairman)	Angus Ross (Vice-Chairman)	Sam Akhtar
Stephen Conway	Gary Cowan	Carl Doran
Pauline Jorgensen	Rebecca Margetts	Andrew Mickleburgh
Rachelle Shepherd-DuBey	Bill Soane	

ITEM NO.	WARD	SUBJECT	PAGE NO.
79.		APOLOGIES To receive any apologies for absence.	
80.		MINUTES OF PREVIOUS MEETING To confirm the Minutes of the Meeting held on 9 February 2022	5 - 12
81.		DECLARATION OF INTEREST To receive any declaration of interest	
82.		APPLICATIONS TO BE DEFERRED AND WITHDRAWN ITEMS To consider any recommendations to defer applications from the schedule and to note any applications that may have been withdrawn.	
83.	Wescott; Wokingham Without	APPLICATION NO.192325 - LAND SOUTH EAST OF FINCHAMPSTEAD ROAD, SOUTH WOKINGHAM SDL Recommendation: Conditional approval subject to legal agreement	13 - 142
84.	Winnersh	APPLICATION NO.214183 - LAND AT 1040 AND 1100 SERIES ESKDALE ROAD, WINNERSH TRIANGLE BUSINESS PARK, RG41 5TS Recommendation: Conditional approval subject to legal agreement	143 - 174
85.	Remenham, Wargrave and Ruscombe	APPLICATION NO.214108 - HARE HATCH SHEEPLANDS, LONDON ROAD, TWYFORD, RG10 9HW Recommendation: Refusal	175 - 240
86.	Hawkedon	APPLICATION NO.214046 - AUTO TRADER HOUSE AND HARTMAN HOUSE, DANEHILL, LOWER EARLEY, RG6 4UT Recommendation: Conditional approval subject to legal agreement	241 - 278

87. Evendons

**APPLICATION NO.213975 - INDIGO HOUSE,
MULBERRY BUSINESS PARK, WOKINGHAM, RG41
2GY**

279 - 304

Recommendation: Conditional approval subject to legal agreement

Any other items which the Chairman decides are urgent

A Supplementary Agenda will be issued by the Chief Executive if there are any other items to consider under this heading.

GLOSSARY OF TERMS

The following abbreviations were used in the above Index and in reports.

C/A	Conditional Approval (grant planning permission)
CIL	Community Infrastructure Levy
R	Refuse (planning permission)
LB	(application for) Listed Building Consent
S106	Section 106 legal agreement between Council and applicant in accordance with the Town and Country Planning Act 1990
F	(application for) Full Planning Permission
MU	Members' Update circulated at the meeting
RM	Reserved Matters not approved when Outline Permission previously granted
VAR	Variation of a condition/conditions attached to a previous approval
PS	Performance Statistic Code for the Planning Application
Category	

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**MINUTES OF A MEETING OF THE
PLANNING COMMITTEE
HELD ON 9 FEBRUARY 2022 FROM 7.00 PM TO 8.30 PM**

Committee Members Present

Councillors: Chris Bowring (Chairman), Angus Ross (Vice-Chairman), Sam Akhtar, Stephen Conway, Gary Cowan, Pauline Jorgensen, Rebecca Margetts, Rachelle Shepherd-DuBey and Bill Soane

Councillors Present and Speaking

Councillors: Stuart Munro

Officers Present

Callum Wernham, Democratic & Electoral Services Specialist
Chris Easton, Head of Transport, Drainage, and Compliance
Marcia Head, Head of Development Management
Mary Severin, Borough Solicitor

Case Officers Present

Senjuti Manna
Kieran Neumann

71. APOLOGIES

Apologies for absence were submitted from Councillors Carl Doran and Andrew Mickleburgh.

72. MINUTES OF PREVIOUS MEETING

The Minutes of the meeting of the Committee held on 12 January 2022 were confirmed as a correct record and signed by the Chairman.

73. DECLARATION OF INTEREST

Rebecca Margetts made a statement with regards to items 76 and 77, application numbers 213903 and 213927. Rebecca stated that she was a Parish Councillor at Finchampstead Parish Council, who was the applicant for the two applications regarding the War memorial. Rebecca added that she had not been involved with the applications or the committee set up for these applications at the Parish Council and she did sit on the Planning Committee for the Parish Council. Rebecca stated that she came to the Planning Committee with an open mind, and she would listen to all representations from public speakers, planning officers and members of the Committee prior to coming to any conclusion on either of these applications

74. APPLICATIONS TO BE DEFERRED AND WITHDRAWN ITEMS

Item number 75, application number 213796, was withdrawn from the agenda.

75. APPLICATION NO.213796 - 302 LONDON ROAD, WOKINGHAM, RG40 1RD

This item was withdrawn from the agenda.

76. APPLICATION NO.213903 - JUNCTION OF JUBILEE ROAD / B3016

Proposal: Application for Listed Building Consent for the proposed dismantling of war memorial and relocation and reinstallation on new site.

Applicant: Mrs Katy Dagnall (Finchampstead Parish Council)

The Committee considered a report about this application, set out in agenda pages 37 to 52.

The Committee were advised that whilst there were no updates contained within the supplementary planning agenda, additional comments and officer responses had been received in relation to this application and had been circulated to the Committee. An additional representation had also been received from the War Memorial Trust, and had been circulated to the Committee.

Ian Adnams, local resident, spoke in objection to the application. Ian maintained that the reasons stated by the Parish Council for relocation of the memorial were not relevant. Ian stated that the Parish Council believed that no significant improvements could be made to the current junction whilst keeping the memorial in its current location, despite a suggestion from himself which would have made the junction safer whilst keeping the memorial in its existing location. Ian added that the Parish Council claimed that the current location was unsafe for the Armistice Day parade, however the Royal British Legion Remembrance Parade Policy Update advised that a parade was not the remembrance event, and it was not uncommon to have an event which did not involve a parade on a highway. Ian stated that St James' Church was fully accessible from the memorial hall car park via a direct footpath with no road to close or negotiate, presenting a viable and safe option. Ian commented that a survey from the Parish Council claimed 89 percent of respondents supported plans to relocate the war memorial, however it had been pointed out by others that in fact 90.1 percent of respondents were in objection to the proposals. Ian stated that the memorial was designated both as a war memorial and as a wayside cross, and to carry out its function as a wayside cross the monument was required to remain where it was currently situated. Ian commented that he had lived and worked in Finchampstead all of his life, and the monument was a historic icon of the village of which its location had been chosen by the village's forefathers. Ian asked that the monument be left in its current location, where he felt it belonged.

Graham Jukes, on behalf of the applicant, spoke in support of the application. Graham stated that the key issue relating to the application was the consideration of harm, and noted that the grade two listing applied only to the memorial itself and not to its setting. Graham stated that in the war memorial's current location the Parish Council's legal obligation to maintain the memorial could not be fulfilled, as the site was subjected to 11,000 vehicle movements each day. Graham stated that the site was too hazardous for contractors to operate without road closures, and after careful consideration of a range of options the Parish Council concluded that the only sustainable way for the Parish Council to continue to fulfil its legal obligations was to move the monument to a setting where it fulfilled its function as a place of respect and remembrance whilst allowing for careful maintenance for years to come. Graham stated that the original location had seen vast changes to traffic since the monument was placed there after the First World War, and careful checks had revealed no original debate over where the monument was to be situated. Graham added that the site was currently extremely dangerous for people to visit, was surrounded by heavy duty high curbed stones, and the Parish Council could no longer recruit contractors to carry out works on the monument due to health and safety concerns. Graham stated that following a consultation sent to all households, parishioners supported the relocation of the monument to a safer and more accessible location. Graham stated that the aim of the Parish Council was to maintain the memorial and re-establish its purpose as a purpose for accessible remembrance and quiet contemplation which was not possible at the current location. Graham accepted that there were some residents who

wished for the monument to remain at its current location for personal reasons, however he felt this was a minority of people, and asked the Committee to approve the application.

Rebecca Margetts stated that the proposal to relocate the war memorial was an emotive subject, and it was very important that any relocation decision was correct and allowed current and future residents to pay their respects. Rebecca added that the War Memorial Trust had recommended that the war memorial be kept at its current location with bollards and barriers installed to protect the memorial. Rebecca commented that she had concerns with the suggestion of bollards or barriers, and noted that the War Memorial Trust only recommended a war memorial to move where the current position would put the memorial at risk or where the location was inaccessible. Rebecca felt that both the inaccessibility and potential risk to the memorial were relevant for this war memorial, and added that any traffic calming could change the rural setting. Rebecca stated that there was only a pavement on one side of the road, and it was a great shame that the fantastic armistice service could not visit the war memorial in its current location and instead had to attend a poster of the memorial in a field. Rebecca felt that moving the memorial would future proof it, allowing future generations to pay their respects. Rebecca queried whether barriers were a possibility. Kieran Neumann, case officer, stated that barriers could possibly be achieved however they were not a part of this application.

Angus Ross commented that since he moved to the area in 1980 the traffic had significantly increased in this location. Angus added that he had never personally seen anyone visit the memorial due to its dangerous location, and felt that there was public benefit in moving the monument to a safer location to allow residents to visit and pay their respects.

Pauline Jorgensen stated that she was genuinely torn on this application, in part as the memorial's setting was formed from the overall setting and the view over the water. Pauline felt that this setting would have been selected by people directly affected by World War One, and she was very reluctant to see it moved to a more convenient location.

Chris Bowring queried what evidence was there with regards to how the current location was originally chosen. Kieran Neumann stated that there was not much specific evidence, and the only comments that could be found were in relation to the picturesque location and setting. Kieran added that on balance, the future proofing of the war memorial overrode the current setting of the war memorial.

Sam Akhtar sought clarity with regards to comments that there were two opposing consultation documents, with one showing overwhelming support for the relocation and another showing overwhelming objections. Kieran Neumann stated that the results of the consultation conducted by the Parish Council had no bearing on the scheme. Kieran added that this application was to consider the impact of moving the listed building itself.

Gary Cowan commented that the memorial cross was moved in Arborfield due to works on the roundabout, and very similar points were raised by the War Memorial Trust. Gary added that the project was carried out successfully, and the new location along the side of the roundabout allowed for benches to be installed which now attracted residents to be able to sit down and reflect. In relation to this application, Gary was of the opinion that whilst the current location was appealing it was not protected from the 11,000 daily vehicle movements, whilst any bollards or traffic lights would urbanise the area. Gary added that should the application be approved, the key was to reduce the risk to the fabric of the war memorial, and should it be left at its current location it was at risk of damage from road

vehicles. Should the war memorial be moved, Gary commented that he would like to see a condition protecting the monument at its future location from damage by trees.

Rachelle Shepherd-DuBey commented that as one of the three veterans on the Committee she felt that the proposal was the best option as it would allow current and future residents to pay their respects in safety.

Stephen Conway commented that the conditions that pertained in 1920 no longer existed, and added that the Parish Council now found it difficult to procure a maintenance team to take care of the memorial. Stephen commented that the current location was far from ideal for residents to visit and pay their respects.

Bill Soane stated that he was part of a group of people who raised funds to install a new war memorial in Woodley five years ago, which was sited within the Woodley memorial grounds. Bill added that this memorial now attracted between 200 and 250 people on Remembrance Day who could not attend before, whilst people often sat on the benches nearby to quietly reflect.

Marcia Head, Head of Development Management, stated that this application was to consider the planning merits of what harm would be caused to the building should it be relocated. Marcia stated that any considerations relating to protecting the monument from trees and roots, should it be relocated, would be considered under application number 213927.

RESOLVED That application number 213903 be approved, subject to conditions and informative as set out in agenda pages 38 to 39.

77. APPLICATION NO.213927 - FINCHAMPSTEAD MEMORIAL PARK, THE VILLAGE, RG40 4JU

Proposal: Full application for the proposed relocation and erection of war memorial, plinth and steps, creation of a footpath, installation of culvert and power supply.

Applicant: Mrs Katy Dagnall (Finchampstead Parish Council).

The Committee considered a report about this application, set out in agenda pages 53 to 84.

The Committee were advised that the supplementary planning agenda included an update to paragraph 17 of the officer report. In addition to the supplementary planning agenda, the Committee were advised that additional comments and officer responses had been received in relation to this application and had been circulated to the Committee. An additional representation had also been received from the War Memorial Trust, and had been circulated to the Committee.

Ian Adnams, local resident, spoke in objection to the application. Ian stated that he was the vice-chairman of the Finchampstead Park Management Committee, and stated that the issue of removal of the hedge was still ongoing at meeting discussions. Ian was of the opinion that the Parish Council had made a decision despite opposition from residents, and added that he had spoken to many residents who did not like the suggested location for the relocation of the war memorial. Ian stated that the proposal would take the monument from one busy road and place it next to another busy road, which would offer no more opportunity for quiet contemplation than at its current location. Ian commented

that people in the village would prefer for the war memorial to be relocated at the church if it had to be moved, where there was adequate parking and amenities and available. Ian stated that the War Memorial Trust asked that anyone moving a war memorial should use a conservation accredited architect whereas the Parish Council had hired a landscape architect. Ian felt that the Parish Council should now go back to the village and seek their views as to their preferred location for the war memorial.

Graham Jukes, on behalf of the applicant, spoke in support of the application. Graham stated that the hedge within the memorial park was not material to this application, however it would not be removed prior to full discussion and debate with the Finchampstead Park Management Committee. Graham commented that the consultation survey was circulated to 5,500 households, with 596 responses in total, and 89 percent agreed with the proposal to remove the memorial from its current site and relocate it to the verge adjacent to the memorial park of the village. Graham added that the memorial park was purchased in memoriam to those who fell in both World Wars, and felt that it was right and fitting that the memorial was located and maintained at this site. Graham stated that each year residents gathered at the park in front of a photograph of the memorial statue. Graham added that detailed plans were considered for four separate locations including the church, and following discussions the proposed location was deemed the most suitable given the guidance from the War Memorial Trust relating to the visibility of war memorials. Graham stated that an arboreal expert had been involved to both protect the monument and the plants and trees at the proposed location. Graham added that the proposed location would allow visibility of the memorial from those passing on the road, a safe space for those who wish to visit the memorial, and a safe venue for the remembrance service to take place each year. Graham asked that the application be approved.

Chris Bowring sought clarification with regards to the type and qualifications of the architect which would be carrying out this work, and was required by condition. Marcia Head, Head of Development Management, stated that the contract would ensure the landowner in the proposed location agreed to the erection of the structure on their land, which was a highway verge, prior to removal from the current location. Marcia added that condition attached to the agreed listed building consent included a requirement of provision of methodology detailing how the structure was to be transported, including a detailed schedule.

Rebecca Margetts queried whether a condition managing overhanging trees and tree roots from damaging the war memorial at the proposed location was included within the scheme. Marcia Head suggested that an additional informative could be added to in relation to this. Rebecca Margetts proposed an additional informative requesting that details be submitted including the provision for access to maintain the memorial in the new location including the ability to carry out future tree works should they be required. This was seconded by Gary Cowan, carried, and added to the list of informatives.

Stephen Conway commented that he appreciated that this was a delicate issue, however added that the Planning Committee needed to assess in planning terms whether the proposed location was acceptable. Stephen added that the parking located nearby would allow residents to visit the war memorial and pay their respects safely.

Chris Bowring sought clarification as to how policy TB24 might apply to this application. Kieran Neumann, case officer, confirmed that this policy applied to this application, as if the structure fell into disrepair Wokingham Borough Council would be responsible for its maintenance. Marcia Head stated that one of the conditions attached to the Listed Building

consent required a very detailed scheme showing how the structure would be transported and cared for prior to its removal.

Rachelle Shepherd-DuBey proposed an additional informative, suggesting that the applicant provide a bench to allow people to rest and contemplate near the proposed site of the war memorial. This was seconded, carried, and added to the list of informatives.

Pauline Jorgensen queried what would happen to the previous application relating to listed building consent should this application be refused. Marcia Head stated that the listed building consent required a contract showing that the war memorial could be re-erected. Marcia added that the applicant could submit a further application detailing an alternative location.

Angus Ross queried whether the addition of the informative relating to a bench was reasonable as the bench would be located outside of the red line area. Marcia Head confirmed that both the Parish and Borough Councils had permitted development rights to place benches.

Angus Ross commented that although the proposed location would be located next to a busy road, it would not be as busy as the roads it was currently positioned next to. Angus felt that the condition relating to protection of trees and protection and maintenance of the war memorial were sufficient.

Gary Cowan commented that people sat on the benches next to the Arborfield memorial cross and reflected despite this being located next to a five exit roundabout. Gary added that a qualified firm had to be procured and used to carry out the works in Arborfield, and a similar qualified firm would also be required to carry out the proposed works.

Ian Adnams commented that the local car park was a privately owned car park.

Pauline Jorgensen was of the opinion that the war memorial in Woodley was fundamentally different, as it was a brand new memorial and did not involve moving an existing war memorial.

RESOLVED That application number 213927 be approved, subject to conditions and informative as set out in agenda pages 54 to 55, and additional informatives relating to maintenance and protection of the memorial in relation to trees and the suggested provision of a bench to allow for quiet reflection.

78. APPLICATION NO.213380 - BALCOMBE NURSERIES, BASINGSTOKE ROAD, SWALLOWFIELD, RG7 1PY

Proposal: Full application for the erection of 5 no. detached dwellings, two with detached garages and three with internal garages and associated landscaping works including one balancing pond, 2 no. accesses with entrance gates and 1.2m post and rail fencing.

Applicant: Woodridge Developments, 11 Arkwright Road Reading RG2 0LU

The Committee considered a report about this application, set out in agenda pages 85 to 120.

The Committee were advised that there were no updates contained within the supplementary planning agenda.

John Anderson, Swallowfield Parish Council, spoke in objection to the application. John stated that three-storey homes were not part of the character of the Parish, and the proposals would occupy an elevated position whilst taking centre stage within the wide site entrance. John added that the proposals would give the appearance of tall town houses within a field. John felt that the proposals were inappropriate within a countryside setting. John stated outline permission was granted in 2016 for three houses, which the Parish Council had supported on balance as an alternative to a busy garden centre. John stated that this permission had removed permitted development rights, however this had been subsequently ignored and the proposals were now for 5 tall dwellings with a separate driveway leading to plot one. John was of the opinion that this was a further attempt to increase the size and scope of the development, which would further harm the character of the area and was a step too far.

Lisa Burns-Peake, resident, spoke in objection to the application. Lisa stated that loss of privacy to her home as a result of the proposals would have a huge impact on their quality of life. Lisa was of the opinion that the proposed dwellings were utterly out of keeping with the housing stock on the road, and added that there had been subsequent increases in the proposed size of the development of the site since 2016.

Tristan Parsons, neighbour and on behalf of the applicant, spoke in support of the application. Tristan stated that he lived in the property two doors down from the site, and five years ago there had been an issue with travellers accessing the site and entering his back garden. As a result, the site was purchased with a view to develop. Tristan stated that he was now working from home alongside his wife whilst their children took part in home schooling when required due to Covid-19. Tristan stated that his family would be moving into plot one, and the proposed attic rooms would be added to the other four properties to allow those families space for home working. Tristan stated that many different designs had been considered, and the proposed designs were deemed to have the least impact on the countryside whilst providing space for the attic rooms without losing the attractive curve appeal of the properties, without increasing either the height of the dwellings or the footprint of the properties. Tristan felt the addition of the attics would allow for future families to work from home when required, which could also result in less vehicle movements to and from the properties. Tristan added that the additional floor space would attract an approximate £100,000 in additional CIL contributions to the Council.

Stuart Munro, Ward Member, spoke in objection of the application. Stuart stated that he understood the need for home working space, however the proposed dwellings would be very large via their existing and approved plans. Stuart added that the site had a long and complex planning history, and in his opinion it had reached the point where the proposals were no longer in keeping with the character of the area.

Stephen Conway stated that he fully understood the concerns of the Parish Council and local residents, however the principle of development had been established through previous planning permissions. Stephen commented that it was regrettable that there would be no housing mix on the site, other than four large houses and one very large house. Stephen sought additional details with regards to any potential issues relating to overlooking. Senjuti Manna, case officer, stated that the distance between the old cottage and plot one was approximately 80 metres, which was significantly more than was recommended within the Borough Design Guide, whilst trees would also provide additional screening. Senjuti added that it was accepted that this was a rural setting, however the proposals allowed for more than double the recommended separation distance.

Lisa Burns-Peake commented that plot one would be located directly behind her home and would be an invasion of their privacy with direct views into their garden and bedroom.

Angus Ross stated that the site had a long and complex planning history. Angus stated that the question for the Committee was whether it was reasonable to refuse this application when there was no proposed increase in ridge height or footprint, and sought officer comment on the viability of such a refusal should it go to appeal. Marcia Head, Head of Development Management, stated that the proposed changes were the inclusion of dormers and roof light windows, with no increase to the ridge height or footprint of any of the properties. Marcia confirmed that the separation distances were double that of the recommended distances.

Gary Cowan felt that the changes to the existing planning permission were minor, and Wokingham Borough Council would likely not fare well at an appeal. Gary commented that the site was 2.32 hectares, and in another location up to 70 houses could be placed on such a site.

RESOLVED That application number 213380 be approved, subject to conditions and informatives as set out in agenda pages 86 to 91.

Agenda Item 83.

Application Number	Expiry Date	Parish	Ward
192325	30/04/2022	Wokingham, Wokingham Without	Wescott, Wokingham Without

Applicant	Charles Church Developments Ltd
Site Address	Land South East of Finchampstead Road, South Wokingham Strategic Development Location Wokingham (SDL)
Proposal	Hybrid Planning application (part outline/part full) comprising outline application with all matters reserved for up to 171 no. dwellings, public open space and associated infrastructure and full application for Suitable Alternative Natural Greenspace (SANG).
Type	Hybrid
Officer	Emy Circuit
Reason for determination by committee	Major (EIA) application within the South Wokingham SDL

FOR CONSIDERATION BY	Planning Committee on Wednesday, 9 March 2022
REPORT PREPARED BY	Assistant Director Delivery and Infrastructure: Place and Growth

SUMMARY
<p>The application relates to an 18.75 hectare site to the south of Wokingham; it is allocated in the Development Plan as part of the South Wokingham Strategic Development Location (SDL).</p> <p>Wokingham Borough Core Strategy establishes the need to deliver over 13,000 new homes in borough in the period up to 2026, the majority in four Strategic Development Locations (SDLs) (Core Strategy policy CP17 <i>Housing Delivery</i>). Of these 2,500 are to be in an urban extension to the south of Wokingham. Core Strategy Policy CP21 <i>South Wokingham Strategic Development Location</i>, amplified by Appendix 7 and two Supplementary Planning Documents (SPDs) – the <i>South Wokingham South Wokingham Strategic Development Location SPD</i> and the <i>Infrastructure Delivery and Contributions SPD</i> – set out the Council’s expectations in terms of the comprehensive delivery of these dwellings together with the infrastructure required to support them.</p> <p>A fundamental element of the infrastructure required to support development is the SDL is the South Wokingham Distributor Road (SWDR); a continuous new route running through the SDL connecting the A329 London Road in the north to the A321 Finchampstead Road in the south. This new street will provide access to the new development and form a corridor for sustainable travel, as well as providing some traffic relief in the historic town centre of Wokingham.</p> <p>The first phase of development within the SDL – 636 dwellings and associated infrastructure on land between the A329 London Road and Reading-Waterloo railway line at Montague Park (formerly Buckhurst Farm) - already has planning permission and is substantially complete. Montague Park represents roughly a quarter of the development within the SDL and delivered its proportionate share of the necessary</p>

infrastructure including the first section of the SWDR from the A329 London Road to the railway line, William Heelas Way.

Planning permission for the second section of the SWDR – commonly referred to as the “Eastern Gateway” – was granted in February 2018 and it opened in January 2022, connecting William Heelas Way to Waterloo Road via a new bridge over the Reading-Waterloo railway line. Planning permission for the remainder of the SWDR – from the Eastern Gateway to Finchampstead Road – was granted in May 2021 along with associated highway works on Finchampstead Road, often referred to as the “Western Gateway”.

A suite of three applications for development in the SDL south of the railway (referred to as phase 2) was reported to the planning committee on 18 May 2021, when the committee resolved to grant outline planning permission for up to 1,649 new homes (approximately 65% of the South Wokingham SDL development) subject to completion of S106 legal agreements to secure the necessary infrastructure and full planning permission for two Suitable Alternative Natural Greenspaces (SANG): a new SANG at Holme Park and St Anne’s SANG, an extension to the existing Buckhurst Meadows SANG. Other infrastructure (some of which is also intended to support the current proposal and vice versa, as part of the comprehensive planning of the SDL) includes land for a two-form entry primary school, a local centre including land for a community facility and other public open space required to support the development on-site.

The application is a hybrid, seeking full planning permission for the SANG and outline planning permission for the remainder with access, appearance, landscaping, layout and scale all reserved, although the application does establish parameters for these matters.

These proposals represent the final substantive phase of the planned development within the South Wokingham SDL and – in line with the expectations of the Development Plan – are consistent with the masterplanning of the applications already considered. While the application must be assessed on its own merit, comprehensive planning of the SDL, in particular supporting infrastructure, results in interdependences between the current application and those within phase 2. The application is accompanied by a comprehensive Infrastructure Delivery Plan (IDP) - prepared jointly with the applicants for phase 2 and the SWDR – which sets out how each application will contribute its proportionate share of the infrastructure required to support the SDL development. For ease of reference the current application 192325 is referred to as “phase 3” in this report, although that does not necessarily reflect the phasing of development delivery.

The proposal relates to delivery of a key aim of the development plan, so is acceptable in principle and the level of information provided is sufficient to demonstrate that the proposals are consistent with the council’s planning policy and guidance. A detailed assessment is set out in the appraisal.

The proposal has been considered under Schedule 2 of the Town and Country Planning Act (Environmental Impact Assessment) Regulations 2017: it constitutes an urban development on a site of more than 0.5 hectares and is deemed likely to have a significant effect on the environment. Accordingly, the application is accompanied by an Environmental Statement (ES).

The application is before the planning committee because it constitutes a major development proposal within an SDL and is recommended for approval.

PLANNING STATUS

- Strategic Development Location (SDL CP17/CP21)
- Major development location (CP9/CC01) ¹
- Countryside (CP11)
- Allocated SANG (SAL05) ²
- Thames Basin Heath Special Protection Area Linear Mitigation Zones: 5km Linear Mitigation Zone (CP8/SAL05)
- Sites of Special Scientific Interest Impact Risk Zones
- Tree Preservation Order 1435/2012
- Finchampstead Road is a Green Route (CC03)
- Public Rights of Way within the site Wokingham Without FP9/Wokingham FP24, Wokingham/Wokingham Without FP25, and Wokingham Without FP10
- Main River³
- Ordinary Watercourse⁴
- Flood Zones 1, 2 & 3⁵
- Grade 3a and 3b agricultural land
- Potentially contaminated land consultation zone⁶
- Landfill gas consultation zone⁷
- SSE overhead electricity cable consultation zone & substation consultation zone⁸
- Setting of listed buildings
- Emm Brook Surface Water Nitrate Vulnerable Zone
- Mineral consultation zone
- CC03 proposed Emm Brook path
- Local Plan submitted sites 5WK036 (Land at the rear of Chapel Green House (5WK036), 5WK006 (South of Gipsy Lane, housing, north of application site

RECOMMENDATION

1. **That the committee authorise the GRANT OF PLANNING PERMISSION subject to the following:**
 - A. Completion of a S106 legal agreement to secure the infrastructure set out in section 0 of this report; and**

¹ Based on the South Wokingham SPD

² East and West of Lucas Hospital, Chapel Green

³ Emmbrook west of Wokingham Without Footpath 10

⁴ Emmbrook tributaries: Luckley Brook, a tertiary river on the western boundary of the site and a secondary river, east of Wokingham Without Footpath 10

⁵ Flood Zones 2 & 3 along the Emm brook and ordinary watercourses

⁶ North-western and north-eastern corners of the site.

⁷ North-western corner of the site.

⁸ Substations adjacent to Gipsy Lane Footbridge, adjacent to the railway line at Tesco & at the junction of Luckley Road/the Ludgrove access/Wokingham Without BW25

B. Conditions and informatives as below (subject to any additions and updates agreed with the Assistant Director Delivery and Infrastructure, Place and Growth between the date of the resolution and issue of the decision):

2. **Should the S106 legal agreement not be completed within 6 months of the date of this resolution planning permission be refused due to failure to secure the necessary infrastructure impact mitigation, unless otherwise agreed by the chairman of the planning committee and confirmed in writing by the Local Planning Authority.**

Timescale for development

1. i) The development hereby approved in full, shall be begun before the expiration of three years from the date of this permission.
- ii) a) No phase of development approved in outline shall commence until details of the appearance, landscaping, layout and scale of development (hereinafter called "the reserved matters") for that phase have been submitted to and approved in writing by the local planning authority and the development shall be carried out as approved.
- b) Application(s) for the approval of all reserved matters for the first phase of development shall be made within three years from the date of this permission and all remaining reserved matters applications shall be made within a period of five years from the date of this permission.
- c) The first phase of the development hereby permitted shall begin no later than five years from the date of this permission or two years from the approval of the final reserved matters for the phase, whichever is the later.
- d) Development within each phase of the development hereby permitted shall begin no later than two years from the date of the approval of the last reserved matter for that phase or before the expiration of five years from the date of this permission, whichever is the later.

Reason: In pursuance of s91 & s92 of the Town and Country Planning Act 1990 (as amended by s.51 of the Planning and Compulsory Purchase Act 2004).

Approved drawings

2. This permission is in respect of the submitted application plans and drawings:

P18-2684_02F Site Location Plan

P18-2684_06W Provisional Layout

P18-2684_32E Illustrative Landscape Masterplan

P18-2684_40E Green Infrastructure Provision

P18-2684_41E Parameters Land Use

P18-2684_42F Parameters Building Heights

P18-2684_43L Parameters Movement & Access

P18-2684_44E Parameters POS Typologies
P18-2684_45A SANG Detailed Soft Landscape Proposals (sheet 1 of 3)
P18-2684_46A SANG Detailed Soft Landscape Proposals (sheet 2 of 3)
P18-2684_47A SANG Detailed Soft Landscape Proposals (sheet 3 of 3)
P18-2684_48A SANG Detailed Hard Landscape Proposals (sheet 1 of 3)
P18-2684_49A SANG Detailed Hard Landscape Proposals (sheet 2 of 3)
P18-2684_50A SANG Detailed Hard Landscape Proposals (sheet 3 of 3)
P18-2684_51 Natural Play Area Detailed Proposals
PRI22405-03A Tree Protection Plan Sheet 1 of 2
PRI22405-03A Tree Protection Plan Sheet 2 of 5

The development shall be carried out in accordance with the approved plans unless a non-material amendment is approved by the Local Planning Authority under the Town and Country Planning (Development Management Procedure) (England) Order 2015.

Reason: for the avoidance of doubt and to ensure that the development is carried out in accordance with the application form and associated details hereby approved.

Phasing

3. Before or concurrent with submission of reserved matters details of the phasing of development, including the coordinated delivery of the on and off-site infrastructure required to support it, shall be submitted to and approved in writing by the local planning authority and development shall be implemented in accordance with the approved details. The details shall define the development to be delivered in each phase including:
 - i) the number, size, type and tenure mix of affordable dwellings, demonstrating overall delivery of 35% affordable homes, appropriately distributed across the development;
 - ii) the number and size of housing to be built to M4(2) standard (accessible and adaptable dwellings) or equivalent which should amount to at least 5% of the affordable housing, consisting of one and two-bedroom flats a clusters of three units and at least 5% of the market housing;
 - iii) the size and type of market dwellings in general accordance with the Wokingham Borough Local Housing Needs Assessment 2019 Report of Findings (January 2020) or any policy and guidance that supersedes it;
 - iv) on-site public open space;
 - v) phasing of delivery of the movement network to facilitate sustainable access for residents to key on and off-site destinations, including SANG, play areas schools and bus stops within the SDL;
 - vi) delivery of the emergency access before occupation of more than 100 dwellings within parcel D as identified on the Illustrative layout Drawing No P18-2684_06W;

- vii) delivery of the pedestrian and cycle access before occupation of more than 30 dwellings within parcel D as identified on the Illustrative layout Drawing No P18-2684_06W;
- viii) timing and delivery of the flood compensation works associated with the SWDR;
- ix) timing of the delivery the drainage and SuDS in relation to the development it is to serve;
- x) public art pursuant to condition 7;
- xi) early delivery of landscaping required to mitigate the impact upon the listed buildings at Lucas Hospital;
- xii) on and off-site highway works necessary to mitigate the impact of the development pursuant to conditions **Error! Reference source not found.**, 45 & 46;
- xiii) a car club pursuant to condition 42;
- xiv) implementation of the interim public transport strategy pursuant to condition 41 including details of the bus route.

Reason: to ensure comprehensive planning and timely delivery infrastructure required to support the development in accordance with Wokingham Borough Core Strategy Policies CP1, CP2, CP3, CP4, CP5, CP6, CP17, P21 and Appendix 7; and the South Wokingham SDL SPD. Details are required prior to commencement to ensure coordinated and timely delivery.

Detailed, Site-Wide Masterplan, Design Code and Landscape Design Statement

4. Before submission of the first reserved matters a Site-Wide Masterplan, Design Code and Landscape Design Statement – which build on the principles established by the *Design and Access Statement* (P18-2684_24H, February 2022), *Green Infrastructure Strategy* (P18-2684_16E, February 2022) and Drawing Nos P18-2684_06W *Provisional Layout* and P18-2684_32E *Illustrative Landscape Masterplan* - shall be submitted to an approved in writing by the local planning authority and subsequent applications pursuant to reserved matters and conditions shall be in accordance with the approved details. The details shall include:
- i) how the character analysis of urban form within the Design and Access Statement has informed the design principles for the development;
 - ii) amplification of the principles for development in each of the character areas and street typologies demonstrating a comprehensive approach that will deliver a cohesive development with distinct character areas within it to include sections through streets and cover, among other things, the contribution that density, building setbacks and front garden depths, boundary treatment and enclosure, and materials will make to distinguishing the different character areas;
 - iii) landscape principles for each character area and street typology including a clear hierarchy of connected spaces informed by the surrounding landscape character and incorporating mitigation measures identified in the LVIA and street trees;

- iv) sufficient space around important, retained trees to allow their successful retention;
- v) an interconnected movement network delivering a hierarchy of streets and paths to prioritise movement by pedestrians and cyclists including integration of Public Rights of Way and Greenways within residential parcels;
- vi) principles for how parking to the council's standards will be delivered within each character area including integration of unallocated parking in the public realm;

Reason: To ensure delivery of a comprehensively planned, high quality development in accordance with Core Strategy policies CP1, CP3, CP7, CP21; Managing Development Delivery Local Plan policies TB21 & TB23; the Borough Design Guide SPD, the South Wokingham SDL SPD and NPPF paragraph 131. Details are required prior to submission of reserved matters because they are intended to ensure comprehensive and coordinated masterplanning and delivery.

Crime Prevention Design Advisor

- 5. Reserved matters shall demonstrate how the layout of the site and design of buildings conforms to the principles of Secure by Design and the British Parking Association's Safer Parking Scheme.

Reason: to ensure that the development is safe, inclusive and accessible and that crime and disorder, and the fear of crime, do not undermine the quality of life or community cohesion and resilience in accordance with NPPF Section 12 'Achieving well-designed places, HMCLG's Planning Practice Guidance on 'Design', Core Strategy policies CP1 & CP3.

Waste storage

- 6. Reserved matters for each phase shall incorporate suitable internal and external storage for refuse and recyclable materials as appropriate to the development. The storage should be both functional and well-integrated in the development.

No building shall be occupied until refuse and recycling storage facilities to serve it have been provided in accordance with the approved details. These facilities shall be permanently retained thereafter and used for no purpose other than the temporary storage of refuse and recyclable materials.

Reason: To ensure that adequate provision is made for the storage of refuse and recyclables without detriment to visual amenity in accordance Wokingham Borough Core Strategy Policies CP1, CP3, Managing Development Delivery Local Plan policy CC04 and the Sustainable Design and Construction SPD.

Art

- 7. Before submission of the first application pursuant to reserved matters, a strategy for delivery of public art throughout the site, setting out principles for:
 - a. public engagement;
 - b. the nature of the artwork;
 - c. number of pieces;
 - d. broad locations;

- e. procurement; and
- f. phasing of delivery in accordance with condition 3

shall be submitted to and approved in writing by the Local Planning Authority and the strategy shall be implemented as approved.

Reason: In the interests of achieving high quality development with a sense of place, in accordance with Core Strategy policies CP1 Sustainable Development, CP3 General Principles for Development, Appendix 7 (paragraph A7.52) and the South Wokingham SPD (Design Principles 3f and 1e(iv)). Details are required prior to the first reserved matters in order to ensure coordinated delivery throughout phase 2.

Materials

- 8. Before development in in each phase of the development hereby approved begins, samples and details of the materials to be used in the construction of the external surfaces of the building(s) consistent with the Design Code pursuant to condition 4 and having regard to the need to reinforce that the development is part of the town of Wokingham, shall be submitted to and approved in writing by the local planning authority. Development shall be carried out in accordance with the approved details.

Reason: to ensure a high-quality development, appropriate to the location in accordance with Core Strategy policies CP1, CP3, CP21 and the South Wokingham SDL SPD. Details are required prior to commencement to because a coordinated approach is required to ensure the appearance and overall quality of development is appropriate.

Ground and building levels

- 9. Before submission of the first application for reserved matters a strategy for levels across the site, consistent with the SuDS Strategy to comply with condition 26 and highway design shall be submitted to and approved in writing by the Local Planning Authority. These shall establish the range of levels within each parcel and demonstrate continuity across the site and with the South Wokingham Distributor Road (SWDR).

Reason: in order to ensure a satisfactory form of development relative to surrounding buildings and landscape in accordance with Core Strategy policies CP1, CP3 & CP21; Managing Development Delivery Local Plan policy TB21; and the South Wokingham SDL SPD. Details are required before reserved matters to ensure a coordinated approach to levels and drainage across phase 2 ad the SWDR.

- 10. No development (including laying of the SANG) shall take place in each phase until a measured survey of the site and a plan prepared to scale of not less than 1:500 showing details of existing and proposed finished ground levels (in relation to a fixed datum point) and finished roof levels has been submitted to and approved in writing by the local planning authority, and the approved scheme shall be fully implemented prior to the occupation of the building(s) or the land being brought into use.

Reason: in order to ensure a satisfactory form of development relative to surrounding buildings and landscape in accordance with Core Strategy policies CP1, CP3 & CP21; Managing Development Delivery Local Plan policy TB21; and the South Wokingham SDL SPD. Details are required before reserved matters to ensure a coordinated approach to levels and drainage across phase 2 and the SWDR.

Tree protection

11. Before submission of the first application pursuant to reserved matters or commencement of any operation including landscaping in the SANG (whichever is earlier) a site-wide plan showing the combined tree loss resulting from the proposals and the construction of the SWDR (planning permission 192928 or any application that supersedes it) shall be submitted to and approved in writing by the Local Planning Authority.

Reason: to ensure the combined impact of the proposals for the site and other applications relating to the same land is fully understood and inform the details pursuant to conditions 12, 13 & 15 in accordance with Core Strategy policies CP1, CP3 & CP21; Managing Development Delivery Local Plan policies CC03 and TB21; and the South Wokingham SDL SPD.

12. Each application for approval of reserved matters shall be accompanied by an updated Arboricultural Impact Assessment to inform the design of the phase and the landscaping and tree protection details to comply with conditions 13 & 15.

Reason: to secure the protection throughout the time that development is being carried out, of trees, shrubs and hedges growing within the site which are of amenity value to the area in accordance with Core Strategy policies CP1, CP3 & CP21; Managing Development Delivery Local Plan policies CC03 and TB21; and the South Wokingham SDL SPD.

13. No development or other operation including landscaping in the SANG shall commence until an Arboricultural Method Statement and Scheme of Works for the relevant phase which provides for the retention and protection of trees, shrubs and hedges growing on or adjacent to the site in accordance with BS5837: 2012 has been submitted to and approved in writing by the local planning authority. No development or other operations shall take place except in complete accordance with the details as so-approved (hereinafter referred to as the Approved Scheme).
- a) No operations in that phase shall commence in connection with development hereby approved (including any tree felling, tree pruning, demolition works, soil moving, temporary access construction and or widening or any other operation involving use of motorised vehicles or construction machinery) until the tree protection works required by the Approved Scheme are in place on site.
 - b) No excavations for services, storage of materials or machinery, parking of vehicles, deposit or excavation of soil or rubble, lighting of fires or disposal of liquids shall take place within an area designated as being fenced off or otherwise protected in the Approved Scheme.
 - c) The fencing or other works which are part of the Approved Scheme shall not be moved or removed, temporarily or otherwise, until all works including external works have been completed and all equipment, machinery and

surplus materials removed from the site, unless the prior approval in writing of the local planning authority has first been sought and obtained.

Reason: to secure the protection throughout the time that the development is being carried out of trees shrubs or hedges growing within or adjacent to the site which are of amenity value to the area, and to allow for verification by the local planning authority that the necessary measures are in place before development and other works commence Relevant policy: Core Strategy policy CP3 and Managing Development Delivery Local Plan policies CC03 and TB21. Details are required prior to commencement to ensure that measures are in place when work starts.

14. No trees, shrubs or hedges within the site which are shown as being retained on the approved plans shall be felled, uprooted wilfully damaged or destroyed, cut back in any way or removed without previous written consent of the local planning authority; any trees, shrubs or hedges removed without consent or dying or being severely damaged or becoming seriously diseased within 5 years from the completion of the development hereby permitted shall be replaced with trees, shrubs or hedge plants of similar size and species unless the local planning authority gives written consent to any variation.

Reason: to secure the protection throughout the time that development is being carried out, of trees, shrubs and hedges growing within the site which are of amenity value to the area. Relevant policy: Core Strategy policy CP3 and Managing Development Delivery Local Plan policies CC03, TB21 & TB23; and the South Wokingham SDL SPD.

Landscape, ecology and green infrastructure

- 15.
- i) No development shall take place in each phase of the development including the SANG until full details of both hard and soft landscape works for that phase have been submitted to and approved in writing by the local planning authority and these works shall be carried out as approved. The details shall be consistent with the Site-Wide Masterplan, Design Code and Landscape Design Statement pursuant with condition 4 and include, as appropriate:
 - a) Scheme drawings;
 - b) proposed finished floor levels and contours in accordance with the details pursuant to conditions 9 & 10;
 - c) detailed design of SuDS features in accordance with the SuDS Strategy, demonstrating how they will be integrated into the wider landscape, with attenuation basins having a natural shape and shallow profile (not requiring lifesaving equipment and fence barriers), allowing them to fulfil amenity, ecological and drainage functions;
 - d) soft landscaping details including planting plans, schedules of plants, noting species, planting sizes and proposed numbers/densities where appropriate;
 - e) a Landscape Specification document covering soft landscaping (including site preparation, cultivation, plant handling and other operations associated with plant and grass establishment) and hard landscaping

including all construction works such as paths, bridges and retaining walls within public open space;

- f) a three metre wide flexipave, flexistone or equivalent path through the public open space to the south of parcel D which can also fulfil the emergency access requirements of conditions 3vi), 3vii) & 44.b;
- g) a three metre wide flexipave, flexistone or equivalent northeast-southwest route through the SANG connecting to the equivalent path within phase 2b;
- h) a three metre wide flexipave, flexistone or equivalent east-west route through the SANG connecting parcel D (as identified on Drawing No P18-2684_41E Parameters Land Use) with the new footbridge over the Emmbrook;
- i) consideration of the interface between the shared surface within residential parcel D and Wokingham Footpath 25 outside the application site, west of Chapel Green;
- j) a pedestrian and cycle connection between the access to parcel C2 and Wokingham Footpath 24 and Wokingham Without Footpath 10;
- k) a second pedestrian link between the SANG and parcel D (as identified on Drawing No P18-2684_41E Parameters Land Use)
- l) planting, in particular boundary treatments, to reinforce the rural appearance of the lane and proposed public open space to the west of Lucas Hospital;
- m) hard landscaping materials including samples;
- n) minor artefacts and structures (e.g. street furniture, play equipment, refuse or other storage units, signs, external services) including specifications for the product and its installation.
- o) specification for tree rooting systems and use of structural soils under paving or where rooting volumes are limited;
- p) all boundary treatments and other means of enclosure or controlling access such as gates and bollards, which shall include consideration of ecological permeability and control of access to allotment sites;
- q) car parking layouts, other vehicle and pedestrian access and circulation areas;
- r) measures required for ecological mitigation or Biodiversity Net Gain;
- s) public art in accordance with the strategy approved pursuant to condition 7;
- t) a minimum eight metre wildlife zone to watercourses measured from the top of the bank within which all planting shall be native species of local provenance;
- u) how non-native species such as Himalayan Balsam will be eradicated;
- v) how the buffer zone will be protected during development and managed over the long term; and
- w) how the river channel morphology and bankside habitat will be enhanced to contribute to biodiversity net gain.

- ii) The landscaping details for each Phase of the development shall include a Landscape Phasing Plan demonstrating timely delivery of hard and soft landscaping in relation to adjacent built development and the approved landscaping for each "Landscape Phase" shall be carried out in accordance with the approved phasing. The scheme shall be maintained in the approved form for so long as the development remains on the site.
- iii) No development shall take place in each phase until details of quality control measures, including supervision of landscape contract(s) by a suitably qualified landscape specialist and annual landscape audits for the five-year period from completion of the landscaping for the Landscape Phase or until adoption (whichever is longer) have been submitted to and approved in writing by the Local Planning Authority. The annual Landscape Audit shall be submitted to the Local Planning Authority for information prior to the next planting season and replacement planting undertaken in accordance with the landscape audit and iv) below.
- iv) Any trees or plants which, within a period of five years after planting, are removed, die or become seriously damaged or defective, shall be replaced in the next planting season with others of species, size and number as originally approved and permanently retained.

Reason: In the interests of visual amenity, ecology and the setting of heritage assets in accordance with Core Strategy policies CP1, CP3, CP7 & CP21; Managing Development Delivery Local Plan policies CC03, TB21, TB23 & TB24; the South Wokingham SDL SPD; and National Planning Policy Framework Section 16 (Conserving and Enhancing the Historic Environment). Details are required prior to commencement to ensure that landscaping and ecological mitigation and enhancement can be satisfactorily integrated in the development.

Landscape and Ecological Management Plan (LEMP)

- 16. Prior to the commencement of any Phase of the development including the SANG a Landscape and Ecological Management Plan (LEMP), including long term design objectives, management responsibilities, timescales and maintenance schedules for all landscape areas, other than privately owned, domestic gardens, shall be submitted to and approved in writing by the local planning authority. The Landscape and Ecological Management Plan shall be carried out as approved.

Reason: In order to ensure that provision is made for satisfactory maintenance of the landscaping hereby approved in accordance with Core Strategy policies CP1, CP3, CP7 & CP21; Managing Development Delivery Local Plan policies CC03, TB21 & TB23; and the South Wokingham SDL SPD. Details are required before commencement because then need to be assessed in conjunction with the landscaping proposals.

Suitable Alternative Natural Greenspace (SANG)

- 17. No dwelling shall be occupied until the Suitable Alternative Natural Greenspace (SANG) has been provided in accordance with the details approved pursuant to condition 15, together with a circular walking route of at least 2.3-2.5km in accordance with details that have first been submitted to and approved in writing

by the Local Planning Authority and written confirmation has been received from the Local Planning Authority (LPA) that the SANG and walking route have been delivered to an acceptable standard and are available for public use.

Reason: to ensure that the amount and quality of SANG provided is sufficient to mitigate the impact of the development on the Thames Basin Heaths Special Protection Area in accordance with Core Strategy policies CP8 and CP21 and the South Wokingham SDL SPD.

Ecology

18. The reserved matters for each phase shall include details of the type and location of bat and bird boxes in accordance with strategy established by the Environmental Statement and provision shall be made in accordance with the approved details.

Reason: to ensure appropriate mitigation of the impact upon this protected species in accordance with Core Strategy policies CP3 & CP7; and Managing Development Delivery Local Plan policy TB23.

19. Before commencement of development within each phase including the SANG a Biodiversity net gain Assessment demonstrating an overall net gain shall be submitted to and approved in writing by the Local Planning Authority and development shall be carried out in accordance with the approved Assessment.

Reason: to ensure that the development results a net gain for biodiversity, as required by the NPPF section 15. Details are required prior to commencement to demonstrate that a net gain can be achieved.

20. Prior to commencement of development in either parcels C1 or C2, whichever is the first, mitigation for the very high distinctiveness lowland fen habitat of principal importance shall be implemented in accordance with a detailed mitigation strategy that has first been submitted to and approved in writing by the local planning authority. The strategy shall include:

- i) identification of the translocation donor and receptor site;
- ii) preparation requirement for the receptor site;
- iii) translocation method;
- iv) protection and ongoing management measures for the receptor site;
- v) monitoring and remedial contingency; and
- vi) timings of works;

The approved mitigation strategy shall be implemented in full in accordance with the approved details and a report detailing the works undertaken shall be submitted to the local planning authority prior to adoption of the SANG.

Reason: To ensure appropriate mitigation of the very high distinctiveness lowland fen habitat of principal importance in accordance with Core Strategy policies CP3 & CP7; and Managing Development Delivery Local Plan policy TB23.

Lighting

21.

- i) The first application pursuant to reserved matters shall be accompanied by an Overarching External Lighting Strategy. The Strategy will balance the safety of the public realm with ecological and amenity considerations, referencing current guidance on lighting mitigation and establishing how light spill on habitats used by foraging and commuting bats will be avoided.
- ii) The reserved matters for each phase of the development shall include detailed lighting scheme in accordance with the Overarching External Lighting Strategy.
- iii) Lighting of the public realm shall be installed in accordance with the approved details before the area it will illuminate comes into use.

Reason: to ensure an appropriate balance is achieved between the safety of the public realm and avoiding detriment to wildlife or residential amenity in accordance with NPPF Section 15 (Conserving and Enhancing the Natural Environment); Core Strategy policies CP1, CP3, CP7, CP21; and Managing Development Delivery Local Plan policies TB21 & TB23.

Archaeology

22.

- i) Prior to commencement of development (including landscaping) in the SANG and prior to submission of reserved matters for each phase of the development hereby approved in outline, a programme of archaeological work for that phase, to include a Written Scheme of Investigation, shall be submitted to and approved in writing by the local authority. The scheme shall include an assessment of significance and research questions, methodologies for site investigation and recording, a programme for post-investigation assessment and analysis, and provision for publication of results and archiving. The approved scheme shall be implemented either prior to reserved matters, or phased following consent, as agreed in writing with the local planning authority.
- ii) No demolition or development shall take place other than in accordance with the Written Scheme of Investigation approved i) above.
- iii) No phase of development shall be occupied or brought into use until the site investigation and post-investigation assessment have been completed for that phase, in accordance with the programme set out in the Written Scheme of Investigation approved under i) above, and provision for the analysis, publication and dissemination of results and archive deposition has been secured.

Reason: the site is identified as being of archaeological potential. Investigation is required prior to commencement to allow preservation and recording of any archaeological features or artefacts before disturbance by the development in accordance with National Planning Policy Framework Section 16 (Conserving and Enhancing the Historic Environment) and Managing Development Delivery Local Plan policy TB25.

Permitted Development

23. Notwithstanding the provisions of the Town and Country Planning (General Permitted Development) Order 2015 (as amended) (or any Order revoking and re-enacting that Order with or without modification) no extensions, roof additions or outbuildings permitted by Classes A, B & E of Part 1 of the Second Schedule of the 2015 Order (or any order revoking and re-enacting that order with or without modification) shall be carried out.

Reason: Based on the illustrative details submitted, there is potential for extensions and outbuildings to reduce separation distances and private amenity space to the extent that residential amenity would be harmed, contrary to Core Strategy policies CP1, CP3 & CP21; the South Wokingham SDL SPD and the guidance in the Borough Design Guide.

Internal space standards

24. Each dwelling shall be compliant with the DCLG Technical Housing Standards – Nationally Described Space Standard (March 2015) or any subsequent internal space standards for dwellings that superseded them.

Reason: to ensure appropriate amenity for residents in accordance with Managing Development Delivery Local Plan policy TB07 and Borough Design Guide design principle R17.

Flood Risk Assessment

25. Development shall be carried out in accordance with the *Flood Risk Assessment September 2019* (Issue 4 January 2022) and *Flood Risk Assessment Addendum* (February 2022). There will be no culverting of watercourses other than that already approved by this planning permission. Should the proposals for the SWDR and associated flood storage mitigation and watercourse diversions change materially, a FRA addendum comparing proposed flood levels and extents for the 1%, 1% plus 35% allowance for climate change, 1% plus 70% allowance for climate change and 0.1% annual probability events to those approved within the current model. The FRA shall include additional flood storage mitigation for any increases in flood risk identified and the agreed mitigation shall be implemented, retained and maintained thereafter through the lifetime of the development.

Reason: to prevent the increased risk of flooding, and to protect water quality in accordance with NPPF Section 14 (Meeting the Challenge of Climate Change, Flooding and Coastal Change), Technical Guidance on the NPPF (Flood Risk), Core Strategy policies CP1 & CP21; Managing Development Delivery Local Plan policies CC09 & CC10; and the South Wokingham SDL SPD.

Surface water drainage and Sustainable Drainage Systems (SuDS)

26. Before or concurrent with submission of the first application pursuant to reserved matters or commencement of development within the SANG (whichever is earlier), a Strategy demonstrating coordination with the wider Strategic Development Location (the SuDS Strategy), based on the *Flood Risk Assessment September 2019* (Issue 4 January 2022) and *Flood Risk Assessment Addendum* (February 2022) and including infiltration tests in accordance with BRE Digest 365 (or such other guidance as has first been agreed in writing by the Local Planning Authority),

evidence of the level of the seasonally high groundwater table , drainage calculations, details of the layout, position and size of attenuation basins, and principles for locally based treatments such as rain gardens, filter strips and swales shall be submitted to and approved in writing by the Local Planning Authority.

Reason: to prevent the increased risk of flooding, and to protect water quality in accordance with NPPF Section 14 (Meeting the Challenge of Climate Change, Flooding and Coastal Change), Technical Guidance on the NPPF (Flood Risk), Core Strategy policies CP1 & CP21; Managing Development Delivery Local Plan policies CC09 & CC10; and the South Wokingham SDL SPD.

27. Reserved matters for each phase shall include details of SuDS to serve that phase, based on the approved SuDS Strategy. SuDS shall be provided before occupation of the development it is to serve in accordance with the approved details and the phasing details pursuant to condition 3 and retained thereafter.

Reason: to prevent increased flood risk from surface water run-off in accordance with NPPF Section 14 (Meeting the Challenge of Climate Change, Flooding and Coastal Change); Core Strategy policy CP1; and Managing Development Delivery Local Plan policies CC09 and CC10; and Design Principle 1d(ii) of the South Wokingham SDL SPD.

28. No building hereby permitted shall be occupied until surface water drainage works have been provided in accordance with details that first have been submitted to and approved in writing by the local planning authority.

Reason: to prevent the increased risk of flooding, and to protect water quality in accordance with NPPF Section 14 (Meeting the Challenge of Climate Change, Flooding and Coastal Change), Technical Guidance on the NPPF (Flood Risk), Core Strategy policies CP1 & CP21; Managing Development Delivery Local Plan policies CC09 & CC10; and the South Wokingham SDL SPD.

29. Before each phase of the development is brought into use measures for effective water quality treatment (using the methodology set out in the SuDS Strategy and Section 26.7 of the CIRIA SuDS Manual (C753) or any guidance that supersedes it) shall be provided in accordance with details that have first been submitted to and approved in writing by the Local Planning Authority.

Reason: to protect water quality in accordance with NPPF Section 14 (Meeting the Challenge of Climate Change, Flooding and Coastal Change), Technical Guidance on the NPPF (Flood Risk), Core Strategy policies CP1 & CP21; and the South Wokingham SDL SPD.

30. The existing foul water provision serving properties Chapel Green shall not be disconnected until suitable alternative provision has been made.

Reason: To ensure provision of drainage for these existing properties is maintained in accordance with Core Strategy policies CP1 & CP4.

Drainage exceedance routes

31.

- i) Reserved matters and development in the SANG shall be in accordance with Drawing No 10005/1603 Rev P1 *Surface Water Exceedance Plan* (Appendix B of the *Flood Risk Assessment Addendum*, February 2022) or an alternative Strategy for Exceedance Flow Routing for flows above the 1 in 100+40% climate change across the site which has first been submitted to and approved in writing by Local Planning Authority. Any alternative strategy shall identify exceedance flow routes through the development based on proposed topography with flows being directed to highways and areas of public open space. Flow routes through gardens and other areas in private ownership will not be permitted.
- ii) Reserved matters shall be accompanied by a detailed exceedance flow routing plan consistent with the approved Strategy.
- iii) Landscaping of the SANG and reserved matters shall be in accordance with the approved details and works shall be implemented in accordance with the approved details before any phase of the development is first brought into use/occupied.

Reason: to prevent the increased risk of flooding, and to protect water quality in accordance with NPPF Section 14 (Meeting the Challenge of Climate Change, Flooding and Coastal Change), Technical Guidance on the NPPF (Flood Risk), Core Strategy policies CP1 & CP21; Managing Development Delivery Local Plan policies CC09 & CC10; and the South Wokingham SDL SPD.

Drainage (Maintenance)

32.

SuDS maintenance shall be carried out in accordance with the *Drainage Maintenance & Management Plan* (February 2022) or an alternative plan SuDS Management and Maintenance Plan for the lifetime of the development that has first been submitted to and approved in writing by Local Planning Authority. Any alternative plan should include details of:

- i) arrangements to secure the operation of the scheme throughout its lifetime including adoption by a Private Management Company, WBC or a Statutory Undertaker;
- ii) maintenance access; and
- iii) a method statement for safe and sustainable removal and disposal of waste from drainage system, detailing frequency, the materials to be used and standard of work.

A GIS shape file for the drainage system serving the site shall be submitted to the Local Planning Authority before occupation of the 150th dwelling.

Reason: To prevent increased flood risk from surface water run-off in accordance with NPPF Section 14 (Meeting the Challenge of Climate Change, Flooding and Coastal Change), Core Strategy policy CP1 and Managing Development Delivery Local Plan policies CC09 and CC10.

Construction Environmental Management Plan (CEMP)

33. Prior to commencement of development (including demolition and site clearance) in each phase (including the SANG) a Construction Environmental Management Plan (CEMP) to control the environmental effects of the demolition and construction work within that phase shall be submitted to and approved in writing by the Local Planning Authority. The CEMP shall include:
- i) measures for the control of dust, odour and other effluvia;
 - ii) measures for the control of noise (including noise from any piling and permitted working hours);
 - iii) measures for the control of noise from delivery vehicles and times when deliveries and dispatches will be accepted;
 - iv) vibration monitoring;
 - v) measures for the control of pests and other vermin (particularly during site clearance);
 - vi) pollution control measures;
 - vii) measures to control of surface water run-off including protection of the aquatic environment in terms of water quantity and quality;
 - viii) measures to prevent spoil or building materials being deposited or stored within any area of the site liable to flood;
 - ix) a construction travel protocol or Green Travel Plan for the construction phase;
 - x) construction traffic management plan comprising:
 - a. analysis of the volumes of construction vehicles during construction phases for both light and heavy vehicles;
 - b. vehicle routes and notably lorry routes, with volumes of lorries; and
 - c. traffic management proposals including any mitigations, hours of operation and signage
 - xi) site construction access;
 - xii) haul routes within the site (supported by relevant surveys if not already adequately covered);
 - xiii) details of any site construction office, compound and ancillary facilities;
 - xiv) cycle storage and motor vehicle parking and turning for site operatives and visitors;
 - xv) loading, unloading and storage of plant and materials;
 - xvi) measures to prevent deposit of mud on the highway;
 - xvii) provision of an emergency water supply including fire hydrants to meet firefighting needs (including the installation arrangements and the timing of such an installation);
 - xviii) provision of boundary hoarding;
 - xix) lighting;
 - xx) a site security strategy;

- xxi) Details of any temporary diversions of Public Rights of Way and how they will be managed to minimise disruption to users (both distance and duration of diversion);
- xxii) protection of important trees, hedgerows priority habitats and other natural features
- xxiii) relevant ecological mitigation measures for protected species and species of principle importance;
- xxiv) updated survey information (due to long build out period), for example arboricultural and ecological surveys;
- xxv) an invasive, non-native species strategy;
- xxvi) contact details for complaints construction liaison officer;
- xxvii) Communications Plan to keep local residents, town/parish councils and ward members informed;
- xxviii) programme of works, including measures for traffic management and operating hours;
- xxix) monitoring and review mechanisms;
- xxx) implementation of the CEMP through an environmental management system;

Construction activity shall be carried out in accordance with the approved CEMP.

Reason: In the interests of the amenity of the area; protecting ecology and the landscape, avoiding pollution, increased flood risk and construction related congestion during construction in accordance with Core Strategy policies CP1, CP3, CP6 & CP7; MDDL policies CC03, CC06, CC07, CC10, TB21, TB23 and TB24; the Environmental Statement Phase 2 (March 2019) and Addendum (August 2020) and Environmental Statement Phase 2b (March 2019) & Addendum (August 2020). To avoid harm, measures need to be in place upon commencement.

Hours of work

34. No work relating to the development hereby approved, including works of demolition or preparation prior to building operations, shall take place other than:
- i) between the hours of 08:00 to 18:00 Monday to Friday; and
 - ii) 08:00-13:00 on Saturday; and
 - iii) at no time on Sundays or Bank or National Holidays; except for
 - iv) individual operations which cannot reasonably be undertaken within the construction working hours defined above and have been notified to the Local Planning Authority (including details of the nature extent and timetable for the works) at least two weeks in advance and agreed in writing (by exchange of letter).

Where works are agreed by the LPA under iv) above, key stakeholders including residential properties within an identified zone that has first been submitted to and approved in writing by the Local Planning Authority, ward members and town/parish councils shall be given written notice at least one week in advance of

the works taking place. The notification shall include details of the nature, extent and timetable for the works and telephone number that the party responsible the works can be contacted on for the duration of the works.

Reason: To protect the occupiers of neighbouring properties from noise and disturbance outside the permitted hours during the construction period in accordance with Core Strategy policies CP1 Sustainable Development and CP3 General Principles for Development and Managing Development Delivery Local Plan policy CC06 Noise whilst providing the flexibility where works outside the usual hours are unavoidable or would result in unacceptable disruption in the surrounding area.

Noise mitigation

35. The reserved matters for each phase of the development that includes noise sensitive premises shall be accompanied by a Noise Mitigation Scheme demonstrating how appropriate internal and external noise levels will be achieved. The scheme shall be implemented in full before any noise sensitive premises are occupied and the mitigation measures shall be maintained for the duration of the development.

Reason: to ensure that noise does not impact on the amenity of future occupants in accordance with Core Strategy Policy CP1 Sustainable Development, CP3; Managing Development Delivery Local Plan Policy CC06 Noise; and the South Wokingham SDL SPD. Details are required at this stage because measures the layout of the site, internal layout of buildings and construction of buildings all need to be considered.

Contaminated land

36. No development other than that required to be carried out as part of an approved scheme of remediation shall take place in each phase of the development until conditions a-d (below) have been complied with. If unexpected contamination is found after development has begun that is not addressed by the methodology detailed within the approved remediation scheme, development must be halted on that part of the site affected by the unexpected contamination to the extent specified by the Local Planning Authority in writing until d) has been complied with in relation to that contamination.

a. Site Characterisation

An investigation and risk assessment, in addition to any assessment provided with the planning application, must be completed in accordance with a scheme to assess the nature and extent of any contamination on the site, whether or not it originates on the site. The contents of the scheme are subject to the approval in writing of the Local Planning Authority. The investigation and risk assessment must be undertaken by competent persons and a written report of the findings must be produced. The written report is subject to the approval in writing of the Local Planning Authority. The report of the findings must include:

- i. a survey of the extent, scale and nature of contamination;

- ii. an assessment of the potential risks to:
 - human health,
 - property (existing or proposed) including buildings, crops, livestock, pets, woodland and service lines and pipes,
 - adjoining land,
 - groundwaters and surface waters,
 - ecological systems,
 - archaeological sites and ancient monuments;
- iii. an appraisal of remedial options, and proposal of the preferred option(s).

This must be conducted in accordance with DEFRA and the Environment Agency's 'Model Procedures for the Management of Land Contamination, CLR 11'.

b. Submission of Remediation Scheme

A detailed remediation scheme to bring the site to a condition suitable for the intended use by removing unacceptable risks to human health, buildings and other property and the natural and historical environment must be submitted to and approved in writing by the Local Planning Authority. The scheme must include all works to be undertaken, proposed remediation objectives and remediation criteria, timetable of works and site management procedures. The scheme must ensure that the site will not qualify as contaminated land under Part 2A of the Environmental Protection Act 1990 in relation to the intended use of the land after remediation.

c. Implementation of Approved Remediation Scheme

The approved remediation scheme must be carried out in accordance with its terms prior to the commencement of development other than that required to carry out remediation, unless otherwise agreed in writing by the Local Planning Authority. The Local Planning Authority must be given two weeks written notification of commencement of the remediation scheme works.

Following completion of measures identified in the approved remediation scheme, a verification report that demonstrates the effectiveness of the remediation carried out must be submitted to and approved in writing by the Local Planning Authority.

d. Reporting of Unexpected Contamination

In the event that contamination is found at any time when carrying out the approved development that was not previously identified it must be reported in writing immediately to the Local Planning Authority. An investigation and risk assessment must be undertaken in accordance with the requirements of a), and where remediation is necessary a remediation scheme must be prepared in accordance with the requirements of b), which is subject to the approval in writing of the Local Planning Authority.

Following completion of measures identified in the approved remediation scheme a verification report must be prepared, which is subject to the approval in writing of the Local Planning Authority in accordance with c).

Reason: To ensure that risks from land contamination to the future users of the land and neighbouring land are minimised, together with those to controlled waters, property and ecological systems, and to ensure that the development can be carried out safely without unacceptable risks to workers, neighbours and other offsite receptors in accordance with Core Strategy policy CP1.

Pedestrian and cycle strategy

37. Reserved matters shall provide for an interconnected network for routes for pedestrians and cyclists based on the principles established by the Design and Access Statement, the Design Code pursuant to condition 4 and a Pedestrian and Cycle Strategy that has first been submitted to and approved in writing by the Local Planning Authority. Provision shall be made in accordance with these approved details and the approved phasing pursuant to condition 3.

Reason: to ensure provision is made for sustainable travel in accordance with Core Strategy policies CP1, CP3, CP6 & CP21; and the South Wokingham SDL SPD.

PRoW and Greenways

38. Wokingham Footpath 24 and Wokingham Without Footpath 9 within the site and outside the residential parcels shall be upgraded to three metres wide with a flexipave, flexistone or equivalent surface in accordance with details that have first been submitted to an approved in writing by the Local Planning Authority and works shall be delivered in accordance with the approved details and with the phasing pursuant to condition 3v).

Reason: to ensure provision is made for sustainable travel in accordance with Core Strategy policies CP1, CP3, CP6 & CP21; and the South Wokingham SDL SPD.

39. The reserved matters for parcel C2 shall incorporate an alternative three metre wide, pedestrian and cycle route connecting Wokingham Footpath 24 with Wokingham Without Footpath 9 and Wokingham Without Footpath 10, allowing the Public Right of Way and Greenway network to remain useable on occasions when the connection under the SWDR road bridge is unusable due to flooding. The route shall be delivered in accordance with the approved details and the phasing pursuant to condition 3 and retained thereafter.

Reason: to ensure that the SANG, Public Right of Way network and Greenway network remain useable in times of flooding in order to fulfil their leisure and sustainable travel functions in accordance with

40. Reserved matters for any phase that is adjacent to a Public Right of Way (PRoW) shall include details of direct connections between residential parcels and the adjacent PRoW, which have regard to levels and the need to minimise negative effects on the landscape and ecological importance of trees and hedgerows and connections shall be provided in accordance with the approved details and the phasing pursuant to condition 3.

Reason: to ensure provision is made for sustainable travel in accordance with Core Strategy policies CP1, CP3, CP6 & CP21; and the South Wokingham SDL SPD.

Public Transport Strategy

41. Before first occupation details of a phased Interim Public Transport Strategy, to cover the period between first occupation and implementation of the long-term Public Transport strategy secured by the S106, shall be submitted to and approved in writing by the Local Planning Authority and the Strategy shall be implemented in accordance with the approved details.

Reason: to ensure provision is made for sustainable travel in accordance with Core Strategy policies CP1, CP3, CP6 & CP21; and the South Wokingham SDL SPD.

Car club

42. Before first occupation details of a car club, delivery of which may be phased, shall be submitted to and approved in writing by the Local Planning Authority and provision shall be made in accordance with the approved details, which shall include details of the car club operator, the types of vehicle or vehicles to be provided, the parking bays and how these will be implemented and how the car club will be marketed to residents or business users, including any agreed membership discounts or offers

Reason: to ensure provision is made for sustainable travel in accordance with Core Strategy policies CP1, CP3, CP6 & CP21; and the South Wokingham SDL SPD.

Occupations prior to completion of the SWDR

43. Before commencement of development traffic modelling, consistent with the approved phasing pursuant to condition 3, demonstrating the number of occupations that can take place in advance of the full South Wokingham Distributor Road (SWDR) between the A329 London Road and A321 Finchampstead Road being complete and open for public use shall be submitted to and approved in writing by the Local Planning Authority. The number of dwellings occupied before completion of the SWDR shall not exceed that approved.

Reason: to ensure comprehensive, coordinated development with the necessary supporting infrastructure including appropriate mitigation of the impacts on the highway network in accordance with Core Strategy policies CP6 & CP21; and the South Wokingham SDL SPD.

Site access arrangements

- 44.
- a. Access from the SWDR to each development parcel (C1, C2 and D as identified on Drawing No P18-2684_41E *Parameters Land Use*) shall be provided in accordance with planning permission 192928 or alternative proposals that have first been submitted to and approved in writing by the Local Planning Authority before first occupation of any dwelling within each parcel; and
 - b. emergency access from Luckley Road, designed to minimise intrusion into the surrounding public open space and the impact on the setting of the Grade

I Lucas Hospital as well as fulfilling the Greenway requirements of condition 15 f) shall be delivered in accordance with details that have first been submitted to and approved in writing by the Local Planning Authority and the phasing pursuant to condition 3 or before first occupation of 100 dwellings in parcel D (as identified on Drawing No P18-2684_41E *Parameters Land Use*) whichever is earlier. In any case no more than 100 dwellings shall be occupied in parcel D until the emergency access is operational.

Reason: to ensure comprehensive, coordinated development with the necessary supporting infrastructure including appropriate mitigation of the impacts on the highway network in accordance with Core Strategy policies CP6 & CP21; and the South Wokingham SDL SPD whilst also protecting the character of the area generally and the setting of the Grade I Lucas Hospital in particular in accordance with Core Strategy policies CP1, CP3, & CP21; Managing Development Delivery Local Plan policies CC03, TB21 & TB24; the South Wokingham SDL SPD; and National Planning Policy Framework Section 16 (Conserving and Enhancing the Historic Environment).

Off-site junction works

45. Off-site mitigation and junction capacity works shall be provided at the junctions of:
- a. Barkham Road/Barkham Street;
 - b. Bearwood Road/Barkham Road;
 - c. Barkham Road /Molly Millars Lane;
 - d. Molly Millars Lane/Finchampstead Road;
 - e. Finchampstead Road/Oakey Drive/SWDR;
 - f. Peacock Lane/Waterloo Road/Old Wokingham Road;
 - g. Peacock Lane/Vigar Way; and
- new junctions shall be provided at:
- h. Easthampstead Road/SWDR;
 - i. Easthampstead Road/Heathlands Road and the proposed Heathlands link road; and
 - j. from Waterloo Road into phase 2a of the development within the SDL (proposed under application 190914); together with
 - k. a new connection through phases 2a & 2b of the development within the SDL (proposed under applications 190914 & 191068) providing an alternative to Waterloo Road in advance of it being closed to through traffic.

in accordance with the phasing established by condition 3 and details that have first been submitted to and approved in writing by the Local Planning Authority.

Reason: to ensure comprehensive, coordinated development with the necessary supporting infrastructure including appropriate mitigation of the impacts on the highway network in accordance with Core Strategy policies CP6 & CP21; and the South Wokingham SDL SPD.

Off-site pedestrian and cycle provision

46. Off-site pedestrian and cycle and associated enhancement works comprising improvements on:
- a. Easthampstead Road between the SWDR and the Star Lane level crossing;
 - b. Easthampstead Road between the SWDR and the junction of Heathlands Road;
 - c. Easthampstead Road north of the junction with Waterloo Road to and including Peach Street (unless superseded by a wider environmental improvement scheme), and the side road junctions of Easthampstead Road with Westcott Road, Denton Road, Goodchild Road, Moles Close, Murdoch Road, Southlands Road, Waterloo Road and Starmead Drive;
 - d. north of Wokingham Footpath 17 and the railway (to include street lighting), Gipsy Lane, Langborough Road, Luckley Path (Wokingham Footpath 21), Murdoch Road/Howard Road junction;
 - e. Waterloo Road, Rances Lane and London Road, including junctions with Seaford Road and Waterloo Crescent;
 - f. Goodchild Road, Westcott Road and Seaford Road, including junctions with School Road, Orchard Close and London Road;
 - g. Waterloo Road and Clay Lane;
 - h. Waterloo Road and Peacock Lane to Jennets Park, including bus stop infrastructure; and
 - i. Luckley Road, Tangle Drive and Finchampstead Road; and
 - j. Waterloo Road once alternative access has been provided through phases 2a & 2b of the SDL (proposed under applications 190914 and 191068);

shall be implemented in accordance with the phasing established by condition 3 and details that have first been submitted to and approved in writing by the Local Planning Authority.

Reason: to ensure provision is made for sustainable travel in accordance with Core Strategy policies CP1, CP3, CP6 & CP21; and the South Wokingham SDL SPD.

Access, visibility splays and tracking

47. Each reserved matters shall include access details, including visibility splays (for footpaths, cycle paths, private drives and access roads as relevant) and tracking of motor vehicles.

Development shall not be occupied or brought into use until access to it has been provided in accordance with the approved plans.

Reason: in the interests of highway safety and convenience in accordance with Core Strategy policies CP3 & CP6.

48. No development shall be occupied or brought into use until access to the wider area for pedestrians, cyclists and motor vehicles has been provided in accordance with the phasing details approved pursuant to condition 3 v).

Reason: to ensure provision is made for sustainable travel in accordance with Core Strategy policies CP1, CP3, CP6 & CP21; and the South Wokingham SDL SPD.

Parking and servicing

49. Each reserved matters shall include details of car and motorcycle parking and in accordance with the principles established by the Design and Access Statement, the Design Code pursuant to condition 4 and the council's policies at the time.

No development shall be occupied or brought into use until the vehicular accesses, driveways, parking and turning areas to serve it have been provided in accordance with the approved details and the parking shall thereafter be retained in accordance with the approved details and shall remain available for the parking of vehicles at all times.

Reason: to ensure adequate parking provision in the interests of highway safety and convenience in accordance with Wokingham Borough Core Strategy Policies CP1, CP6 & CP21; Managing Travel Demand and Managing Development Delivery Local Plan policies CC07, TB20 & Appendix 2; and the South Wokingham SDL SPD and the Borough Design Guide.

50. Prior to the first occupation of any dwelling in each phase, a Parking Management Strategy for the management of on-street parking shall be submitted to and approved in writing by the local planning authority.

Reason: to ensure adequate parking provision in the interests of highway safety and convenience in accordance with Wokingham Borough Core Strategy Policies CP1, CP6 & CP21; Managing Travel Demand and Managing Development Delivery Local Plan policies CC07, TB20 & Appendix 2; and the South Wokingham SDL SPD and the Borough Design Guide.

51. Notwithstanding the provisions of the Town and Country Planning (General Permitted Development) (England) Order 2015 (or any Order revoking and re-enacting that Order with or without modification), any garages or carports approved pursuant to reserved matters shall be constructed in accordance with the approved details and kept available for the parking of vehicles ancillary to the residential use of the site at all times. Carports shall not be enclosed beyond any enclosure shown on the approved drawings without the prior written approval of the Local Planning Authority. Garages and carports shall not be used for any business use nor as habitable space.

Reason: to ensure provision of adequate parking and reduce the likelihood of unplanned roadside parking in accordance with Wokingham Borough Core Strategy Policies CP1 & CP6; and Managing Development Delivery Local Plan policy CC07 Parking & Appendix 2.

52. Reserved matters shall include details of cycle parking in accordance with the principles established by the Design Code pursuant to condition 4 and the council's policies at the time. No building shall be occupied and the use of public open space shall not commence until the cycle parking to serve it has been provided in accordance with the approved details and the cycle parking shall be retained for its intended purpose thereafter.

Reason: to ensure provision of infrastructure to support sustainable travel in accordance with Wokingham Borough Core Strategy Policies CP, CP6 & CP21; and Managing Development Delivery Local Plan policy CC07 Parking & Appendix 2; and the South Wokingham SDL SPD.

Highway Construction details

53. Prior to the commencement of development within each phase, full details of the construction of roads and footways, including levels, widths, construction materials, depths of construction, surface water drainage, lighting and implementation strategy for that phase shall be submitted to and approved in writing by the local planning authority. The roads and footways shall be constructed in accordance with the implementation strategy and approved details to road base level before the relevant development is occupied and the final wearing course will be provided within 3 months of occupation of the relevant development, unless other minor variations are agreed in writing by the Local Planning Authority.

Reason: to ensure that roads, cycleways and footpaths are constructed to prioritise use by sustainable modes, to a standard that would be suitable for adoption as publicly maintainable highway and incorporate ecological permeability measures in the interests of providing a functional, accessible, safe and high-quality development that supports biodiversity in accordance with Core Strategy policies CP1, CP3, CP6, CP7 & Appendix 7; MDDL policy TB23; and the South Wokingham SDL and Infrastructure Delivery and Contributions SPDs. Details are required before commencement because they are fundamental to the proposal and the whole scheme needs to be considered as one to ensure the comprehensive, coordinated approach required by Core Strategy policy CP21.

Gates

54. Notwithstanding the provisions of the Town and Country Planning (General Permitted Development) (England) Order 2015 (or any Order revoking and re-enacting that Order with or without modification), no gates or barriers shall be erected on any shared vehicular access.

Reason: to assist in the integration of the development into the character and community of the area and in the interests of highway safety in accordance with Core Strategy policies CP1, CP3 & CP6; the South Wokingham SDL SPD and Wokingham Borough Design Guide SPD.

Electric Vehicle Charging

55. Reserved matters for each phase shall be accompanied by an Electric Vehicle Charging Strategy for the phase, including details of on-site infrastructure, the method of charging, the location and installation of charging points and future proofing of the site.

Provision for electric vehicle charging shall be made in accordance with the approved Strategy prior to first occupation of the development it is to serve and retained thereafter.

Reason: in order to ensure that secure electric vehicle charging facilities are provided so as to encourage the use of sustainable modes of travel in accordance with NPPF Section 9 (Sustainable Transport) and Core Strategy policies CP1, CP3 CP6 & CP21; Managing Development Delivery Local Plan policy CC07; and Appendix E of the WBC Living Streets: Highways Design Guide (2019).

Emergency water supply

56. Development shall incorporate provision of a water supply including fire hydrants to meet firefighting needs throughout the development.

Reason: To ensure that adequate measures for firefighting can be incorporated into the development.

Scheme for retention and/or reconfiguration of the electricity distribution apparatus

57. No development shall commence until a scheme for retention and/or reconfiguration of the electricity powerlines and associated apparatus has been submitted to and approved in writing by the local planning authority and works shall be carried out in accordance with the approved details.

Reason: to maintain the electricity supply whilst also achieving a high quality comprehensively planned development in accordance with Core Strategy Policies CP1, CP3 & CP21 and Appendix 7 (paragraph A7.48 d)) reinforced by the South Wokingham SPD (pages 11, 25 and 27).

Low and Zero Carbon Technologies

58. The reserved matters for each phase of the development shall be accompanied by:
- i) An Energy Statement demonstrating how development in the phase will achieve at least a 10% reduction in carbon emissions beyond the minimum requirements of Part L: Building Regulations (at the time of determination); and
 - ii) Details of how the water consumption target of 105 litres or less per person per day for new dwellings will be met.

The approved measures shall be installed and functional before first occupation of the buildings they are intended to serve.

Reason: In the interests of promoting sustainable forms of development in accordance with NPPF, Wokingham Borough Core Strategy Policy CP1; Managing Development Delivery Local Plan Policies CC04 & CC05; the Sustainable Design and Construction Supplementary Planning Document (2010) and the South Wokingham Strategic Development Location Supplementary Planning Document (2011).

59. Reserved matters for each phase shall be accompanied by a Site Waste Management Plan and development shall be carried out in accordance with the approved details.

Reason: to minimise waste in accordance with Core Strategy policy CP1 and the Sustainable Design and Construction Supplementary Planning Document (2010).

Informatives

Reason for recommendation

1. The development accords with the policies contained within the development plan and there are no material considerations that warrant a different decision being taken.

Relevant policies

2. You are advised, in compliance with The Town and Country Planning [Development Management Procedure] [England] Order 2010 that the following policies and/or proposals in the development plan are relevant to this decision:

National Planning Policy Framework

South East Area Plan saved policy	NRM6	Southeast Plan Policy NRM6 <i>Thames Basin Heaths Special Protection Area</i>
Wokingham Borough Core Strategy Development Plan Document (2010)	CP1	Sustainable Development
	CP2	Inclusive Communities
	CP3	General Principles for Development
	CP4	Infrastructure Requirements
	CP5	Housing mix, density and affordability
	CP6	Managing Travel Demand
	CP7	Biodiversity
	CP8	Thames Basin Heaths Special Protection Area
	CP9	Scale and Location of Development Proposals
	CP10	Improvements to the Strategic Transport Network
	CP11	Proposals outside development limits (including countryside)
CP17	Housing Delivery	
CP21	South Wokingham Strategic Development Location	
Adopted Managing Development Delivery Local Plan (2014)	CC01	Presumption in Favour of Sustainable Development
	CC02	Development Limits
	CC03	Green Infrastructure, Trees and Landscaping
	CC04	Sustainable Design and Construction
	CC05	Renewable energy and decentralised energy networks
	CC06	Noise
	CC07	Parking

CC08	Safeguarding alignments of the Strategic Transport Network & Road Infrastructure
CC09	Development and Flood Risk (from all sources)
CC10	Sustainable Drainage
TB05	Housing Mix
TB07	Internal Space standards
TB08	Open Space, sport and recreational facilities standards for residential development
TB12	Employment Skills Plan
TB21	Landscape Character
TB23	Biodiversity and Development
TB24	Designated Heritage Assets
TB25	Archaeology

Borough Design Guide (2012)

South Wokingham Strategic Development Location Supplementary Planning Document (2011)

Infrastructure Delivery and Contributions Supplementary Planning Document (2011)

Affordable Housing Supplementary Planning Document (2013)

Sustainable Design and Construction Supplementary Planning Document (2010) & Companion Guide (2010)

Crowthorne Village Design Statement

DCLG – National Internal Space Standards

Community Infrastructure Levy (CIL) & S106

3. The development hereby permitted is liable to pay the Community Infrastructure Levy. As an affordable housing development a claim for relief can be made. This is a matter for the developer. The Liability Notice issued by Wokingham Borough Council will state the current chargeable amount. Anyone can formally assume liability to pay, but if no one does so then liability will rest with the landowner. There are certain legal requirements that must be complied with. For instance, whoever will pay the levy must submit an Assumption of Liability form and a Commencement Notice to Wokingham Borough Council prior to commencement of development. For more information see <http://www.wokingham.gov.uk/planning/developers/cil/cil-processes/>.

4. This permission should be read in conjunction with the legal agreement under Section 106 of the Town and Country Planning Act the contents of which relate to this development.

Public engagement

5. The CEMP pursuant to condition 33 shall include provisions for engagement with local residents throughout the construction phase including a communications plan and appointment of a complaints liaison officer.

Materials

6. Condition 8 refers to the materials to be used in the construction of buildings. Hard landscaping materials including those to be used in construction of footpaths etc. will be considered under condition 15.
7. Due to the way that the town of Wokingham has evolved, including 20th century expansion, materials – in particular widespread use of red/orange brickwork and tiles - are one of the most distinctive and unifying characteristics of the town. The materials pursuant to conditions 4 & 8 should reflect this local character, reinforcing the connection with the town of Wokingham and avoiding the use of uncharacteristic materials and colours.

SANG Landscaping

8. The SANG landscaping details are broadly acceptable but need to be updated to incorporate footpaths in accordance with the requirements of conditions 15g) & 15h), the Biodiversity Net Gain assessment pursuant to condition 19 and the lowland fen translocation details pursuant to condition 20. The ecologist's consultation response dated 24 February 2022 refers.

Landscape and Ecological Management Plan (LEMP)

9. The LEMP pursuant to condition 16 is broadly acceptable but needs to be updated to ensure consistency with Biodiversity Net Gain assessment pursuant to condition 19 and the lowland fen translocation details pursuant to condition 20, including the including the long term management regime associated with proposed high distinctiveness habitats. The ecologist's consultation response dated 24 February 2022 refers.

Hard landscaping & furniture specification

10. The proposed SANG and the Holme Park SANG, within phase 2b of the South Wokingham SDL have been designed to be experienced as a single open space and, therefore, the hard landscaping and furniture specification to comply with condition 15 should demonstrate a consistent approach to hard landscaping and furniture.

Play Space Design Guide

11. Details of play areas to comply with condition 15 should have reference to the council's Play Space Design Guide at <https://www.wokingham.gov.uk/planning->

Neighbouring properties

12. At the reserved matters stage, careful consideration of the relationship with 1 Chapel Green Cottages will be required in order to avoid unacceptable overlooking of the neighbouring property and its amenity space.

Affordable Housing mix

13. The affordable housing mix details pursuant to condition 3 i) shall be broadly in accordance with the mix set out in section 4.2 of the report to Planning Committee.

Cycle storage

14. Proposals for cycle storage pursuant to conditions 4 & 52 shall have regard to the advice in paragraph 236 of the committee report, namely: garages (3 x 7 metres internal dimensions) can accommodate two bicycles as well as a parked car. Where sheds are used direct access to the garden should be possible (balanced with achieving a secure layout). Drives should be at least 3.1 metres wide to allow a cycle to be pushed past a parked vehicle. Cycle storage for flats should either be within the building or in a purpose built structure, located where it would not encroach on amenity areas.

Waste

15. The provision for refuse storage to comply with condition 6 shall have regard to the advice on the council's website at <https://www.wokingham.gov.uk/rubbish-and-recycling/collections/waste-information-for-developers/> .

PRoW

16. Reliance on gates should be minimised. Where they cannot be avoided, gates or other infrastructure proposed in relation to delivery of Public Rights of Way shall be compliant with the British Standard for Gaps, Gates and Stiles (BS5709:2018), to ensure that they are accessible and easy to use by the public. No new gates or other barriers, other than any shown on the approved drawings, are to be installed without express consent from the Public Rights of Way Team. Self-closing mechanisms should be via off-set hinges rather than spring assemblies to allow ease of use by the public and reduce potential maintenance issues. Latches may be required where the gates lead out of the SANG to ensure that the gates cannot be opened by dogs without their owner present and to reduce the risk of children running out of the site unattended.

Transport

17. The Corporate Head of Environment at the Council Offices, Shute End, Wokingham should be contacted for the approval of the access construction details before any work is carried out within the highway. This planning permission does NOT authorise the construction of such an access.
18. If it is the developer's intention to request the Council, as local highway authority, to adopt the proposed access roads etc. as highway maintainable at public expense, then full engineering details must be agreed with the Corporate Head of Environment at the Council Offices, Shute End, Wokingham. The developer is

strongly advised not to commence development until such details have been approved in writing and a legal agreement is made with the Council under S38 of the Highways Act 1980.

19. Any works/events carried out either by, or at the behest of, the developer, whether they are located on, or affecting a prospectively maintainable highway, as defined under Section 87 of the New Roads and Street Works Act 1991, or on or affecting the public highway, shall be coordinated under the requirements of the New Roads and Street Works Act 1991 and the Traffic management Act 2004 and licensed accordingly in order to secure the expeditious movement of traffic by minimising disruption to users of the highway network in Wokingham.
20. Any such works or events commissioned by the developer and particularly those involving the connection of any utility to the site, shall be co-ordinated by them in liaison with Wokingham Borough Council's Street Works Team, (telephone 01189 746302). This must take place at least three months in advance of the works and particularly to ensure that statutory undertaker connections/supplies to the site are coordinated to take place wherever possible at the same time.
21. The junctions referred to in condition 45.e & 45.h are the subject of a separate application, 192928, approved on 24 May 2021. Condition 6 of 192928 requires detailed design to be submitted to and approved in writing by the LPA. For the purposes of conditions 45 & 46 submission of the relevant Local Planning Authority decision notice listing the details submitted pursuant to the relevant condition of 192928 and confirming their approval will be sufficient. Should 192928 or an alternative application not be implemented, full details would need to be submitted.
22. The junction referred to in condition 45.d is the subject of a separate application 203535, approved on 24 May 2021. Condition 3 of 203535 requires detailed design to be submitted to and approved in writing by the LPA. For the purposes of condition 45 submission of the relevant Local Planning Authority decision notice listing the details submitted pursuant to the relevant condition of 203535 and confirming their approval will be sufficient. Should 203535 or an alternative application not be implemented, full details would need to be submitted.
23. The junction referred to in condition 45.f is likely to be the subject of a separate planning application to Bracknell Forest Borough Council. Should planning permission be granted, for the purposes of condition 46.f), submission of the relevant Local Planning Authority decision notice listing the relevant details and confirming their approval will be sufficient. Otherwise, full details would need to be submitted.

Crime Prevention

24. The CEMP to comply with condition 33 shall have regard to the Construction (Design and Management) Regulations 2015 at <https://www.hse.gov.uk/pubns/priced/hsg151.pdf> which provides advice on site boundary treatments; access control; compound security and security precautions and advises the contractor to liaise with their local Police Crime Prevention Design Advisor.

25. Guidance on meeting Secure by Design standards can be found at https://www.securedbydesign.com/images/downloads/HOMES_BROCHURE_2019_NEW_version_2.pdf

Ecology

26. This permission does not convey or imply any approval or consent required under the Wildlife and Countryside Act 1981 for protected species. The applicant is advised to contact Natural England with regard to any protected species that may be found on the site.
27. Any gully pots required as part of the detailed SuDS proposals to comply with condition 26 shall be designed for ecological permeability and to prevent entrapment of wildlife.
28. Lighting to comply with condition 21 should be designed in accordance with the best practice guidance issued by the Bat Conservation Trust and Institute for Lighting Professionals (ILCP 2018) or any guidance that supersedes it.

Drainage

29. The Sustainable Drainage details to comply with conditions 26, 27 & 28 shall have regard to The Wokingham SuDS Strategy (2017).
30. The measures to control surface water run-off and pollution pursuant to condition 33 (parts vi), vii) & viii) in particular) should be based on CIRIA Site Handbook for the construction of SuDS C698 and incorporate intercept drains at the boundary.
31. The applicant is reminded that any works affecting the flow of water, including temporary works, within an ordinary watercourse will require consent from the Lead Local Flood Authority. The applicant is also reminded of Wokingham Borough Council's Land Drainage Bylaws that require no obstruction within eight metres of the edge of a watercourse without the consent of the Lead Local Flood Authority.
32. No person without the previous consent of the council shall erect any building or structure, whether temporary, or permanent, or plant any tree, willow or other similar growth within eight metres of the landward toe of the bank where there is an embankment or wall or within eight metres of the top of the batter where there is no embankment or wall, or where the watercourse is enclosed within eight metres of the enclosing structure. Eight metres is the minimum unless applicant making a special case and supplying technical data about soil stability etc.

Network Rail

33. Due to the close proximity of the proposed development to Network Rail land, Network Rail recommends the developer contacts Network Rail's Asset Protection team via AssetProtectionWessex@NetworkRail.co.uk prior to any works commencing on site, with a view to enter into an Asset Protection Agreement to enable approval of detailed works. More information can be obtained from our

website <https://www.networkrail.co.uk/running-the-railway/looking-after-the-railway/asset-protection-and-optimisation/>.

34. Whilst it would appear from the application that the proposed development is to be entirely within the curtilage of the application site, the granting of planning permission does not authorise you to gain access or carry out any works on, over or under your neighbour's land or property without first obtaining their consent, and does not obviate the need for compliance with the requirements of the Party Wall etc. Act 1996.

SUMMARY OF RELEVANT PLANNING HISTORY		
Application Number	Proposal	Decision
SWDR within Montague Park (Phase 1 of the SDL, formerly Buckhurst Farm).		
O/2010/1712 as varied by	Outline planning permission (with details of access) for up to 650 dwellings (final total 636 following reserved matters and non-material amendments) with associated infrastructure including SANG.	Approved 18 December 2012.
VAR/2015/0342 &		Approved 02 June 2015.
161963		Approved 24 April 2017.
RM/2013/0242	Reserved matters for the SWDR within Montague Park	Approved 12 June 2013
NMT/2014/0378	Non-material amendments to the SWDR design approved under RM/2013/0240	Approved 25 March 2014.
South Wokingham Distributor Road (SWDR) south of the railway		
172934	Eastern Gateway: full planning permission for the bridge over the Reading-Waterloo Railway line and connection to Waterloo Road.	Approved 19 February 2018 following the resolution of the planning committee on 14 February 2019. Opened January 2022.
173198	Scoping opinion for the South Wokingham Distributor Road (SWDR) between Waterloo Road and Finchampstead Road including associated works in the Finchampstead Road corridor.	
Stopping Up Order under Section 247 of the T&CPA 1990 (as amended)	Stopping Up of the section of Waterloo Road between the level crossing and the SWDR, once the new bridge and highway extending William Heelas Way to connect to Waterloo Road is complete.	Planning Committee resolution to submit an application to the DfT 13 December 2017.
		Order made by the Secretary of State for Transport on 09 November 2018.
190989	Full application for formation of a temporary vehicular access to Britton's Farm during construction of the Eastern Gateway.	Approved 23 July 2019.
190991	Formation of an attenuation pond and drainage ditch (works forming part of the drainage strategy	Approved 23 July 2019.

	pursuant to condition 17 of planning permission 172934 but requiring separate approval as they fall outside the red line).	
191080	Full application for erection of temporary class B1(a) office building and construction of temporary compound including associated car parking, storage, boundary treatment and spur road to Eastern Gateway haul road (to support the Wokingham Major Highways Programme).	Approved 23 July 2019.
192928	Full application for the construction of the South Wokingham Distributor Road (SWDR) between Finchampstead Road and Waterloo Road, including a link to Heathlands Road.	Approved on 24 May 2021 following the resolution of the Planning Committee on 18 May 2021.
203535	Full application for a larger roundabout at the junction of the A321 Finchampstead Road and Molly Millars Lane	Approved on 24 May 2021 following the resolution of the Planning Committee on 18 May 2021.
Consortium land		
190900	St Anne's SANG - a full, cross-boundary application with Bracknell, north of the railway line/adjoining the Montague Park SANG	Approved on 24 May 2021 following the resolution of the Planning Committee on 18 May 2021.
190914	Phase 2a: an outline application (with details of access) for up to 215 dwellings with associated infrastructure on the land adjacent to Bigwood/to the east of the SDL.	Planning Committee resolution to conditionally approve subject to S106 on 18 May 2021.
191068	Phase 2b: a hybrid application; outline (with details of access) for up to 1,434 dwellings with associated infrastructure and full for the SANG.	Planning Committee resolution to conditionally approve subject to S106 on 18 May 2021.
Land South East of Finchampstead Road		
10389	Use of land as a riding establishment	Approved 15 March 1979.
42995	Application for relocation & redevelopment of Wokingham Football Club and associated residential development on land east of the railway line (now Tesco) extending into the western fringes of the application site and the former football club site (now Oakey Drive) refused for reasons relating to character, flooding and car parking.	Refused 24 November 1994 & dismissed at appeal 26 July 1995.

43907	94 dwellings on land east of the railway line (now Tesco) refused due to lack of public open space, residential amenity and lack of affordable housing.	Refused 17 February 1995 & dismissed at appeal 26 July 1995.
183493	Scoping opinion for a residential development of up to 200 dwellings.	Responded 28 February 2019.

SUMMARY INFORMATION		
Site Area		18.75 hectares
Previous land use(s) and floorspace(s)		Equestrian sui generis
Residential		
Existing units		None
Proposed units	market	Up to 111 (65% of the maximum number of dwellings)
	affordable	Up to 60 (35% of the maximum number of dwellings)
	total	Up to 171
Proposed density		41 dph
public open space		11.93 hectares

CONSULTATION RESPONSES	
Berkshire Archaeology	No objection subject to a condition to secure further archaeological investigations. <i>(Officer Note: condition 22 refers.)</i>
Berks, Bucks and Oxon Wildlife Trust	No comments received.
Bracknell Forest Borough Council (BFBC)	No objection subject to improvements at the junction of Waterloo Road/Old Wokingham Road/Peacock Lane (within Bracknell Forest) and to cycle connectivity from the SDL to that junction. <i>(Officer note: sections 0, 0 & 0 and conditions 45.f & 46.h refer.)</i>
Cadent Gas	No objection: advise of the location of gas mains in the vicinity.
Crime Prevention Design Advisor	No objection but identifies issues that require further consideration at the reserved matters stage, which have been considered in the report. <i>(Officer Note: conditions 5 & 33 xx) and informatives 24 & 25 refer.)</i>
Environment Agency (EA)	Holding objection: <i>(Officer Note: an updated Flood Risk Assessment and Biodiversity Net Gain assessment have been submitted in response to the Environment Agency's comments and are currently under review. It is anticipated that these will enable the EA to withdraw their objection and recommend conditional approval. Drainage and Landscaping conditions have been drafted based on their</i>

	<i>advice in relation to other developments in the SDL and changes to these would be agreed with the Assistant Director Delivery and Infrastructure: Place and Growth before issue of planning permission. Part B of the Recommendation refers as do conditions 15 & 25.)</i>
Fields in Trust	No comments received.
Health and Safety Executive	No comments: the site does not currently lie within the consultation distance of a major hazard site or major accident hazard pipeline.
Highways England	No objection and no conditions recommended.
Historic England	No objection on heritage grounds.
National Grid	No comments received.
Natural England	No objection: the identified impacts on Thames Basin Heaths Special Protection Area can be appropriately mitigated with measures secured via planning conditions and obligations. <i>(Officer Note: sections 0, 0 and condition 17 refer.)</i>
Neos Networks	No objection: provided a plan showing that there are no Neos assets in the vicinity.
Network Rail	<p>Welcomes the provision for access to the residence that requires the use of the Smiths Private User worked Level Crossing, which could facilitate possible future closure of the crossing. <i>(Officer note: The Provisional Layout, Drawing No P18-2684_06W identifies a “potential access to western field and Knoll Farm” from parcel C2 and the S106 would secure this. See section 0 v). Should an access be proposed in future it would need to be assessed on its merits including consideration of the impact on very high distinctiveness habitat.)</i></p> <p>The development would have an impact on use of Star Lane level crossing and discussions between Network Rail, WBC and the developers are invited. <i>(Officer note: discussions have been taking place and improvements in the vicinity of the crossing are proposed. Condition 46.a and section 17.1 xxiii) refer. The expectation is that any future works by Network Rail on the level crossing with tie in with the</i></p>

	<p><i>improvements immediately to the north and south of it).</i></p> <p>Recommend the developer enters into an Asset Protection Agreement (<i>Officer note: informative 33 refers.</i>)</p>
NHS Wokingham Clinical Commissioning Group (CCG)	<p>No objection. Comment that the potential population numbers have been noted for planning purposes and the surgeries closest to the proposed development have been informed. The CCG continue to liaise with WBC with regard to S106 and CIL contributions for health services to help meet the increased demand associated with the aggregate impact of housing developments in the Borough but have nothing specific has been requested in relation to this proposal. (<i>Officer note: section 0 refers.</i>)</p>
Royal Berkshire Fire and Rescue	<p>No comments received. (<i>Officer Note: based on the requirements for phase 2, conditions 33 xvii) & 56 refer.</i>)</p>
Scottish & Southern Electricity Networks	<p>No objection: advise of the locations of electricity mains in the vicinity of the site</p>
Southern Electric Power Distribution	<p>No comments received.</p>
South East Water	<p>No comments received.</p>
Southern Gas Networks	<p>No objections raised: advise of the locations of gas mains in the vicinity.</p>
Sport England	<p>No objection and no detailed comments: consideration should be given to how the additional demand for sport generated by additional housing will be met and how development will provide opportunities or healthy lifestyles. (<i>Officer Note: sections 0 & 0 refer.</i>)</p>
Thames Water	<p>No objection: it is not proposed to discharge surface water to the public network (Lead Local Flood Authority approval would be required). However, it is proposed to pump sewage into the network which puts a higher strain on the network and a condition is recommended to confirm that there is sufficient capacity in the network. (<i>Officer Note: the utility company is a risk management company under the Flood and Water Management Act 2010 and is therefore required to follow statutory processes to ensure adequate delivery of foul sewage infrastructure. The WBC</i></p>

	<i>drainage team will obtain regular updates via the utility company liaison officer to ensure delivery).</i>
WBC Built Heritage Officer	No comments on water supply which is the remit of the South East Water Company. No objection: the proposals are unlikely to have an adverse impact on the setting and views of the heritage assets at Lucas Hospital, subject to appropriate treatment of the boundaries of the allotments and play area. <i>(Officer Note: condition 15 l) would secure this mitigation.)</i>
WBC Children's services/Education	No objection. <i>(Officer note: sections 0 & 0 refer.)</i>
WBC Community Sustainability	No objection. The proposals are consistent with the policy and Guidance for the South Wokingham SDL. <i>(Officer Note: section 0 refers.)</i>
WBC Drainage	No objection subject to conditions: the application is supported by a FRA. <i>(Officer Note: conditions 25 - 29 & 31- 33 refer).</i>
WBC Ecology	No objection subject to conditions and mitigation to be secured through the S106 agreement. <i>(Officer Note: conditions 15-21 & 33 refer.)</i>
WBC Economic Prosperity and Place	No objection subject to provision of affordable housing in line with the requirements of Core Strategy policy CP5. <i>(Officer Note: see section 0 and 0 of the appraisal.)</i> No objection subject to the S106 securing an Employment Skills Plan. <i>(Officer Note: sections 0 & 0 refer.)</i>
WBC Environmental Health Officer	No objection subject to conditions to secure mitigation of construction and operational impacts. <i>(Officer Note: conditions 33-36 refer.)</i>
WBC Green Infrastructure	No objection subject to conditions and mitigation to be secured through the S106 agreement. <i>(Officer Note: sections 0 & 0, conditions 3 & 15 and informatives 10 & 11 refer.)</i>
SDL Growth and Delivery (Planning Policy)	No objection: the principle of development has been established by Core Strategy CP21 and the proposals, including SANG, are in line with established expectations, subject to a detailed assessment.

WBC Health and Wellbeing	No comments received.
WBC Highways	No objection subject to conditions and mitigation to be secured through the S106 agreement. (<i>Officer Note: conditions 3, 4, 33, 37-55 and section 0 refer.</i>)
WBC Libraries and Information Services	No comments received. (<i>Officer note: section 0 refers.</i>)
WBC Place Clienting (Waste Services)	No objection but provide guidance on requirements to be considered in the detailed design. (<i>Officer Note: condition 6 & informative 15 refer.</i>)
WBC Public Rights of Way	No objection subject to conditions/a S106. (<i>Officer Note: conditions 4, 15, 33 & 38-40 and informative 16 refer.</i>)
WBC Trees and Landscape	No objection subject to conditions. (<i>Officer Note: sections 0 & 0 and conditions 3, 4 and 9-16 refer.</i>)

REPRESENTATIONS

53 representations were received in response to the initial public consultation (March 2020), objecting either to the development as a whole or to specific aspects of it. A further 63 representations were received following full consultation on the revised proposals (August 2021): one in support and the remainder objections. Of these 14 (including the Wokingham Society and Great Langborough Residents Association) had also commented on the original proposals and 49 were new correspondents. Four more representations were received following a second, more limited re-consultation (December 2021). Of these three had commented previously and one was a new correspondent. Other than commenting that the number of dwellings proposed is still significant, the majority of comments on the re-consultations related to the principle of residential development generally, rather than the differences between the original and revised proposals, so have been summarised together with comments received during the original consultation. The most commonly cited objections relate to the loss of green space that is well used for recreation and impact on infrastructure.

Specific to the revised plans received in December 2021

The proposals include removal of trees on land within the curtilage of 1 Chapel Green Cottages (confirmed by the Land Registry) which is included within the application site boundary. The application is therefore invalid. (*Officer Note: This is essentially a boundary dispute. The changes to the application site boundary related to the northern part of the site and the boundary in the vicinity of Chapel Green Cottages, which follows the OS base, has not been altered. Certificate B has been signed and notice served on the owner of Chapel Green Farm only. The tree is considered in paragraph 67.*)

Specific to the revised plans received in September 2021

The reduction in the number of dwellings goes some way to meeting concerns about the proximity of dwellings to Chapel Green House and the view over Chapel Green from Footpaths 10 & 11.

The revised plans would bring development Closer to Chapel Green Farm, which would now overlook a car park rather than gardens. *(Officer note: while the revised provisional layout does show dwellings slightly closer to Chapel Green Farm, the extent of residential parcel D has not changed in this location. Nor has the location of the allotment car park. As the application is in outline, the layout would not be fixed until the reserved matters stage but there is sufficient information available at this stage to ascertain that an acceptable relationship could be achieved; see paragraph 161.)*

The septic tank for Chapel Green Farm is within the application site and the revised plans still do not address how the house's sewage will be serviced. *(Officer note: the applicant has confirmed that there is a cesspool within the application site/the ownership of their landowner and that properties in Chapel Green have a right to connect to it. Any properties that currently discharge into the cesspool will be connected to mains drainage. Condition 30 refers.)*

In support

This site, close to the town centre should be capable of providing a high quality, sustainable development whilst at the same time protecting the environs of the Grade I former Lucas Hospital.

The proposal is consistent with the Local Plan adopted in 2010 & is well laid out, partly because of the necessity of avoiding the Emmbrook.

The development would be compatible with southern access to the homes at Knoll Farm which, if constructed, would enable the permanent closure of the user worked crossing (UWC) over the Wokingham to Bracknell railway, which is a hazard requiring a 30 mph speed restriction and adding about a minute to journey times.

The principle of development

Some think the need for additional housing should be reviewed: Wokingham has already met/exceeded its housing targets for the period to 2025; there are still empty homes on other recent developments; house building should be in other areas where there is real need; the pandemic has highlighted the importance of agriculture; it is not clear that demand for housing will remain at pre-pandemic levels. *(Officer note: the purpose of the ongoing Local Plan Update is to review the need for housing and establish a strategy for the next plan period. However, the current application must be assessed against the existing policy framework – in particular the Development Plan - as set out section 0 of this report.)*

What alternatives have been considered? Brownfield sites could be utilised; in the light of recent events, office sites may become available for redevelopment. *(Officer note: whether land is previously developed is a factor that weights in favour of sites when they are allocated and at the application stage but there are not sufficient brownfield sites available to meet the boroughs housing needs. The council's Five-Year Housing Land Supply includes an allowance for windfall sites. See section 0. Factors such as changing demand for sites for business use will be taken into consideration through the Local Plan Update.)*

The site is not allocated in the local plan and is situated outside of the settlement where it would erode the gap between Wokingham and Finchampstead. *(Officer note: the site is*

allocated for development as set out in section 0 of this report. Landscape impacts are considered in section 0.)

The council needs to prioritise the welfare of its current residents before trying to attract new ones. *(Officer note: minimum Local Housing Need is calculated using a standard method set out in national planning practice guidance and is based on national household growth projections and affordability.)*

How can 190 dwellings be acceptable when four houses at chapel Green were dismissed at appeal recently due to the impact on the setting of the Grade I Lucas hospital and views *(Officer Note: each application must be assessed on its own merit and the two differ in that the proposal referred to was for a small, piecemeal, windfall development, whereas the current application forms part of the comprehensively planned development of an allocated site.)*

Landscape, character & recreational value

Wokingham is known for its market town charm, rural features, open fields and farmland but is already overdeveloped/becoming an anonymous dormitory suburb extending from Bracknell to Reading, overcrowded and bland in appearance. The amount of dwellings/scale of development proposed is excessive and will accelerate this process. *(Officer note: the proposal is part of a planned urban extension - required to meet housing need – as set out in section 0 and, while development of this scale will inevitably change the character of the area, the proposals are consistent with Development Plan policy and guidance that require high quality development that respects its context. Sections 0 & 0 refer.)*

Wokingham lacks green areas and outdoor amenity space. The western part of the SDL is one of last tranquil, green spaces on the outskirts Wokingham (between Wokingham and Finchampstead) and functions as the lungs of the town. It is of high amenity value and contributes to the quality of life, being one of the nicest natural spaces within an easy walk of the town centre, due to its rural character, the contribution made by the Lucas Hospital and the footpaths which are well used by walkers, runners & cyclists; benefits that have been highlighted by the pandemic. The council should consider enhancing access and encourage greater use (a country park). Loss of this countryside will result in existing and new residents travelling further and increased pressure on the remaining open spaces (Gorick Wood and Ludgrove). Some consider SANG will not compensate for the loss of countryside (the more formal layout would be less attractive to wildlife) and views, in particular views to the west/over Chapel Green from Footpaths 10 & 11. Others welcome its inclusion, together with additional footpaths and the commitment to include public art. The SANG land too wet to be suitable for this purpose. *(Officer note: while the proposal does involve the development of accessible countryside, it does not involve the loss of public open space (POS) and incorporates POS – see section 0 – as well as maintaining and enhancing pedestrian and cycle access to the new POS within the SDL and countryside beyond -see sections 0 & 0.)*

There is a lack of separation between the proposed development and Chapel Green, a hamlet within the Green Belt and an important Gap. In accordance with Core Strategy policy CP11 *Proposals outside development limits (including countryside)* and a recent appeal decision there should be a transition at the rural interface, between the built form of the Strategic Development Location (SDL) and Chapel Green. Development here should respect the informal rural character of the area with larger, with wider plots and more

spacing between buildings to generate a more informal, looser feel in response to the immediate rural context. The proposal does not contribute to a sense of place and integrate with its surroundings. *(Officer Note: The site is not designated Greenbelt and there are no “Gap” polices in the current Development Plan. Section 0 sets out the relevant policy designations. The allocation of the site for development will inevitably affect the setting of Chapel Green but the proposals are consistent with Development Plan policy and guidance that require high quality development that respects its context. Sections 5 & 6 refer.)*

Parcels C1 and C2 should be omitted to achieve a better balance between housing delivery and other, negative impacts: they would not make a significant contribution to housing delivery (about 40 dwellings) but would require two junctions off the SWDR (interrupting traffic flow and increasing the cost of the road) and have a negative impact on Footpath 10 parts of which would run through housing estates. *(Officer Note: the application must be assessed on its merits against planning policy and guidance (which indicates development in these locations) and the fact that an alternative proposal might be preferable is not a reason to refuse an otherwise acceptable proposal. See sections 0 & 0.)*

Tree planting and high quality soft landscaping are required to soften the harsh appearance of this urban development, in particular the eastern boundary of parcel C1 should be strengthened. *(Officer note: section 0 refers.)*

The proposals for enhancing the Emmbrook corridor are welcomed. *(Officer note: sections 0 & 0 refer.)*

Three-storey development is inappropriate on a rural site like this. *(Officer note: section 0 refers.)*

It is assumed that the three-storey dwellings will be affordable housing, which should be provided in lower structures/less noisy locations. *(Officer Note: affordable housing requirements – including provision of a range of dwelling types and sizes - are considered in section 0. The distribution of affordable housing will not be established until the reserved matters stage but concentrating it in a single location would be contrary to policy. The impact of noise is considered in section 0.)*

The proposed density of 25 dph seems high for plot D which the SDP identifies as rural interface. *(Officer Note: see section 0.)*

The three-storey/16 metre high properties would dominate the skyline which once boasted views of St Paul’s church spire. The design does not reflect the existing style, which is varied and adds to the rural setting. *(Officer note: the building heights parameters plan has since been revised, reducing the maximum height of three-storey budlings to 12.5 metres as described in section 0. The Landscape and Visual Impact Assessment assesses the impact of the proposals on the wider landscape which is considered in section 0; design and layout is considered in section 0; design of buildings would not be considered until the reserved matters stage.)*

Public Art

The developers have said that they are willing to liaise with the South Wokingham Development consortium regarding provision of public art. Since this site will be some

distance from the rest of the development, the applicants should be required to make their own specific provision. *(Officer note: section 0 and conditions 3 x) & 7 refer.)*

Lucas Hospital

The SANG would partially protect the setting of the listed buildings/wall at Henry Lucas Hospital but the proposed housing to the north is too close. The harm to the pastoral setting of the Hospital is underrepresented in the Heritage Statement. Dwellings should not be permitted against the perimeter of Chapel Green House and screening (hedging) should be required on the northern boundary of Chapel Green House. *(Officer note: section 0 refers.)*

Will sheds and or green houses be allowed to be built on the allotments, opposite Lucas Hospital? *(Officer note: such structures are generally associated with allotment use and the application has been assessed on the basis that they would be expected. The number and distribution across the site would be assessed at the reserved matters stage and controlled by condition if necessary.)*

Loss of trees and hedgerows

A substantial number of trees and hedges will be lost, though some will be replaced; this will have an immediate, detrimental effect on ancient woodlands and the ability of local trees to absorb CO₂. *(Officer note: loss of trees and mitigation is considered in section 0. and also 0.)*

We have seen enough loss of trees and promises to replace them however this has not happened (e.g. the trees within Wokingham town centre which have been removed and not replaced). *(Officer note: each application must be considered on its own merits and in the case of the Elms Field site (application 153125), it was accepted that there were other benefits that outweighed the harm caused by the reduction in tree cover. Generally, where mature trees are lost the number of trees is expected to significantly exceed the number lost, in recognition of the time that it takes for newly planted trees to mature. Given the extensive areas of open space within the proposed development, there is no reason why this cannot be achieved in this case. Section 0 and condition 15 refer.)*

The large willow in the field adjacent to Chapel Green Farm Cottage No1 (estimated to be 90 years old) is not shown on the layout or Tree Reference Plan/to be removed/not TPO. *(Officer Note: the tree was missing from the original Tree Protection Plan. It is shown to be removed on the updated drawing although the tree report does not contain any information on its condition. See paragraph 67. Conditions 11 & 12 would ensure that its importance is properly assessed at the reserved matters stage.)*

Amenity of existing and future residents

The proximity of the proposed dwellings to existing dwellings in Chapel Green (particularly Chapel Green Cottage, which has extant planning permission for a single-storey side extension, due to be completed this year) would cause them to crowd the existing properties, noise disturbance, overlooking (from residential parcels and new footpaths within the SANG) and overshadowing (these concerns have been partially addressed by the reduction in the number of dwellings proposed but the treatment of the northern

boundary of Chapel Green House will need careful consideration – a hedge – at reserved matters stage). (*Officer note: section 0 and informative 12 refer.*)

Noise and light pollution from the new development and associated traffic would have a negative impact. No homes should be permitted east of Chapel Green House. (*Officer Note: the impact upon the residential amenity is considered in section 0 and noise in 0. The extent of noise and light pollution arising from the proposed development would be no greater than in any other residential area.*)

Loss of views. (*Officer Note: there is no right to a view and loss of one is not a reason to withhold planning permission.*)

The new dwellings would be cramped, overlooked and have limited privacy. (*Officer Note: sections 0 & 0 refer.*)

There is lack of green space within the development with SANG and play areas in remote, poorly overlooked locations on the edge of the development/in the floodplain. (*Officer note: sections 0 & 0 refer.*)

The northwest corner of parcel D, C1 and C2 are unsuitable for housing due to noise from the railway and SWDR. (*Officer Note: noise is considered in section 0.*)

Ecology

There is insufficient information relating to wildlife: the site supports a broad array of flora and fauna and acts as a link allowing wildlife to move between rural areas. The proposals would result in loss of wildlife – birds including herons, lapwings, kestrels, buzzards, sparrowhawks, kites, bats, foxes, deer, mature oak trees, wildflowers, fish, newts – and irreplaceable habitats which have not been adequately assessed (there is no bat survey and the majority of other surveys are desk based) and will not be adequately mitigated by provision of SANG and relocation techniques. (*Officer Note: section 0 refers.*)

Pollution

The reduction in green space combined with increased traffic/congestion (on Finchampstead Road in particular) would increase noise, reduce air quality in the area/Finchampstead Road (where air pollution is already in excess of government targets)/the town centre and cause light pollution. Health should be prioritised over housing delivery. The site is very close to “road side primary school” and increased traffic will endanger children. (*Officer Note: section 0 refers.*)

The reduction in traffic during the coronavirus crisis has resulted in improved air quality.

The proposed play area would be on land that is contaminated due to the proximity to a septic tank/use of the land for sewage dispersal for the last 120 years. (*Officer note: should any contamination have occurred; it would be addressed by condition 36.*)

Flooding

The Emmbrook has a history of regular flooding and much of the site (parcels C1 & C2 in particular) is within flood zones 2 and 3. Increased hard surfacing will reduce the floodplain, prevent infiltration and increase runoff leading to flooding downstream in Woosehill, Emmbrook, Matthews Green, Luckley Wood (where there are existing problems with runoff from the railway) and Wokingham town centre (where significant areas of are

already as risk of surface water flooding). The Flood Risk Assessment does not consider the effect on land outside the application site. *(Officer note: section 0 refers.)*

Consideration should be given to climate change; finished floor levels 30 cm above current flood levels may be inadequate. *(Officer note: section 0 refers.)*

The Emmbrook will change from a brook to a spate drainage ditch, overflowing during high rainfall and drying out, killing water dependant wildlife, during dry periods. *(Officer note: section 0 refers. The application site forms only a small part of the catchment for the Emmbrook and the proposals would not reduce the channel flow from upstream so are unlikely to cause the river to dry out. The hydrology of the brook has been considered in relation to drainage and ecology – sections 0 & 0 – and overall, subject to the recommended conditions, the proposals would be beneficial in terms of SuDS and aquatic wildlife.)*

Increased risk of flooding will increase insurance costs. *(Officer note: section 0 explains that the proposals would not increase the risk of flooding. Insurance costs are not a planning matter.)*

Transport

The application is premature as the application for the SWDR has not been determined and financial restrictions resulting from Covid19 are likely to delay delivery for many years. *(Officer Note: planning permission for the SWDR was granted in May 2021. That delivery may be delayed is not a reason to withhold planning permission for an otherwise acceptable proposal.)*

Finchampstead Road is an A road, already at capacity with severe congestion/delays throughout the day/evening and particularly at peak times (there are already long delays on local journeys – eg from Eastheath Avenue and Gipsy Lane - at peak time and it is difficult/dangerous to turn onto Finchampstead road from side roads, especially if turning right). The additional generated by the SWDR/SDL (and other developments) will join Finchampstead Road at Tesco exacerbating existing congestion and reducing road safety on Finchampstead Road and more widely. *(Officer note: the SWDR, including the connection to Finchampstead Road, was the subject of a separate planning application (192928), approved in May 2021. The assessment of the application included redistribution of traffic resulting from the provision of a new route, the impact on the existing network and any necessary mitigation. Section 0 considers the need for further off-site mitigation resulting from SDL traffic.)*

Pavements on Finchampstead Road (used by pupils walking to Evendons Primary and Luckley House) are uncomfortable for pedestrians due to their narrow width, overhanging vegetation, air quality and traffic, which is a deterrent to walking. In line with national planning policy pedestrians and cyclists should be prioritised but – despite having identified Finchampstead as being within the 5km cycling catchment of the site – the application does not take the opportunity to address cycling safety on Finchampstead Road and to increase the proportion of journeys that are undertaken by cycle. There is no scope for/proposals to widen Finchampstead Road or improve pedestrian and cycle provision, in particular at the two railway bridges, and there is only a limited bus service. *(Officer note: Pedestrian and cycle connectivity, including provision along the Finchampstead Road corridor is considered in section 0. Public transport is considered in section 0.)*

The proximity of the accesses to parcels C1 and C2 and the pedestrian crossing where Footpath 10 crosses the SWDR is potentially unsafe for pedestrians and likely to disrupt the flow of traffic on the SWDR. *(Officer Note: the SWDR including junctions and pedestrian crossings is the subject of a separate planning permission, 192928.)*

The SWDR should have a cycleway linking into central Wokingham and towards Bracknell. *(Officer Note: the SWDR planning permission incorporates provision for cyclists. On- and off-site improvements are to be secured through conditions and the S106. Sections 0 & 0 refer.)*

The TA indicates lower levels of traffic on Finchampstead Road in 2026 with development than without. *(Officer Note: this is because of the change in the distribution of traffic following opening of the SWDR. Section 0 refers.)*

Congestion at the junction of Nine Mile Ride and Heathlands Road has already been made worse by the TRL development. *(Officer note: mitigation at this junction is proposed. Section 0 refers.)*

The TA suggests there have only been seven minor accidents within the study area but this is unlikely to be a reliable figure because many accidents are not recorded (even if reported to the Police). *(Officer note: Person Injury Accidents are the main priority, especially when involving vulnerable users including pedestrians & cyclists. The underestimating referred to relates to damage only accidents which are often not reported or added to the road accident database.)*

The proposals would create a rat-run through Montague Park. *(Officer Note: William Heelas Way was always intended to be the first section of the SWDR as explained in section 0.)*

The proposed development has a single access point and would effectively be a cul-de-sac, with limited access to the local network, which is contrary to guidance that emphasises the need for permeability. *(Officer note: section 0 refers.)*

Access to Chapel Green should remain as existing but there should be no vehicular access to the proposed development from Luckley Road: it would increase the volume of traffic using the narrow railway bridge and the junction of Luckley Road with Finchampstead Road. An additional junction on the SWDR would be preferable. *(Officer Note: section 0 refers.)*

The access to Chapel Green is a private road and residents pay for its upkeep. If it is proposed to use this route to access the allotment car park residents would need to be compensated for the additional use. *(Officer note: access to the allotment car park would not be fixed until the reserved matters stage although it is anticipated it would be through parcel D as currently illustrated. Access over private land, outside the application site boundary would require a separate planning application as well as agreement with the landowners outside the planning process.)*

Luckley Road, south of the railway bridge is already used by walkers for informal parking and this is likely to increase due to the proposed emergency/pedestrian/cycle route and the proximity to the playground, allotments and SANG resulting in congestion, potentially obstructing the emergency access, and damage to verges. *(Officer Note: this need for measures to control informal parking would be considered in conjunction with the detailed*

design of the emergency, pedestrian and cycle connection to Luckley Road. See paragraphs 190 & 210.)

The proposals should include an alternative access to Knoll Farm, to avoid the nuisance to local residents that would be caused by reintroduction of whistle boards at the private crossing, and the developer should be required to work with Network Rail and the owners of The Knoll Farm to deliver it prior to completion as a condition. *(Officer note: the proposals would not prejudice alternative access arrangements for Knoll Farm which would be secured by the S106. Section 0 v) refers.)*

There should be a hierarchy of speed limits within the development: 30 for the major roads; preferably 20 mph and certainly no more than 30 mph for the minor roads; and no more than 20 on the shared surface roads. *(Officer note: while speed limits are not directly a planning matter, sections 0 & 0 establish that a hierarchy of street typologies and design speeds is proposed, which may be reflected in speed limits.)*

Most households will have more than one vehicle/an average of two cars per household is unrealistic and pavement parking will be inevitable. *(Officer Note: section 0 refers.)*

Concerns have been raised about the amount of parking for the allotments. *(Officer Note: this would be considered as part of assessment of the reserved matters and conditions for the allotment site.)*

Will construction traffic be banned from the centre of Wokingham, Finchampstead Road and Nine Mile Ride? *(Officer Note: legitimate use of the adopted highway cannot be prevented but the CEMP secured by condition 33 will manage construction traffic routing.)*

Public Rights of Way (PRoW)

The existing PRoW are heavily used by (dog) walkers, runners and cyclists and should be maintained with safe, appropriate crossings over the (30mph) SWDR, ideally accessible pedestrian bridges. *(Officer Note: section 0 explains proposals for the PRoW network. Appropriate pedestrian crossing facilities over the SWDR, were approved as part of the SWDR application, 192928.)*

Footpaths 24 and 25 would be closed or diverted during construction and upon completion would use parts of the housing development road network, which would have a negative impact on their recreational value. There is concern about the safety of shared surfaces on these, well used routes: pedestrians should be protected from vehicles. To mitigate, the existing footpaths should be upgraded to Greenways and an additional footpath should be provided east of the development, Chapel Green and Lucas Hospital, joining the on the eastern boundaries to join the Ludgrove School access road (a private road and permitted right of way which is already well used and use is likely to increase). *(Officer note: see section 0.)*

Temporary closure and diversions of footpaths during construction should be minimised and coordinated to ensure access is maintained. *(Officer note: the phasing conditions and CEMP – conditions 3 & 33 – will consider this and separate approval is required.)*

Improvements to the existing PRoW are welcomed and should balance widening of the paths with maintaining a countryside character, good connectivity with the rest of the network including Greenways and separation of pedestrian and fast cycle traffic, protect from flooding. *(Officer note: section 0 conditions 15, 38, 39 & 40 refer.)*

Traffic noise from the SWDR will detract from the attractive walk along Luckley Road. *(Officer note: noise generated by use of the SWDR was considered as part of the assessment of that application. Section 0 also refers and noise is a factor in assessing the suitability of a site as SANG.)*

There are contradictions in the DAS which indicates that existing public rights of way will be respected “as far as possible” and that there is an opportunity to extend the footpath network. *(Officer note: sections 0 & 0 refer.)*

The SANG paths east of Chapel Green should be omitted as the additional use would disturb residents and adequate connections between existing PRoW can be provided without them. *(Officer note: sections 0 & 0 refer.)*

Other

The medieval system of surface water control in the has been destroyed over the past decades and new watercourses have developed. Due to the geology and wet, boggy conditions in the north-eastern part of the site/parcel C1, construction of foundations (for three-storey structures and less demanding loads) could cause structural stability and drainage problems. *(Officer note: Drainage is considered in section 0. The proposed development is located outside areas liable to flooding and there is nothing to suggest land instability that could not be mitigated through appropriate design.)*

Properties in Chapel Green have a septic tank on the development site and services run across the fields *(Officer Note: this point has been considered under representations Specific to the revised plans received in September 2021. Condition 30 refers.)*

Residents of Chapel Green paid a premium for their properties due to its private setting *(Officer Note: the impact on residential amenity has been considered in section 0 but property prices are not a planning issue.)*

The applicant is a professional firm with no local connection. They have tried to use the system to obtain planning permission, despite the site not being allocated for development and, despite having failed on previous planning applications went ahead with the SANG. *(Officer note: each application must be assessed on its own planning merits and these comments appear to relate to a different application site. It is to be expected that major, strategic sites will be brought forward by professional developers and their advisors. This hybrid application (for housing, SANG and other infrastructure on an allocated site) submitted in 2019 has been the subject of ongoing discussion and revised plans have been submitted. As the planning history shows there have been no previous applications for SANG alone on this site.)*

While it is not a planning matter, there are restrictive covenants preventing residential development of the land which will become enforceable once residential development starts. *(Officer note: this has been shared with the applicant.)*

The proposals are lacking in detail and there are inconsistencies between the Illustrative landscape masterplan. *(Officer note: outline proposals will by definition lack detail. The level of information available is sufficient at this stage and further detail – including resolution of non-fundamental inconsistencies - will be established through reserved matters and other conditions.)*

Affordable housing

No land is allocated for affordable housing/no affordable housing is proposed. *(Officer note: this is not the case. Section 0 refers.)*

Infrastructure

There is support for the approach of growth being linked to delivery of supporting infrastructure as set out in the council's 'Homes for the Future' document but this proposal is not consistent with that approach. *(Officer Note: 'homes for the future' formed part of the consultation on the emerging local plan for the period up to 2036. Nevertheless, the principle of comprehensively planned development, with the infrastructure required to support it is already established in the adopted Development Plan and the current proposal relates to a Strategic Development Location, allocated in the Core Strategy. See sections 0 0, 0, 0, 0, 0 & 0.)*

Local services such as such as schools (Evedons School has already been extended), NHS/medical services (doctors and dentists), care provision, social services and libraries are already oversubscribed and no additional provision is proposed to meet the needs of incoming population. *(Officer note: sections 0 & 0 refer.)*

There are no spaces in any of the secondary schools the site is in the catchment area for. *(Officer note: paragraphs 17 & 18 refer.)*

How will the development 'plug in' to utility networks which are already overloaded and frequently fail, resulting in emergency repairs? *(Officer note: utility providers are consulted and can request capacity improvements where they are required as a consequence of the proposed development.)*

Tesco is small and couldn't cope with the additional demand *(Officer Note: Core Strategy policy CP14 sought to achieve the "Growth and Renaissance of Wokingham Town Centre", partly to meet increased demand from new development in the SDLs. Pursuant to this aim the council has brought forward a number of developments in the town centre including a new foodstore at Elms Field. In addition local provision is proposed within the SDL. Section 0 refers.)*

The application is 'freeloading' on the publicly funded SWDR. *(Officer Note: the council is funding the SWDR from Community Infrastructure Levy (CIL) receipts; payments made by all developments, including those within the South Wokingham SDL for the purpose of delivering supporting infrastructure.)*

Sustainable development

All new housing should be carbon neutral/to Passive House Standards with predominantly renewable energy in order to reach the goal of a net zero economy. *(Officer note: section 0 assesses the proposals against current planning policy.)*

Consultation

The applicants should hold an exhibition/public meeting along with the local authority, indicating how the proposal fits into the overall context of development south of the railway. There has been no detailed communication from WBC in respect of the application. *(Officer note: the applicant's Statement of Community Involvement sets out*

the engagement that has taken place, including an exhibition and attendance at the South Wokingham Forum, in line with the council's Statement of Community Involvement (SCI). Given the scale of development proposed at South Wokingham, the Local Planning Authority consultation on the application has been significantly wider than necessary to fulfil the requirements of the SCI (letters were sent to approximately 4,000 households) and the full suite of application documents is available on the website).

Wokingham Town Council

Full comments are attached. In summary:

There are concerns about flooding (*Officer note: see section 0*), increased traffic on local roads (*Officer note: see sections 0 & 0*), the impact on wildlife (*Officer note: see section 10*), removal of trees (which should be replaced with mature trees) (*Officer note: see section 5.*), lack of infrastructure to support the development (*Officer note: see sections 2, 0, 0 & 0*) and the impact on the Lucas Hospital (*Officer note: see section 0*). (Relevant Core Strategy policies are referenced).

The distributor road should be complete before development. (*Officer note: see section 0*)

Support provision of 35% affordable housing (*Officer note: section 0*).

Additional comments made following the revised plans consultations were:

The Greenway proposed through this development does not separate cyclists from pedestrians. (*Officer note: see section 0*).

The planned bicycle storage is by the bins and is too far away from the buildings. (*Officer note: see section 0 & 0*).

There are formal amenity areas but there should be an area to allow children to play and kick a football around & allocation of public exercise equipment. (*Officer note: see sections 0, 0 & 0.*)

Removal of a 400m hedge. (*Officer note: see sections 0 & 0.*)

Lack of infrastructure, in particular GP surgeries. (*Officer note: see section 0*)

Wooden stakes should be introduced to discourage informal parking along Luckley Road, which is likely to be increased by SANG visitors. (*Officer Note: this need for any such measures would be considered in conjunction with the detailed design of the emergency, pedestrian and cycle connection to Luckley Road. See paragraphs 189 & 209.*)

Wokingham Without Parish Council

Full comments of are attached. In summary:

The site is allocated for housing in the Core Strategy. Development is acceptable in principle but should be assessed against the adopted policy and the adopted Supplementary Development Document for the South Wokingham SDL.

Residents' objections on grounds of increased traffic, noise and pollution should be taken into account when determining the application. (*Officer note: these issues are considered in sections 0 & 0-0.*)

Concerns have been raised with respect to the location and infrastructure for the additional new housing required to compensate for the reduction on this site. (*Officer note: the*

reduction from a maximum of 190 to 171 was necessary to achieve a satisfactory form of development on the site and would not result in a material shortfall in the amount of development in the SDL; see paragraph 9. Any application for development - within or outside the SDL – would need to demonstrate compliance with Development Plan policies regarding integration with the surroundings and mitigation of infrastructure impacts.)

Crowthorne Parish Council (adjoining parish)

Full comments are attached. In summary:

The pandemic crisis has shown the importance of agriculture and the loss of agricultural land should be reconsidered. *(Officer note: the application must be assessed against the existing policy framework – in particular the Development Plan - as set out section 0 of this report.)*

The application is reducing the strategic gap between Wokingham and Crowthorne. Green belt land should not be built on and the effect on Wokingham Borough's Green Belt strategy should be clarified. *Officer Note: the relevant policy designations are set out in section 0. For avoidance of doubt the current development plan does not contain any strategy gap polices (which would be contrary to national planning guidance) and the none of the site is designated as Green Belt.)*

If the council has achieved its housing land supply this application would be redundant. *(Officer Note: section 0 refers.)*

How can WBC uphold their pledge to make Wokingham Borough carbon neutral by 2030 if applications such as this are approved? *(Officer note: the application has been assessed against current Development Plan policy, including consideration of measures to reducing the need to travel and facilitate sustainable travel, sustainable design and construction and landscaping. See sections 0, 0 & 0.)*

Finchampstead Parish Council (adjoining parish)

Full comments are attached. In summary:

Accept the principle of development but the distributor road and bridge widening should be completed before development commences. *(Officer note: section 0 refers.)*

Flooding, the impact on wildlife, removal of trees and the impact on the Lucas Hospital are of concern. *(Officer note: sections 0, 0, 10 & 13 refer.)*

Support the allocation of 35% affordable housing and the comments made by Wokingham Town Council. *(Officer note: section 0 refers.)*

The revisions do not alter the council's previous comments, particularly with regard to additional traffic going under the narrow bridge up to Molly Millars Lane. *(Officer note: section 0 refers.)*

Ward Members and adjoining ward members

Comments from Cllrs Maria Gee (Wescott) and Sarah Kerr (Ewendons) are available in full on the website. In summary:

It is acknowledged that this is an allocated site but members question whether demand for housing will remain at pre-Covid19 levels. The need for more housing and its location should be reconsidered, given the number of unplanned office-to-residential conversions and that the crisis has demonstrated how essential open countryside is for health and wellbeing. (*Officer Note: the purpose of the ongoing Local Plan Update is to review the need for housing and establish a strategy for the next plan period. However, the current application must be assessed against the existing policy framework – in particular the Development Plan - as set out section 0 of this report. The council's Five-Year Housing Land Supply includes an allowance for windfall sites such as office conversions. See section 0*)

Provision of 35% of affordable homes is welcome and should not be reduced following determination. (*Officer Note: currently there is no suggestion that 35% affordable housing would not be delivered but any viability case that were put forward in future it would have to be assessed on its merits. The appropriate procedure would depend on the stage at which the matter was raised but it is likely that a report to Planning Committee would be required.*)

The reduction in agricultural land combined with the increase in traffic will increase air pollution/ have an adverse effect on carbon neutrality. (*Officer Note: air quality is considered in section 0.*)

The site is next to an Air Quality Management Area (AQMA) and one of the most heavily congested roads in the borough. The additional traffic from the development will increase pollution. 2018 air quality data for the town centre should not be relied on because a large section of the town centre AQMA was closed to vehicular traffic due to the regeneration. *PM_{2.5}'s* are the worst pollutant, harmful at low concentrations and levels are already above WHO safe limits; they should be considered in the Air Quality report. (*Officer note: the Environmental Health Officer advises that, while there was variation in specific locations, overall 2018 and 2019 nitrogen dioxide levels in the Wokingham Town Centre AQMA did not vary significantly different and they are satisfied that the applicant's assessment is robust. The assessment also considers available data on PM₁₀ and PM_{2.5}. Concentrations of PM₁₀ are below air quality objectives and, while concentrations of PM_{2.5} are slightly above WHO recommended levels – the UK government has yet to set limits - road transport and domestic activities make only a relatively small contribution to overall levels, which fell by 11% between 2010 and 2019.*)

Given the declaration of a climate emergency and the aim of carbon neutrality there should be an assessment of additional carbon associated with the development and a plan for carbon offsetting, including replacement of felled trees, and carbon neutral dwellings. (*Officer Note: the application has been assessed against current Development Plan policy, including consideration of measures to reducing the need to travel and facilitate sustainable travel, sustainable design and construction and landscaping. See sections 5, 12 & 15.*)

Congestion is already an issue and the application should not be approved before the southern distribution road and western gateway are completed due to pressure on local roads, in particular, Finchampstead Road. (*Officer note: section 0 refers.*) There should be a focus on alternative modes of transport with specific mention of the need to secure improvements to the public right of way network and Finchampstead Road, which has poor walking and cycling infrastructure. (*Officer Note: sections 0, 0 & 0 refer.*) Edneys Hill is

dangerous but listed in the TA as a quiet link for cyclists. (*Officer Note: this is derived from the council's own My Journey Wokingham Area Cycle Map.*)

No secondary or emergency access is proposed for a parcel of over 100 dwellings. (*Officer Note: the revised plans address this issue. See section 0*)

No housing should be built on land that is at risk of flooding and development should not increase flood risk. (*Officer Note: no housing is proposed in the floodplain. See section 0.*)

The impact on the trees and wildlife on the site are not adequately explored (*Officer Note: these issues are considered in sections 0 & 0.*)

The dwellings in the northeast of the application site are cut off from those in the southwest of the site and are premature unless the area to the east is also developed. (*Officer Note: the Planning Committee resolved to approve applications for development of the land to the east (applications 190914 & 191068 in May 2021, subject to completion of a S106 agreement which is being progressed.)*)

There is a lack of provision for secondary education and doctors' surgeries; infrastructure such as schools, hospitals and leisure facilities should be completed to coincide with increased housing numbers. (*Officer Note: the infrastructure required to support the development is considered in section 0 and conditions/the S106 would establish appropriate triggers for delivery.*)

Loddon Valley Ramblers

The application refers to opportunities to extend the existing footpath network and delivery of measures to provide good accessibility by foot and cycle but does not address this opportunity adequately. Nor does it reflect the PRoW proposals within the SWDR application (especially those affecting the western end of Wokingham Within Footpath 25) or the Council's Greenway plans, which aim to connect Wokingham Town Centre with the communities to the south. (*Officer note: The proposals have been updated since these comments were submitted. Sections 0 & 0 refer.*)

There are concerns about the proximity of development to parts of Footpaths 10, 24 and 25, which should be separated from houses and trafficked roads, and lined by dense hedgerows on either side. The whole length of Footpath 25 should be of Greenway standard and the surface of Footpath 10 should also be improved. (*Officer note: sections 0, 0 and conditions 15, 38, 39 & 40 refer. Given the changing character of the area the paths pass through, and consequently their function, the approach described in sections 0 & 0 is considered preferable to segregating the paths with dense hedges which would result in green 'tunnels' through the built-up areas, reducing opportunities for natural surveillance, which should be embraced in the interests of the safety of users.*)

The proposed new path through the SANG, between the SWDR and Wokingham Without Footpath 10 would be useful for walkers, wishing to avoid walking along the SWDR. Public Right of Way status for this path would be valuable if the current permitted east-west route along Ludgrove Road were ever to be closed. (*Officer note: sections 0, 0 and condition 15g) refer. The S106 would secure the transfer of the SANG to the council in perpetuity so there is no need to dedicate the paths with the SANG as PRoW to secure public access.*)

PLANNING POLICY		
National Policy	NPPF	National Planning Policy Framework
South East Area Plan saved policy	NRM6	Southeast Plan Policy NRM6 <i>Thames Basin Heaths Special Protection Area</i>
Adopted Core Strategy DPD (2010)	CP1	Sustainable Development
	CP2	Inclusive Communities
	CP3	General Principles for Development
	CP4	Infrastructure Requirements
	CP5	Housing mix, density and affordability
	CP6	Managing Travel Demand
	CP7	Biodiversity
	CP8	Thames Basin Heaths Special Protection Area
	CP9	Scale and Location of Development Proposals
	CP10	Improvements to the Strategic Transport Network
	CP11	Proposals outside development limits (including countryside)
	CP17	Housing Delivery
	CP21	South Wokingham Strategic Development Location
Adopted Managing Development Delivery Local Plan (2014)	CC01	Presumption in Favour of Sustainable Development
	CC02	Development Limits
	CC03	Green Infrastructure, Trees and Landscaping
	CC04	Sustainable Design and Construction
	CC05	Renewable energy and decentralised energy networks
	CC06	Noise
	CC07	Parking
	CC08	Safeguarding alignments of the Strategic Transport Network & Road Infrastructure
	CC09	Development and Flood Risk (from all sources)
	CC10	Sustainable Drainage
	TB05	Housing Mix
	TB07	Internal Space standards

	TB08	Open Space, sport and recreational facilities standards for residential development
	TB12	Employment Skills Plan
	TB21	Landscape Character
	TB23	Biodiversity and Development
	TB24	Designated Heritage Assets
	TB25	Archaeology
Supplementary Planning Documents (SPD)	BDG	Borough Design Guide (2012)
		South Wokingham Strategic Development Location Supplementary Planning Document (2011)
		Infrastructure Delivery and Contributions Supplementary Planning Document (2011)
		Affordable Housing Supplementary Planning Document (2013)
		Sustainable Design and Construction Supplementary Planning Document (2010) & Companion Guide (2010)
Other		DCLG – National Internal Space Standards
		Crowthorne Village Design Statement

PLANNING ISSUES

The principle of development

1. The National Planning Policy Framework establishes an underlying presumption in favour of sustainable development which is carried through to the local Development Plan: applications that accord with the Development Plan for the Borough will be approved, unless material considerations indicate otherwise (Managing Development Delivery Local Plan (MDDLDP) Policy CC01 *Presumption in Favour of Sustainable Development*).
2. The Wokingham Borough Core Strategy establishes the spatial vision for the Borough for the period 2006-2026, including a requirement to provide at least 13,487 new dwellings, with associated infrastructure (Core Strategy policy CP17 *Housing Delivery*). The majority of this new residential development is to be in four Strategic Development Locations (SDLs), of which South Wokingham is one. Core Strategy policy CP21 *South Wokingham Strategic Development Location* anticipates a comprehensively planned, phased urban extension of around 2,500 dwellings and associated infrastructure on 85 hectares of land within the South Wokingham SDL (Appendix A7.50).
3. This approach is consistent with Core Strategy Policy CP9 *Scale and Location of Development Proposals* which identifies Wokingham as a Major Development Location – one which offers a good range of facilities and services, accessible by a choice of modes and capable of accommodating major development - and also

with paragraph 73 of the NPPF which advises that *'The supply of large numbers of new homes can often be best achieved through planning for larger scale development, such as new settlements or significant extensions to existing villages and towns, provided they are well located and designed, and supported by the necessary infrastructure and facilities (including a genuine choice of transport modes).'*

4. The Core Strategy requirements are amplified by the *South Wokingham SDL Supplementary Planning Document* (the South Wokingham SPD) and the infrastructure SPD *Infrastructure Delivery and Contributions Supplementary Planning Document* (2011) (the Infrastructure SPD). In line with these documents, MDDL policy CC02 *Development Limits* establishes the extent of the settlement of Wokingham, including the built-up area of the SDL. The land within the SDL boundary but outside development limits remains designated Countryside (Core Strategy policy CP11 *Proposals outside development limits (including countryside)*) and some parts are allocated as SANG (MDDL policy SAL05 *Delivery of avoidance measures for Thames Basin Heaths Special Protection Area* and section 0 of this report). However, recognising that masterplanning of the SDL was yet to take place, policy policies CC02 and SAL05 allow flexibility for alternative layouts, where they are accompanied by a deliverable SDL wide masterplan and Infrastructure Delivery Plan (IDP).
5. Outline planning permission for the first phase of development (roughly a quarter of the development in the SDL) was approved in 2012 (planning permission O/2010/1712) and the development of 636 new homes and associated infrastructure at Montague Park (formerly Buckhurst Farm) is now substantially complete.
6. On 18 May 2021 the planning committee resolved to grant planning permission for Phase 2 of development in the SDL: a hybrid application comprising an outline proposal for up to 1,434 new dwellings and a full proposal for a 24.71 hectare Suitable Alternative Natural Greenspace (SANG) (application 191068) and an outline application for up to 215 dwellings (application 190914) were approved subject to completion of a S106 agreement to secure supporting infrastructure; and a full application for SANG (application 190900) was approved on 24 May 2021. These applications represent approximately 67% of the 2,500 new homes planned in the SDL. The committee also resolved to grant planning permission for the South Wokingham Distributor Road (SWDR) between Finchampstead Road and Waterloo Road (application 192928) associated highway works in the Finchampstead Road corridor (application 203535), also approved on 24 May 2021.
7. The current application is hybrid application for up to 171 dwellings plus SANG (application 192325). It is final phase of the development in the SDL and would deliver roughly 7% of new homes planned.
8. The SPD Framework Plans (Figures 3.1 & 4.1) identify broad locations for built development and open space within the SDL, showing extensive areas of open space on the eastern and western sides connected by a green-blue corridor along the river valley. The current proposals, which concentrate housing on the northern part of the site, adjacent to the SWDR, with include public open space to the south are broadly consistent with the approach anticipated by the SPD. This is considered in more detail in sections 0, 0 & 0.

9. Together, with the development at Montague Park and the five applications mentioned in paragraph 6, the current application would deliver a compressively planned development of up to 2,475 new dwellings (as with Montague Park the outline planning permission establishes a maximum and the number of dwellings may fall through detailed design at the reserved matters stage) together with the required infrastructure (referred to throughout the report and summarised in section 0). This is consistent with Core Strategy policies CP17 and CP21 which require delivery of around 2,500 dwellings; the proposals are in line with the spatial strategy established by the Core Strategy and acceptable in principle.

Housing land supply

10. The NPPF (paragraph 73) requires Local planning authorities to maintain a supply of specific, deliverable sites sufficient to provide a minimum of five years' worth of housing. The council's latest published Five-Year Housing Land Supply Statement (31 March 2021) demonstrates a 5.10-year supply against the Local Housing Need of 768 dwellings plus a 5% buffer, without reliance on development in the South Wokingham SDL south of the railway. Although it does not anticipate any completions here until after 2025/2026, the application relates to an allocated site and would make an essential contribution towards the supply going forward.

Community Infrastructure

11. Core Strategy policy CP21 *South Wokingham Strategic Development Location*, Appendix A7.53 and the supporting SPDs amplify the requirements of policy CP4 *Infrastructure Requirements*, setting out a range of education and community facilities that should be delivered within the SDL.

Neighbourhood centre: local retail and community facilities

12. Development Plan policy is supportive of mixed-use development that provides for the shopping needs of local residents and anticipates provision of new or enhanced local centres (consistent with the retail hierarchy) within the SDLs, in order to achieve sustainable patterns of development (Core Strategy policies CP3 *General Principles for Development*, CP13 *Town Centres and Shopping* with paragraph 4.65 of the supporting text and MDDL Policy TB15 *Major Town, and Small Town/District Centre Development*).
13. Accordingly, Core Strategy Policy CP21 (plus Appendix 7, paragraphs A7.42, A7.49, A7.52 & A7.53), the Infrastructure SPD and the South Wokingham SPD (Design Principles 2a and 4a in particular) require a sustainable, mixed use development incorporating appropriate retail facilities and social infrastructure, including two new schools. Two neighbourhood centres (of up to 1.5 hectares each) should be provided as a focus for community activity within the south Wokingham SDL: a smaller centre to the north of the railway (or alternatively expansion of the Rances Lane parade) and a larger one (incorporating a community centre to serve the whole SDL) to the south.
14. The first neighbourhood centre is being delivered north of the railway at Montague Park and a second, southern neighbourhood centre – including land for a community facility and school – is to be located on the south side of the SWDR, just to the west of Easthampstead Road, within phase 2b.

Education

15. The social infrastructure identified by Core Strategy policy CP21 *South Wokingham Strategic Development Location* and Appendix 7 (paragraphs A7.42 and A7.49) includes two new, two-form-entry primary schools at accessible locations within the development.
16. The first school has been provided within the first phase of the SDL at Montague Park (the Floreat Montague Park Primary School) and suitable site for a primary school is to be secured within phase 2b, to be delivered by the council from the Community Infrastructure Levy (CIL). Thus, collectively the applications for the SDL would deliver two, new, two-form entry primary schools in line with planning policy. The IDP would ensure delivery of the school site is appropriately phased in relation to the proposed development.
17. Secondary education requirements would be met through CIL and there is no requirement for land for a new secondary school within the development. Thus, planning policy requirements would be met.
18. The need for additional secondary school capacity has been raised in representations. While not directly relevant to the determination of this application, the People and Place Strategy and Commissioning Team acknowledge that the number of children admitted to the borough's secondary schools is rising: year 7 numbers are likely to peak before 2025, declining thereafter as the lower roll numbers seen in younger primary school age groups work their way into secondary schools. It is anticipated that additional secondary school capacity (to 2021 capacity) will be required over the next decade. Where and how this will be provided is (in November 2021) the subject of ongoing discussions with schools.

Community facilities including libraries and indoor sport

19. Core Strategy policies CP2 *Inclusive Communities* and CP3 *General Principles for Development* seek to secure a network of community facilities to support sustainable and inclusive communities and a multi-use community hall is among the on-site infrastructure required to support development in the South Wokingham SDL (Appendix 7, paragraph A7.53 and the Infrastructure SPD). Managing Development Delivery Local Plan policy TB08 *Open Space, sport and recreational facilities standards for residential development* establishes standards for indoor sports provision.
20. The outline planning permission for Montague Park pre-dated CIL but the S106 secured proportionate contributions towards provision of a community facility (including indoor sports provision) within the larger neighbourhood centre south of the railway, swimming pools and libraries. Because it would be some years before the development south of the railway would come forward, the planning permission also secured an additional 200m² community facility within a mixed use building in the Montague Park neighbourhood centre (this was in addition to the policy requirement but was offered to address the lag in provision).
21. Under the CIL regime, community and social infrastructure (community centres, libraries and indoor sports facilities) are to be provided by the council from CIL receipts, with the exception of the land where facilities are required within the SDLs. Accordingly, the S106 for phase 2b will secure a 500m² site for a two-storey community centre with ancillary outside space, within the proposed

neighbourhood centre (based on the standard of 0.117m²/capita established by the council's Shaping Our New Communities (SONC) (January 2015). The site would be of sufficient size to deliver a policy compliant facility and, a well-designed building within the proposed parameters could facilitate future expansion if required. Delivery would be funded through a combination of S106 contributions from Montague Park and CIL.

22. In addition to the on-site provision, the enhanced indoor sport, swimming and library facilities being delivered by the council through the redevelopment of the Carnival Pool site in the town centre (funded in part from S106 and CIL receipts) are intended – in part - to meet the needs of residents of the SDLs.

Primary Health Care

23. Core Strategy Appendix 7 paragraph A7.49 e) anticipates provision of new primary health care facilities within the SDL, which would most appropriately have been located within the larger neighbourhood centre, within phase 2b. However, circumstances have changed since adoption in 2010 and – for reasons explored in section 2.4 of the report on phase 2b - the NHS Wokingham Clinical Commissioning Group are no longer seeking primary health provision within the SDL. As discussed in section 0, WBC will be delivering the community building within the neighbourhood centre and there could be an opportunity to provide supplementary medical facilities within that building if there were a need at the time of development.

Fire station site

24. Core Strategy Appendix 7 paragraph A7.49 anticipated a site for a relocated fire station would be provided at South Wokingham. However, shortly before adoption of the Core Strategy in January 2010, planning permission was granted for extension of the existing fire station in Denton Road and alterations to the access arrangements (application F/2009/2429). Hence, the requirement for a fire station site has fallen away.

Housing density, mix and tenure

25. Core Strategy policy CP5 *Housing mix, density and affordability* requires a mix and balance of densities, dwelling types, tenures and sizes.

Making efficient use of land

26. The density of development should be design led (Borough Design Guide principle R10), incorporating a mix of densities (Core Strategy policy CP5 *Housing mix, density and affordability*) and utilising the potential of the site to incorporate complementary uses (Core Strategy policy CP3 *General Principles for Development*).
27. Core Strategy Appendix 7, paragraph A7.50 and the South Wokingham SPD anticipate an average density of 30–35 dwellings per hectare (dph) within the South Wokingham SDL but with significant variation across different character areas: a relatively low density of 25-30 dph on the “rural interface”, in particular on the southern edge of the development; 30-40 dph in “general residential” areas; and 40-45dph in “urban residential” areas, such as around the neighbourhood centres and along the SWDR.

28. The residential parcels have a combined area of 4.35 hectares result in an average net density of around 39 dph, 41 dph based on the net developable area of 4.17 hectares. For comparison, Montague Park achieved an average density of 34 dph, phase 2a 47 dph and phase 2b 38 dph, with variation between the different character areas. Although the DAS does not establish a density range for each character area it is apparent that density would vary considerably between the parcels. Based on the Provisional layout the densities would range from 32 dph in parcel C1 (which would be in the rural interface character area except for the SWDR frontage which would be urban residential) to 36 dph in parcel D (which is proposed to contain all three character areas) and 116 dph in parcel C2 (entirely urban residential). The latter is not particularly useful as an indication of character because, unlike the other parcels, there is a very limited road network within the parcel which makes the dph appear unusually high. The Provisional Layout shows two apartment blocks in parcel C2: a two storey building containing eight flats and a three-storey building containing 21 flats. This scale of development is comparable to many of the buildings fronting William Heelas Way in Montague Park and, subject to good design, would be appropriate to the urban residential character area fronting the SWDR.
29. While the average density is higher than anticipated it is similar to phase 2, consistent with the NPPF aim of optimising the use of land and – based on the assessment of character in section 0 of this appraisal - acceptable in principle. The contribution that variation in densities would make to establishing different character areas would be refined by condition 4 and through reserved matters.

Housing mix

30. Core Strategy Policy CP5 *Housing mix, density and affordability* and MDDL Policy TB05 *Housing Mix*, reinforced by SPD Design Principle 2b, seek to provide a mix and balance of dwelling types and sizes, with a predominance of houses with private gardens (Core Strategy Appendix 7 paragraph A7.51), having regard to both the underlying character of the area and current and projected needs of households.
31. At the time of first submission, the most up-to-date information on market housing mix was the *Berkshire (including South Bucks) Strategic Housing Market Assessment* (February 2016) (SHMA). This has since been updated by the *Wokingham Borough Local Housing Needs Assessment 2019 Report of Findings* (January 2020). The more recent advice places less emphasis on two bedroom dwellings and more on larger, three and four bedroom properties than the earlier guidance.
32. While the precise mix would not be established until the reserved matters stage, the Planning Statement indicates a variety of houses and flats, ranging from one to four-bedrooms and the Provisional Layout is based on the indicative mix set out in the Transport Assessment Addendum (July 2021), which is summarised in the table below.

Dwelling size	1 Bed units	2 Bed units	3 Bed units	4+ Bed units
Number proposed ⁹	20	35	33	24
% proposed	18%	31%	29%	21%

33. The indicative mix places more emphasis on smaller market homes than the guidance suggests but the precise mix would not be established until the reserved matters stage and would be assessed against current guidance at that time, balanced with other planning considerations. Condition 3 would ensure phased delivery of an appropriate overall mix and distribution across the site.

Affordable & specialist housing

Amount of affordable housing

34. Core Strategy policy CP5 *Housing mix, density and affordability*, reinforced by SPD Design Principle 2b and the Infrastructure and Contributions SPD, requires residential development to provide a mix of tenures, including a proportion of affordable housing. The proportion depends on the size of the development, location and whether the land was previously developed: for developments of five or more dwellings (net) or on sites of 0.16 hectares or larger within SDLs the requirement is for 35% affordable homes.
35. For the maximum of 171 dwellings proposed in Phase 3, this equates to up to 60 units. This proportion would be secured by a S106 agreement.

Affordable housing size and tenure mix

36. MDDL P policy TB05 *Housing Mix* requires an appropriate mix of affordable dwelling types and sizes, assessed on a site by site basis and reflecting the Councils Housing Strategy and Affordable Housing SPD (2013). The MDDL P and Affordable Housing SPD suggest a guide size mix, to be considered in conjunction with the latest information from the Housing Register.
37. The applicant's affordable housing statement also referred to advice in the SHMA, although this has since been updated by the LHMA, and to site constraints which are likely to result in a relatively high proportion of one-bedroom properties.
38. Taking into account current requirements and the applicant's points, the Housing Policy Officer has agreed the following mix: to compensate for the number of one-bedroom properties and achieve a better fit with local housing needs, the agreed mix includes a higher than usual proportion of four-bedroom houses, of which five would be for social rent and four for shared ownership, or six for social rent and three for shared ownership if First Homes applies, as is likely (four-bedroom houses are unlikely to be deliverable as First Homes).

⁹ Based on the mix in TA Addendum (July 2021) Table 5.1

Affordable Housing size mix		Proposed	
		%	No
One-bedroom flat		30%	
two-bedroom flat	predominantly houses	3%	35%
two-bedroom house		18%	
three-bedroom house		20%	
Four + bedroom house		15%	
Phase 3 total		60	

39. The Affordable Housing SPD also provides guidance on the tenure of on-site affordable housing, to be assessed on a site by site basis but generally 70% social rented and 30% shared ownership. However, a written Ministerial Statement on 24 May 2021 introduced First Homes, a kind of discounted market sale housing, considered to meet the definition of ‘affordable housing’ for planning purposes. First Homes are the government’s preferred discounted market tenure and should account for at least 25% of all affordable housing units. While there is a transition period for applications that have been subject to significant pre-application engagement and are determined before 28 March 2022, it is unlikely that the S106 agreement will be completed and the decision issued before that date so First Homes will almost certainly apply in this case. Accordingly 25%, equating to 15 of the affordable homes should be First Homes, with the remainder split between 70% (42 dwellings) social rent and 5% (three dwellings) for shared ownership. In line with the SPD, the shared ownership units would be delivered with a minimum equity share of 35% on the initial sale and rent capped on the unsold equity at 1.5% per annum.
40. All affordable housing units should be built in accordance with the standards, requirements and latest guidance issued by Homes England and/or the Regulator of Social Housing, meet the national space standards.
41. The affordable dwellings should be transferred to the Council’s Local Housing Company (Wokingham Housing Limited) or one of the Council’s preferred Registered Provider (RP) partners for a price that will enable the RP or Local Housing Company to deliver the affordable housing without the need for public subsidy.
42. The SPD also requires affordable housing to be dispersed through the development (“pepper potted”).
43. The S106 agreement would secure the overall proportion of each size and tenure and condition 3, combined with applications for approval of reserved matters, would ensure appropriate phasing of delivery and distribution of affordable housing of different types through the development. informative 13 also refers.

Specialist housing

44. In accordance with Core Strategy Policy CP2 *Inclusive Communities* new development should meet the needs of the aging population and people with special needs, among others, and MDDL policy TB05 *Housing Mix* requires a proportion of housing (determined on a site-by-site basis) to be built to Lifetime Homes standards. Based on needs at the time, the Infrastructure SPD anticipated an element of extra care housing within the South Wokingham SDL as well as a proportion of the affordable housing being built to Lifetime Homes standards.

45. Based on current needs and strategies, there is no need for additional extra care accommodation within the SDL but the need for Lifetime Homes remains: 5% of the affordable housing and 5% of the market housing should conform to M4(2) standard (accessible and adaptable dwellings) (or any other applicable standards that superseded them) to ensure the provision of lifetime homes which are adaptable to varying needs. These should consist of one and two-bedroom flats in a cluster of at least three units. The S106 agreement would secure this and condition 3, combined with applications for approval of reserved matters, would ensure appropriate phasing of delivery and distribution.

Landscape

46. Core Strategy policy CP1 *Sustainable Development* and CP3 *General Principles for Development* established a requirement for high quality of design that respects its context and maintains or enhances the quality of the environment. This includes the way development integrates with its surroundings and the use of appropriate landscaping.

Landscape Character

47. MDDL policy TB21 *Landscape Character* amplified by the Borough Design Guide (General Principle G1) and South Wokingham SPD (Design Principle 1a) require proposals to demonstrate how they have addressed the requirements of the council's Landscape Character Assessment and respond positively to the local landscape context, retaining or enhancing features that contribute to the landscape including topography, natural features – hedgerows, trees, watercourses etc. - heritage assets, settlement patterns and the network of routes. New development should protect and enhance green infrastructure networks, promoting connectivity between different parts of the network and integrating with adjacent open space (MDDL policy CC03 *Green Infrastructure, Trees and Landscaping*).
48. Core Strategy Policy CP21 *South Wokingham Strategic Development Location* and the Concept Rationale (Core Strategy Appendix 7) amplified by the South Wokingham SPD (Section 4, part 1 in particular, is concerned with the Landscape Framework) identify the landscape setting of the SDL – especially the course of the Emmbrook - as a key determinant of the urban form.
49. These policies and guidance establish that there should be continuous network of open space running through the SDL, focused on a linear corridor along the course of the Emm Brook and its tributaries. New homes should be embedded within the landscape setting with existing landscape features and notable buildings providing structure. Management of the transition between town and country will be critical to the successful integration of the new development within the landscape and the southern extent of the SDL should be defined by the landscape setting. Borough Design Guide Design Principle RD9 also highlights the need for the edge of built up areas to relate to local patterns and for landscaping to soften the edge of settlements and to help integrate new housing into its rural setting.
50. The council's Landscape Character Assessment identifies the area to the South of Wokingham as N1 *Holme Green Pastoral Sandy Lowland*. It is a gently undulating, agricultural landscape in predominantly pastoral use – small scale 'hobby' farming, speciality farms and pick-your-own - with smaller areas of arable land and an 'unmanaged' character. It is peaceful and sparsely settled, with

clusters of settlement at to Holme Green and Gardeners Green and large manor houses converted to educational establishments. The Emm Brook and its tributaries form a network of brooks in small valleys, although these are not visible in the landscape being hidden within woodland and trees. The land appears to slope up from these watercourses to the edge of Wokingham to the north and to the forested plateau (Gorrick Plantation) to the south.

51. It is a landscape of moderate quality and condition and the strategy is to enhance the existing character through reinstatement of hedgerows on historic field boundaries, maintaining open views and appearance of settlements being integrated within the rural landscape and increasing the extent of native deciduous woodland, liking existing sites whilst avoiding loss of significant views.
52. The application site was in equestrian use until recently and comprises a number of irregularly shaped, small to medium size fields defined by a combination of fencing, hedgerows, and mature trees. This site is enclosed by bands of trees and small areas of woodland along the site boundaries and the Emm Brook but is visible from the Public Right of Way that traverse it. The topography of the site is influenced by the Emm Brook which forms a relatively wide, flat area to the centre and east of the site, rising by about ten metres to the northeast and with a shallow domed landform which elevates the southwestern part of the site.
53. The SPD Framework Plan identifies a residential parcel to the north of Chapel Green and two further parcels to the east of the application site, north and south of the SWDR, effectively extending the parcels within Phase 2b that form the 'elongated central neighbourhood' (see paragraph 71). The land to the west of Chapel Green and between Chapel Green and the central neighbourhood are identified as open space, a suitable location for SANG.
54. The proposals are broadly in accordance with this strategy, with two relatively minor differences. The residential parcel north of Chapel Green (Parcel D) extends further south than anticipated by the SPD and the land west of Parcel D is open space but not SANG. These changes have implications for the setting of the grade I listed Lucas Hospital (considered in section 0) and the delivery of SANG and other public open space to serve the SDL delivery strategy (considered in sections 0 & 0), which are acceptable. In terms of landscape character, the land east and west of parcel D is proposed as public open space (a park and allotments), the undeveloped nature of which remains consistent with the spatial strategy in the SDP.
55. The most extensive area of open space within the development would be the SANG, which would be contiguous with the Holme Park SANG within phase 2, together forming a continuous open space extending from the SWDR to Heathlands Road. The proposed SANG would contain two distinct areas, described in the applicant's Green Infrastructure Strategy as Emm Brook Parkland and Southeast Meadows. The Emm Brook Parkland would follow the line of the watercourse. Existing vegetation would be retained and reinforced with new native planting using species that thrive in damp soils, including several large growing species that would, over time, stand as individual parkland trees. The Southeast Meadows, adjacent to chapel Green and Lucas Hospital would have a more open character (allowing intermittent views of Lucas Hospital to be retained, see section 0) with both wetland meadows and drier meadows on higher ground.

56. The natural characteristics of the SANG would be complemented by the South-West Park, the which would provide a more formal area of open space, to the south of parcel D containing a park, playground and allotments. Existing tree belts would provide a degree of enclosure to the west with some semi-permeable openness to the wider landscape to the south.
57. A Northern Natural/Semi-Natural Corridor focused on existing trees and woodland, would incorporate areas of ecological value and provide a visual buffer. A valued ecological area of Lowland Fen is located in the north-east of the Project Development.
58. The Emm Brook Parkland, Southeast Meadows and South-West Park would continue the approach established by the Holme Park SANG in phase 2b, providing a 'soft' southern edge to the SDL, consistent with the requirement to provide a transition between built-up areas and the adjacent countryside.
59. The existing landscape framework of hedgerows, woodland, tree belts, riparian trees and shrubs and individual field trees provides maturity which enhances the landscape character and provides screening between the areas of proposed development and the surrounding landscape as well as a range of habitats supporting biodiversity. The green infrastructure strategy for the site aims to retain existing vegetation as far as possible, reinforced with suitable native planting to provide green corridors and a variety of habitats, minimising impacts on existing ecology and maximising ecological benefits. These habitats include lowland fens and acid grassland, wetland and riparian vegetation and planting along the Emm Brook and woodland and tree planting.
60. The proposed network of open space would be consistent with the principles established by the SPD providing a multi-functional network of blue-green open spaces, informed by the existing landscape structure. As anticipated by policy, as well as providing for outdoor recreation, it is proposed to incorporate SuDS within the open spaces and landscaping (using local native plants to enhance the natural or semi-natural vegetation characteristics of the area), which would allow the range of habitats to be increased, supporting ecology as well as providing variety and interest for users and mitigating for loss of vegetation arising from the development. The functions of these spaces as Public Open Space are considered further in section 0 of this appraisal.
61. An important characteristic of Wokingham is the verdant, tree lined streets that lead into the town centre, making an important contribution to the character and environment of the area. This character is reflected in the SPD street typologies which require tree planting on both sides of primary streets and at least one side of secondary streets. Accordingly the SWDR design (albeit the subject of separate applications) incorporates verges and trees on both sides. The street typologies within the development will also need to reflect this approach. Condition 4 refers.
62. The applicants' Landscape and Visual Impact Assessment considers the impact of the proposed development and how it can be appropriately mitigated within the open space. The Provisional Layout shows how mitigation could be delivered through the design and layout of development parcels themselves, through integration of structural planting and SuDS elements for instance. This would

need further consideration at the reserved matters and conditions stages (conditions 4 & 15).

Trees

63. MDDL policy CC03 *Green Infrastructure, Trees and Landscaping*, supported by Borough Design Guide Design Principle R14, requires new development to retain and protect existing trees, hedges and other landscape features and to incorporate high quality, ideally, native planting and landscaping.
64. Existing trees and hedgerows on the site are generally concentrated along field boundaries, footpaths and watercourses. A number of individual trees, groups of trees and woodlands are protected by Tree Preservation Order (TPO 1435/2012). These are generally along footpaths and watercourses and include the trees and woodland that line footpath 10, incorporating the areas of woodland on the northern part of the site (where the path converges with Footpath 9) and north of Ludgrove school (which is designated ancient woodland) together with the alders along the southern part of Emm Brook and its tributary.
65. The application is accompanied by a Tree Report, Arboricultural Impact Assessment (AIA) and Arboricultural Method Statement (AMS) which categorise trees according to their quality and landscape value and establishes their root protection areas, in accordance with BS5837:2012. It recorded 29 individual trees, one woodland and one group as category A (high quality and landscape value); 61 individual trees and 11 groups as category B; 25 trees and 34 groups recorded within the survey are category 'C' (low value) and seven category U trees (not suitable for retention).
66. A number of these trees are on the route of the SWDR and their loss has been considered as part of the assessment of that application (application 192928). Nevertheless, a drawing showing the combined tree loss is required to ensure a coordinated approach and ensure that there is no reliance on trees that are to be removed for the road scheme. Condition 11 refers.
67. While the proposals for Phase 3 - other than the SANG - are in outline, the parameters for development establish which trees would be in areas of open space and which would be within development parcels. Outside the SWDR corridor, the majority of trees are proposed to be retained within areas of public open space but the proposals would entail partial removal of five groups of trees within parcel D, all category C or lower: these are to the east of Chapel Green House (G47); a row of trees along the south side of Wokingham Footpath 24 (probably a remnant of the original hawthorn hedge, too small to be classed as trees in relation to BS5837 or significant in landscape terms); a group at north-western corner of parcel D to facilitate access to the site and SuDS (G12); and two sections from on the western edge of parcel D (G3), one (approximately six metres wide) to provide access to the field to the west of the application site and another section at the rear of plots 113-118. The Tree protection plan also identifies an unnumbered tree on the northern boundary of 1 Chapel Green Cottages for removal. No further information is on the condition of the tree has been provided but conditions 11 & 12 would ensure that its importance is properly assessed at the reserved matters stage. Subject to replacement planting as part of the landscaping of the site - which would be secured by condition 15 - there is no objection to the loss of these low value trees to facilitate a satisfactory layout.

68. A group (G3) interspersed with individual trees marks the western edge of parcel D. The provision layout shows a row of six houses (plots 113-118) backing onto this boundary. Overshadowing of the west-facing gardens and rear elevations of these properties, in particular plots 115-117, could result in future pressure to reduce or remove vegetation, especially T18 and T19 which are category A oaks. Garden depths are shown to be 12-17 metres and subject to thinning of the eastern face of the group, the applicant's AIA concludes that the layout is unlikely to result in shading or undue pressure from future occupants to remove or excessively prune trees. This aspect of the layout would require careful design at the reserved matters stage to ensure an acceptable relationship is achieved and the landscape officer has indicated crown lifting may be required. Conditions 4iv), 12 & 13 refer.

Character of the development

69. Core Strategy policies CP1 *Sustainable Development* and CP3 *General Principles for Development* establish an overarching requirement for high quality design that maintains or enhances the high quality of the environment; development should be appropriate in scale of activity, mass, layout, built form, height, materials and character to the area; protecting amenity; and providing an attractive, functional, accessible, safe, secure and adaptable environment: buildings and spaces should contribute to a sense of place in themselves and in the way they integrate with their surroundings including the use of appropriate landscaping. The Borough Design Guide provides more detailed guidance on general principles for good design (section 4 in particular), while Core Strategy Appendix 7 and the South Wokingham SPD give site specific advice on how high quality development would be achieved within the SDL. There is also a National Design Guide: *Planning practice guidance for beautiful, enduring and successful places* (October 2019).
70. The parameters plan and Design and Access Statement should establish the design principles and concepts that would be applied to the development.

Distinct neighbourhoods

71. Residential development should contribute positively towards the character and quality of the local area. For large sites, a distinctive identity may be created by establishing a new character that relates well to the existing (Borough Design Guide design principle R1). The SDL presents such an opportunity. Core Strategy A7.52 and SPD Design Principle 2a establishes that development in the SDL should consist of a series of distinct neighbourhoods, with recognisable centres, following the linear nature of the site: a northern neighbourhood north of the railway, a central neighbourhood spanning out from Easthampstead Road, an elongated residential area following the SWDR and railway and a small pocket to the west of the SDL close to Chapel Green Farm.
72. The northern neighbourhood has already been delivered at Montague Park and phase 2 will deliver the central area. Proposed parcels C1 and C2 would complete the elongated central neighbourhood, most of which lies within phase 2, and proposed parcel D would deliver the final, western neighbourhood at Chapel Green. Thus, the pattern of development would be consistent with the SPD. The DAS identifies key spaces within parcel D (at points where existing public footpaths converge) and potential locations for landmark buildings. These combined with the relationship with the open spaces within the development will

establish a distinct identity, reinforced by the application of character typologies described in section 0 and the design code pursuant to condition 4.

Layout of the residential parcels

73. New development should be integrated with its surroundings, providing an interconnected network of streets and spaces (as described in section 0 of this appraisal, The transport network within the SDL). Residential areas should be organised around a robust, traditional pattern of perimeter blocks, with clear distinction between public and private areas and building frontages overlooking public areas, providing natural surveillance and generating activity (Section 4 of the Borough Design Guide and SPD Design principle 2a).
74. The DAS proposes an arrangement of perimeter blocks, presenting a strong frontage to define the public realm whilst ‘sealing-off’ rear gardens, thereby protecting the amenity of residents. Landmark buildings, vistas, edges and nodes would enhance the legibility of this development and street widths and building lines would reflect the local characteristics of built form contributing to a sense of place and local distinctiveness. The DAS identifies the opportunity to incorporate windows in the side elevation of houses on corner plots and the Crime Prevention Design Advisor has highlighted the importance of this in achieving active frontages and natural surveillance. The provisional layout shows how a layout based on the Movement and Access parameters plan could achieve this.

Character typologies

75. SPD design principle 3a establishes three character typologies which should be applied to reinforce the settlement structure and help create distinctiveness within the SDL: urban residential, general residential and rural interface. The distinctiveness of each character area would result from a combination of its layout, built form, density, block configuration, approach to the design & landscaping of the public realm, provision of car parking and boundary treatments.
76. The building heights parameters plan and the character area plans in the DAS establish the proposed pattern of development.
77. The two eastern parcels (C1 south of the SWDR and C2 to the north of it) form a continuation of the elongated central neighbourhood within phase 2b, albeit separated from it by the green corridor along Footpath 10. The SPD anticipates urban residential development fronting the road with general residential adjacent to the railway line and rural interface on the southern edge. The character area plans in the DAS and Building Heights parameters plan follow the same pattern, with urban residential development of up to three-storey (12.5 metres maximum to the ridge) in parcel C2 and along the SWDR frontage of C1. The Landscape and Visual Impact Assessment assesses the impact, including the proposed three-storey elements, from a variety of viewpoints and – once established - there would be only partial views of rooftops over existing buildings and vegetation. For comparison, existing dwellings on Gipsy Lane, which are on higher ground, are visible from some viewpoints but do not break the skyline as trees create a vegetative backdrop. The remainder of C1 would be a combination of urban residential, with rural interface to the south but limited to two-storey (10 metres maximum to ridge). The northern parcel doesn’t extend as far north as the general residential area anticipated by the SDP due to landscape and ecological constraints. This also follows the pattern within phase 2b with urban residential

development of up to three-storeys (12.5 metres) along the SWDR corridor and general residential beyond up to 2.5-storeys (11.5 metres beyond).

78. The indicative SPD Framework Plan anticipates parcel D being separated from the SWDR by a green corridor and having a rural interface character. As proposed the parcel would adjoin the SWDR. This is consistent with design principles of providing an interconnected network of streets with adjacent, perimeter-block development providing active frontages, and – as a degree of continuity along the length of the road is desirable - it is appropriate to continue the pattern of urban residential development fronting the road, falling away to the south, as is proposed. The northern part of the parcel is proposed to be urban residential with the development along the SWDR frontage being up to three-storey (12.5 metres maximum to the ridge) and the remainder being up to 2.5-storey (11 metres maximum to ridge) falling to two-storey (10 metre maximum to ridge) on the eastern side adjacent to the SANG. The central part of the parcel, to the north and west of Chapel Green would have a general residential character, with two storey development (10 metre maximum to ridge) and the southern and eastern edges, adjoining Public Open Space would have a rural interface character, also with a maximum building height of two-storey.
79. While the building heights parameters plan and DAS establish maximum heights for each part the DAS also explains that, in general, the dwellings are proposed to be two-storey with occasional 2.5 or three-storey buildings to provide emphasis and legibility in key location such as on corners and to enclose key streets or spaces within the development (although paragraph 5.17 explains landmark status does not necessarily dictate the need for increased height).
80. The DAS provides an indication of the features that would differentiate the different character areas, which is summarise in the following paragraphs and would be amplified by condition 4.

Urban residential

81. The SPD establishes that development along the SWDR should have a formal “urban residential” character with closely spaced town-houses, terraces and apartments fronting the street, providing continuous built frontages and a high degree of enclosure.
82. Consistent with this, the DAS proposes the highest density of development, with a greater proportion of flats and buildings of up to three-storeys within the urban residential character area. There should be a particular focus on the massing and architectural style of buildings in key frontages, such as those fronting the SWDR, so that they contribute positively to the quality and character of the new development. Box bay windows and balconies are proposed to be used animate facades and provide vertical emphasis while soft landscaping would be informed by the urban context, to include street trees on at least one side of the road.
83. The SPD acknowledges that there will be limited opportunities for direct access from the SWDR and consistent with this a single access from the SWDR is proposed to each parcel. Tree lined roads leading into the town are a characteristic of Wokingham (MDDPL Policy CC03 refers to Green Routes) and the proposed layout provides an opportunity for structural landscaping along the SWDR, which would be characteristic of the major routes through the town.

General residential

84. The SPD establishes that development in “general residential” character areas should also generally be parallel to the street but have a less regular built form consisting of shorter terraces, semi-detached and occasional detached two-storeys buildings, occasionally rising to three at key corners and along important secondary streets, with varying plot widths. Combined with small front gardens - enclosed by walls, railings or hedges - this would create variety while retaining a sense of enclosure. Parking is expected to be predominantly on-plot, with some mews and informal on-street parking.
85. In accordance with this guidance, the DAS proposes two-storey development with a relatively high proportion of terraced and semi-detached dwellings and use of on-street parking to generate activity within streets. Details such as string courses would provide horizontal emphasis with bays and gables used to create vertical emphasis in key locations. Frontage parking would be designed with streetscape in mind, incorporating opportunities for tree planting.

Rural interface

86. The Core Strategy (Appendix A7.52) requires a managed transition between town and country. Accordingly, the SPD expects development on the southern edges of the built-up area, at the “rural interface” to respond to the rural context with less dense, development of two-storey, detached or semi-detached houses on wider plots, with a more informal layout, facing onto open space. Front gardens should be enclosed by hedges rural fences. Parking should be on-plot with informal, on-street visitor parking.
87. Accordingly, relatively low density, two-storey development of largely detached dwellings set within larger plots are proposed in this character area. The buildings would have horizontal emphasis with gables, reflecting features found in suburbs nearby, breaking the eaves line. Soft landscaping would be informed by the rural context. As this would be an edge of the settlement location, some streets - such as the southern and eastern edges of Parcel D and the western edge of Parcel C1 - could incorporate a more open aspect with deeper front gardens and facades set further back (DAS paragraph 5.14).

Materials and enclosure

88. Materials and enclosure are an important aspect of local character and the SPD - design principles 3a and 3d - provides guidance on how these elements can be used both to reinforce local character and to help distinguish the different character areas within the development.
89. The DAS includes an indicative materials palette which is very similar for each character area. Walls would be predominantly brick with secondary materials to create accents, if necessary, and roofs could be slate, composite slate, tile, standing seam metal or single ply membrane. Materials are one of the defining characteristics of Wokingham and the secondary materials - including timber - and materials palette proposed - reds, browns, ochre, grey, occasional white – need further consideration to ensure that the development achieves a distinctly “Wokingham” character. Conditions 4 & 8 together with informative 7 require further consideration of the choice of materials and how they would be used to

help distinguish the different character areas within the development and convey a 'Wokingham' character.

90. The DAS indicates that the soft landscape for each character area will be informed by its context and that ownerships and responsibilities for external spaces would be clearly distinguished but does not include any discussion about the definition of external space, provision of a clear hierarchy of connected spaces or how the surrounding landscape character and mitigation measures identified in the LVIA can be incorporated into each of the residential character areas. The design code pursuant to condition 4 would establish principles, to be applied at the reserved matters and conditions stage (condition 15 p).

Public Art

91. Core Strategy Appendix 7 (paragraph A7.52) and the SPD (Design Principles 3f and 1e(iv)) establish that public art should be an integral component of the built environment and landscape framework. Paragraph 8.50 of the planning statement acknowledges that Public Art can be secured either within the development parcels or public open space and the SANG hard landscaping proposals identify potential locations. Conditions 3 x) & 7 would secure a strategy for delivery of public art in the SDL.

Heritage

The setting of listed buildings

92. Section 66(1) of the Planning (Listed Buildings and Conservation Areas) Act 1990 establishes a statutory duty to have special regard to the desirability of preserving listed buildings or their setting or any features of special architectural or historic interest which they possess. Consistent with this, Core Strategy Policy CP3 *General Principles for Development* and MDDLDP policy TB24 *Designated Heritage Assets* establish that development should not have a detrimental impact on important heritage features and works affecting heritage assets or their setting should conserve and, where possible, enhance their important character and special architectural or historic interest.
93. The application is supported by a Built Heritage Baseline Assessment considers heritage assets within 2km of the site: there are no listed buildings within the application site boundary but it encircles the Lucas Hospital complex, so there is a potential effect on its setting; there would be no significant impact on other built heritage due to intervening built form and lack of intervisibility, as well as lack of functional historic associations with the application site.

Lucas Hospital

94. The application site wraps around three sides of the Lucas Hospital complex, which consists of the Grade I listed Henry Lucas Hospital, Grade II* listed outbuildings immediately to the north of it and, beyond them, a walled garden (the wall is Grade II listed). The principal elevation of the 1860 Hospital building is to the south, where it overlooks a gravel driveway, lawn and a meadow beyond. Access is via a lane which continues along the Hospital's western boundary, also serving Chapel Green. The ownership of Lucas Hospital has never extended beyond the walled garden and land to the south, although there is evidence that land to the north, at Chapel Green may have been used in conjunction with the Hospital complex during the mid-late 19th century.

95. The Built Heritage Baseline Assessment establishes that the heritage significance of the complex is derived primarily from the architectural and historic interest of the buildings and the relationship between them, with the setting being of lesser importance.
96. Other than the expansion of Chapel Green Farm, there has been little change to it or the surrounding landscape since the mid-19th century. English Heritage identified the impact on this tranquil rural setting as their main concern, commenting that it is unusual in an alms house of this period, most of which are urban buildings; the situation must have been an intentional decision on the part of Henry Lucas. The surprise of coming across the building in a quiet country lane enhances the impact of its architecture, making it feel like a grand country house.
97. The Hospital complex is relatively confined and public views of it in its rural context are limited. The best views are from the southern part of Footpath 25 towards the south elevation of the main building. There are also glimpsed views from the wider footpath network but these are limited and contribute little to the appreciation of the Hospital complex. Thus, the central and southern parts of the application site make the greatest contribution to the overall significance of the Hospital, while the northern part of the site is separated from it by development at Chapel Green, so is less important.
98. Proposed residential parcel D is situated largely to the north of the Hospital complex, separated from it by the development at Chapel Green. SANG is proposed to the east of the Hospital and allotments immediately to the west, with a park and play area beyond, separated from the Hospital by a vegetated strip of land (outside the application site), which screens the lane contributing to its verdant, rural character and setting to the Hospital. Thus the relatively undeveloped nature of the more sensitive, central and southern parts of the application site would be maintained. While the proposals extend the residential parcel a little to the south of the area shown in the SPD, bringing it closer to the Hospital, the encroachment is only minor and the rural approach to the alms houses from the west would be maintained.
99. Hence English Heritage and the council's Conservation Officer are of satisfied that enough space has been left around the listed building to maintain its rural setting, subject to additional planting, along the eastern and south boundary of the proposed allotments, to reinforce the rural appearance of the lane and the setting of the Hospital. Conditions 3 xi) & 15 l) refer.

Archaeology

100. Core Strategy Policy CP3 *General Principles for Development* establishes that development should not have a detrimental impact upon heritage assets. This is amplified by MDDL Policy TB25 *Archaeology* which requires developments in areas of high archaeological potential to provide an assessment of the impact of the development upon archaeological remains and to secure preservation in situ or - where this is not practical - excavation, recording and archiving of remains.
101. There are no identified areas of high archaeological potential within phase 3 and no known heritage assets. Nevertheless, the site has modest archaeological potential enhance by its size and largely undeveloped nature.

102. A large proportion of the site is proposed to be green open space, creation of which would involve only minimal and localised impacts on below ground deposits but residential development has potential for more major impacts. The applicant's Environmental Statement and archaeological desk-based assessment recommend a phased programme of trial trenching in advance of reserved matters together with a strategy for mitigation, including preservation in situ. Condition 22 would secure this.

Public Open Space

103. Core Strategy policy CP3 g) *General Principles for Development* establishes an overall requirement for 4.65 ha/1,000 population of open space within new development. This is amplified by MDDL policy TB08 *Open Space, sport and recreational facilities standards for residential development* which sets out the requirements for different types of open space. The now historic CIL Regulation 123 List anticipated delivery of amenity open space and play areas within the SDLs together with land for other types of green infrastructure.
104. In line with the comprehensive masterplanning approach required by the Core Strategy and SPD's, the open space requirements for Phase 3 have been considered in conjunction with the proposals for Phase 2 and what has already been delivered in Montague Park to ensure appropriate distribution of open space across the SDL.
105. The Core Strategy (A7.42 c), A7.45 and 7.46) and the Landscape Design Principles set out in Section 4 of the South Wokingham SPD, anticipate that much of the open space provision will to be provided in a multi-functional corridor, along the course of the Emm Brook and its tributaries, providing for recreation as well as flood water attenuation and biodiversity. Consistent with this, the SANG, which would be the most substantial area of Public Open Space within the phase, follows the course of the Emm Brook.
106. The proposed green infrastructure network within Phase 3 consists of the SANG, a park to the south of parcel D which would contain an area of parks and gardens, play area, allotments and amenity open space, plus green corridors along the western and northern edges of the site provide in for natural and semi-natural green space. The remainder of section 0 explains how Development Plan requirements would be met.

Parks and gardens

107. The 0.5 hectare site proposed, to the south of the largest development parcel, would fulfil the 0.45 hectare requirement for **parks and gardens** for phase 3 (at 1.10 hectares/1,000 population).

Children's play

108. Two play areas are proposed within phase 3: a combined Local Equipped Area for Play (LEAP) & Local Area for Play (LAP) in the open space to the south of the largest residential parcel and a Local Landscaped Area for Play (LLAP) within the SANG. Their combined area would be 0.15, slightly over the 0.1 hectare requirement for phase 3 (at 0.25 hectares/1,000 population). The size of the individual play areas and distribution of different types of provision is appropriate

considering the phase on its own and also in the context of the wider SDL; a Multi-Use Games Area (MUGA), for use by older age groups, a Neighbourhood Equipped Area for Play (NEAP) and three LEAPs and a LLAP in Phase 2 and a NEAP, two LEAPS and LLAP and community use of the school MUGA at Montague Park. The character of each play area would respond to its location and landscape context.

109. While the detailed design is for the reserved matters stage, it has been demonstrated it would be possible to achieve suitable buffers (to prevent undue disturbance to neighbouring properties) whilst also achieving natural surveillance (for safety reasons, South Wokingham SPD Design Principle 1c(v)).

Allotments

110. A 0.59 hectare **allotment site** is proposed, exceeding the 0.21 hectare requirement for phase 3 (at 0.52 hectares/1,000 population). It would be located to the south of the largest development parcel, where it can be integrated into the landscape (SPD Design Principle 1c(iv) and the WBC Open Space, Sports and Recreation Strategy, November 2013). Combined with the allotment site in Montague Park and the sites proposed in phase 2b provision would be well distributed through the SDL.

Amenity greenspace

111. 0.4 hectares of **amenity greenspace** for informal recreation and visual amenity is proposed within the part to the south of parcel D, meeting the 0.4 hectares policy requirement (at 0.98 hectares/1,000 population).

Natural & semi-natural greenspace

112. **Natural and semi-natural greenspaces** are intended primarily to protect and enhance biodiversity; suitably designed SANG can count towards this provision (MDDL policy TB08 *Open Space, sport and recreational facilities standards for residential development*). 3.2 hectares of natural and semi-natural greenspace are proposed, in addition to the SANG which is proposed to incorporate a variety of habitats, intended to support biodiversity as well as providing recreational opportunities. Thus, the overall provision would exceed the requirement for 1.17 hectares of natural and semi-natural greenspace (at 2.84 hectares/1,000 population).

Civic space

113. The most appropriate location for **civic space** is within the neighbourhood centres, which are intended as a focus for community activity. The development at Montague Park provided civic space for phase 1 and the 400m² space proposed in the neighbourhood centre in phase 2b (to be secured through the S106 for phase 2b) would meet the needs of the remainder of the SDL, including the 42m² requirement for phase 3 (at 0.01 hectares/1,000 population).

Securing public open space and future maintenance

114. Reserved matters, plus conditions 3 & 15 would control phased delivery of on-site public open space, with suitable landscaping (except for allotments which are to be delivered by the council through CIL funding); transfer to the council or other

suitable body would be secured through the S106, together with a commuted sum for future management and maintenance. Section 0 refers.

Outdoor Sports Facilities

115. MDDL policy TB08 *Open Space, sport and recreational facilities standards for residential development* requires a total of 7.25 hectares of land for outdoor sport for the SDL development south of the railway (at 1.66 hectares/1,000 population): 0.86 hectares for phase 2a, 5.71 hectares for phase 2b and 0.68 hectares for phase 3 (reduced from the 0.76 hectares reported to committee in May 2021 due to the subsequent reduction in the maximum number of dwellings proposed). In addition, the provision for Montague Park was met partly through dual use of facilities at the Floreat Montague Park school and partly through a contribution towards 1.14 hectares off-site provision (some of which has already been used for delivery of a 3G pitch at Emmbrook School). The South Wokingham SPD envisages playing fields will open-up off the linear corridor of that runs through the development.
116. The, now historic, CIL Regulation 123 List anticipated provision of land for outdoor sport within the SDLs but, since the adoption of the Development Plan, the South Wokingham SPD and the grant of outline planning permission for Montague Park, the council has acquired approximately 26 hectares land adjoining the SDL at Gray's Farm, with the intention of delivering a sport hub to provide outdoor sports capacity for the remainder of the development at South Wokingham and elsewhere (and the site has been promoted for this use through the Local Plan Update; Site 5WW006). 3.3 hectares (which is unsuitable for sports use) is included in the Holme Park SANG, leaving 22.7 hectares for sport. The S106 would secure a contribution of £2,347 per unit (indexed linked) to secure land for off-site provision of sports facilities (consistent with the approach taken in other SDLs). Laying out of the pitches and associated development, including access routes within the site, would be undertaken by the council from CIL.

Thames Basin Heaths Special Protection Area (SPA)

117. The Thames Basin Heaths Special Protection Area was designated under European Directive due to its importance for heathland bird species. Southeast Plan Policy NRM6 *Thames Basin Heaths Special Protection Area* and Core Strategy policy CP8 *Thames Basin Heaths Special Protection Area* establish that – alone or in combination - new residential development within a 7km zone of influence is likely to contribute to a significant impact upon the integrity of the Special Protection Area and is, therefore, required to provide avoidance and mitigation measures in the form of Suitable Alternative Natural Greenspace (SANG) and Strategic Access Monitoring and Management (SAMM).

Suitable Alternative Natural Greenspace (SANG)

118. The South Wokingham SDL falls within the 5km zone of influence and, accordingly, Core Strategy Policy CP21 amplified by Design Principle 1c(vi) require provision of SANG at a minimum of 8 hectares/1,000 population (calculated at a rate of 2.4 persons per household).
119. MDDPL policy SAL05 *Delivery of avoidance measures for Thames Basin Heaths Special Protection Area* allocates six areas of SANG within the South Wokingham SDL and establishes a presumption that these sites will be used as SANG unless

sufficient alternative avoidance can be provided. Two of these allocated SANG sites – east and west of Lucas hospital - fall within phase 3.

120. Through more detailed masterplanning (since adoption of the MDDL in 2014) an alternative strategy for delivery of SANG to serve the SDL south of the railway has emerged which would deliver two substantial SANGs with sufficient capacity to mitigate the impact of the entire SDL. The SANG at St Anne's Manor (application 190900), which was approved in May 2021, would form an extension to the existing Buckhurst Meadows SANG and the proposed SANG would form a continuation of the Holme Park SANG (within phase 2b, application 191068), forming a single 32.82 hectare SANG extending along the southern boundary of the western SDL.
121. The approach was considered in the report on phase 2b, which concluded that the SANGs would be less fragmented than the MDDL approach, with good access for residents (all the new homes in phase 3 the SDL would be within a 400 metre radius of a SANG). The circular walk within the phase 3 SANG alone would be short of the 2.3-2.5km required to fulfil Natural England SANG design quality standards but would connect to the footpath network within the Holme Park SANG which provides options for longer more varied routes, including a 2.7km circular route (condition 17 and the S106 refer). Thus the combined SANGs would fulfil design quality standards including walk length, multiple access points, a variety of types of routes and habitats achieving a semi-natural character. This approach is consistent with the Core Strategy requirement for comprehensive planning and delivery.
122. The design quality standards require car parking to be provided where SANGs are more than four hectares and the homes they serve are more than 400 metres away. All the dwellings within Phase 3 are within 400 metres of the SANG, so there is no requirement for parking but, should development elsewhere seek to rely on the spare capacity in the SANG in future, the need for parking would be a consideration. (The parking provision for the neighbourhood centre within phase 2b includes an allowance for dwellings within phase 2 that are more than 400 metres from SANG).
123. In terms of capacity, the 8.12 hectares of SANG proposed would meet the 3.28 hectare requirement to mitigate the impact of up to 171 dwellings in phase 3, leaving 4.83 hectares spare capacity. Similarly, the St Anne's SANG would have 3.7 hectares capacity after mitigating the impact of phase 2a. Together these two SANGs would have 8.53 hectares spare capacity, more than the additional 6.12 hectares required to mitigate the impact of phase 2b (condition 18 of 191068 refers).
124. Within phase 3, the SANG provision would be concentrated to the east of Lucas Hospital with other types of open space to the west. The implications in terms of landscape character and the setting of Lucas Hospital, are considered in sections 0 & 0 and are acceptable.
125. Thus, the proposals meet both the quantitative and qualitative requirements for SANG and are compliant with Core Strategy policies CP8 and CP21. While the strategy differs from that indicated by MDDL policy SAL05, it would provide sufficient (and otherwise acceptable) alternative avoidance so is compliant with this policy too. Conditions 3 & 17 and the S106 would ensure delivery of SANG

before occupation of the dwellings it is to serve and the S106 would secure the transfer of the land to the council (to ensure it is maintained in perpetuity in line with Natural England's standards).

Strategic Access Monitoring and Management (SAMM)

126. Strategic Access Monitoring and Management (SAMM) is also required to mitigate the impact upon the SPA but is not considered to constitute infrastructure and remains to be secured by S106 at a rate of: £464.00 per one-bedroom dwelling; £646.00 per two-bedroom dwelling; £858.00 per three-bedroom dwelling; £1,010.00 per four-bedroom dwelling & £1,153.00 for each dwelling with five or more bedrooms.

Habitats Risk Assessment (HRA) Appropriate Assessment

127. The application is supported by a Report to inform Habitat Regulations Assessment Screening Assessment and Shadow Habitat Regulation Assessment (HRA), to inform the Appropriate Assessment for the site, which the council must undertake as the Competent Authority under Regulation 7 of the Habitats Regulations. (This is the same report that accompanied the phase 2 applications and assesses the combined impact of the SDL developments).
128. The report screens all European sites within 10km and identifies two instances where there could be likely significant effects: recreational pressure and air quality impacts on the Thames Basin Heaths SPA.
129. Recreational pressure – and how it can be avoided and mitigated - is considered in section 0 & 0 of this appraisal.
130. Traffic modelling identified sections of road where the development (alone or in combination) may lead to a significant change in traffic movements, resulting in a likely significant effect on air quality and consequently the adjacent habitats and their ability to support qualifying bird species. Where heathland habitat has been identified within the 200 metres of a screened-in road, the analysis concludes that changes to pollutant levels and/or retardation of background improvements would not have a significant effect on the SPA bird populations, either alone or in-combination and, accordingly, it can be concluded that there would be no adverse effect on the integrity of Thames Basin Heaths SPA.
131. Thus, modelling has demonstrated with sufficient certainty that the air quality pathway will not result in likely significant effects and the measures proposed to mitigate the recreational pathway are in line with a delivery framework approved by Natural England and are secured in such a way that gives certainty of their effectiveness. Therefore this project passes appropriate assessment.

Ecology

132. Core Strategy policies CP3 *General Principles for Development* and CP7 *Biodiversity* establish that proposals should not have a detrimental impact on ecological features. Species and habitats of conservation value should be protected and the ability of a site to support fauna and flora, including protected species, should be maintained and enhanced. Where the need for development outweighs the need to safeguard nature conservation importance and there are no less harmful alternatives available harm should be mitigated or compensated for.

In addition, MDDL policy TB23 *Biodiversity and Development* requires proposals to enhance and incorporate new biodiversity features, provide appropriate buffer zones between development and designated sites as well as habitats and species of principle importance for nature conservation and ensure ecological permeability. These principles are reiterated in Core Strategy A7.46 and SPD Design Principle 1b.

133. The ES Ecology chapter establishes the ecological baseline and the value of the ecological features identified (based on a desk study and field surveys in 2015 – 2017, updated in 2019 and 2021), assesses the effects of the proposed development on the identified features (during construction and once complete); recommends mitigation where necessary and identifies any residual effects. It also details species enhancements, not required for mitigation, to be implemented as part of the development.
134. The ecological strategy for the site is to retain and enhance key habitats (including existing woodland, trees, hedgerow, grassland, lowland dry acid grassland, fens and purple moor grass rush pasture) where possible, strengthened with suitable native planting and with buffers around to create a transition and ensure their continued success. While the development would result in loss of or reduction in the area of some habitats, there would be opportunities to replace these: native and wildlife friendly planting would be used to enhance existing habitats and create new ones - including reinforcement of riparian habitats, enhanced wet meadow grasslands and woodland and parkland tree planting - and to ensure ecological connectivity across the development. SANG's – and potentially other categories of open space - provide better habitats, in terms of distinctiveness and condition for biodiversity, than managed farmland).

Designated sites

135. There are no designated sites within the application site, although the ancient woodland north of Ludgrove School and east of the site is a Local Wildlife Site (LWS). The woodland is to be incorporated in the Holme Park SANG (within phase 2b), which will form a continuation of the SANG within the application site. While the woodland is likely to experience increased recreational pressure, this can be mitigated through a Woodland Management Plan (secured by condition on the planning permission for phase 2b).
136. The ES identified four other designated sites within the Project Zone of Influence which could be affected by the proposals: the Thames Basin Heaths Special Protection Area; Heath Lake Site of Special Scientific Interest (SSSI) & Local Nature Reserve (LNR); Holt Copse and Joel Park Wood LNR & Local Wildlife Site (LWS) and Trench's Bridge LWS. Due to the distance and lack of connectivity between the application site and these designated sites no significant effects are predicted during the construction phase. Provision of SANG within the development (section 0) would prevent significant additional recreational pressure on the SPA and the green infrastructure provision within the SDL would mitigate the recreational pressure on the other sites.

Habitats and species within the Project area

Priority habitats

137. The majority of the fields within the application site have been subject to some level of management through horse grazing, so are of low to moderate biodiversity distinctiveness value. There are pockets of fen habitat, rush habitat and acid grassland on the margins of the site. Field boundaries are marked by a combination of fencing, mature tree lines and hedgerows (identified as groups of trees in the arboricultural and ecological reports). Other habitats on-site included broadleaved deciduous woodland, stands of ruderal vegetation and a number of seasonally and permanent wet ditches. Of these six are habitats of principal importance: lowland fen meadow, purple moor grass and rush pasture, lowland dry acid grassland, lowland mixed deciduous woodland, wet woodland and hedgerows (one qualifying as “important” under the Hedgerow Regulations).
138. The ecological strategy for the site is to retain and enhance key habitat features where possible, with buffers around to provide a transition and ensure their continued success. The retained habitats would be supplemented with new green infrastructure mitigating for losses, creating new habitats and providing ecological connectivity across the site.
139. There are two pockets of Lowland fen meadow within the application site, within a wider complex of wetland habitats. Development in parcel C2 and formation of the SuDS basin to the south of parcel C1 would result both pockets being reduced in area. However, it proposed to translocate the fen habitat to a SuDS area within the SANG, where it could take advantage of SuDS outfalls and be managed to create a larger area of this habitat (conditions 16 & 20 refer). Recreational pressure on this area would be mitigated through provision of boardwalks.
140. The remaining priority habitats would be retained within the green infrastructure network and where necessary enhanced (conditions 15 & 16 refer). This includes removal of Himalayan Balsam – an invasive species the spread of which is forbidden by the Wildlife Countryside Act – from the wet woodland. Conditions 15 u) & 33 xxv) refer.
141. While the Emm Brook is not Priority Habitat, the Environmental Statement acknowledges that it is of local importance, forming an ecological corridor across the site. Water quality and discharge rates are important for the Book itself but also the wetland habitats within the site. Conditions 15, 16 & 19 would ensure appropriate landscaping, including enhancements to the river corridor, and future management.

Protected species

142. The baseline ecological assessment included surveys for bats, dormice, great crested newts, reptiles, badgers and invertebrates and identified the need to consider the impact on bats, badgers and breeding birds in more depth.

Bats

143. Surveys found varying **bat** activity levels across the site and a number of trees with potential to support roosting bats, although no roosts were identified. It is proposed to retain the woodland, hedgerow and river habitats that are of most significant value to bats and no significant breaks in linear corridor features are

proposed, so connectivity across the site would be retained. No trees with potential to support roosting bats are proposed to be removed. Loss of foraging habitat would be mitigated for through new native hedgerow, tree and shrub planting along with areas of species rich grassland. Provision of bat boxes within retained woodland and mature trees would provide roosting opportunities.

144. Increased light pollution would impact upon bat species, resulting in indirect loss of foraging and commuting habitat, but with sensitive lighting design, in accordance with the best practice guidance issued by the Bat Conservation Trust and Institute for Lighting Professionals (ILCP 2018), this impact could be mitigated and there would be no significant effect on bats. Condition 21 and informative 28 refer.

Breeding birds

145. In terms of **bird** populations on-site, the proposals would result in the loss of approximately 4.1 hectares of grassland habitat, which may adversely impact upon species that forage within grassland such as starling. However, the habitat remains common in the local area, therefore, no significant effect is likely to result to bird populations.
146. An increase in local residents is likely to result in the increase of pets, including cats, which are known to predate on birds, and has the potential to impact birds commonly associated with residential areas such as the more common garden birds. However, enhancement planting and the use of integrated bird boxes within buildings and on trees, would reduce impacts, albeit some limited residual impacts may remain. Condition 18 refers.
147. The direct loss of bird nests would be avoided through the removal of suitable habitat only outside of the bird breeding season (March to August) or under ecological supervision. Condition 33 refers.

Badger

148. No **badger** setts were identified on the site but there is a sett nearby and likely that badgers forage within and commute across the site. It is recommended in the ES that an updated survey is undertaken within six months of construction works commencing on-site and best practice methods should be adopted when working in the vicinity of known badger setts (the CEMP - condition 33 xxiii) – would secure updated surveys and ecological mitigation).
149. The landscaping and future management of the site would ensure the mitigation and enhancement set out in the ES is delivered (conditions 15, 16, 18 & 21). The CEMP (condition 33) would incorporate ecological mitigation measures required during the construction phase.

Biodiversity Net Gain (BNG)

150. The application is supported by a Biodiversity Net Gain (BNG) Assessment, calculated using DEFRA's Biodiversity Metric 3.0 Calculation Tool (Beta) (Natural England, 2021). This provides an assessment of the net change in the biodiversity value of the site as a consequence of the proposals, taking into consideration habitat type, area, distinctiveness and condition, ecological connectivity and strategic significance. It is an iterative process and the calculation would be refined as design and implementation progresses.

151. The initial assessment indicates that a minimum 10% Biodiversity Net Gain can be achieved. However, the next iteration would need to include land required in the short-term only for construction of the SWDR and a baseline assessment for river corridor units, given that there are main river and two ordinary watercourses flowing through the site. The SANG landscaping proposals would also need to be revised to fully achieve the high distinctiveness habitats proposed in the BNG assessment. Conditions 15, 16, 19 & 20 and informatives 8 & 9. The S106 would provide a mechanism for securing off-site BNG, should the final assessment result in a net gain of below 10% on site (see section 0 xx)).

Residential amenity

152. In addition to the overarching requirement for good design, Core Strategy Policy CP3 *General Principles for Development* establishes that development should not harm the amenity of adjacent sites. There are a number of aspects of residential amenity to consider: privacy, light and overbearing impacts (sections 0-0); internal space standards (section 0), external amenity space (section 0) and noise (section 0).

Separation distances between dwellings

153. The Borough Design Guide SPD establishes that buildings should be designed to provide reasonable levels of privacy in habitable rooms (design principle R15) and appropriate levels of daylight and sunlight to new and existing properties (design principle R18). Paragraph 4.7 provides guidance on the separation distances generally necessary to achieve appropriate levels of privacy, avoid unacceptable loss of light or overshadowing and overbearing impacts. It recommends minimum separation distances of 10 metres front-to-front across the street, 12 metres back-to-flank and 22 metres back-to-back, whilst acknowledging that greater separation may be desirable between higher buildings: up to 26 metres back-to-back and 15 metres back-to-flank is desirable between houses with three or more storeys and 30 metres for flats/houses with living rooms above ground floor. Conversely development in more urban settings or areas with a more intimate character may require a tighter, more compact layout.
154. This guidance applies equally to the relationships between new and existing dwellings and to those within new development but SPD Design Principle 1a(iv) emphasises the need for careful treatment of the transition between new and existing developments.

Neighbouring properties outside the site

155. Although the residential element of the application is in outline with all matters reserved, the land use and building heights parameters plans identify the broad location of residential parcels and maximum building heights (in metres and the number of storeys) allowing a high level assessment of the impact on neighbouring properties outside the site which would be reviewed at the reserved matters stage, when more detail is available. Furthermore, in this case, the Provisional Layout provides quite a high level of detail and – while this is illustrative and the proposals at reserved matters could differ – it does give a greater level of certainty about the relationships that are achievable.

156. The application site wraps around three sides of the cluster of existing residential properties and equestrian buildings at **Chapel Green**.
157. **Chapel Green House and annex** is two-storey, chalet style property with a single-storey side wing, gardens to the north and east, and equestrian buildings (demolished since submission if this application) to the west. Residential Parcel D would wrap around three sides of this property. To the west, the site of the former equestrian buildings and Public Right of Way (Wokingham Footpath 25) would provide about 40 metres separation between the existing dwelling and Parcel D. The provisional layout and parameters plans show an access road running along the eastern boundary of the development at that point, with two-storey houses (up to 10 metres in height) facing onto it. The garden to the north of the house is approximately 11 metres in depth and the application site boundary is a further 17 metres away, giving an overall separation of around 28 metres. The provisional layout shows two-storey houses to the north of Chapel Green House, with their flank elevations facing south. The garden to the east is about 35 metres in depth and the Provision Layout shows two-storey houses with gardens of 12-14 metres backing onto the garden, giving an overall separation of over 45 metres. Thus, it has been demonstrated that the normal separation distances between two-storey dwellings can be met, avoiding an adverse impact on Chapel Green House.
158. There is also an extant planning permission (192411) for two **bungalows on the site of the former equestrian buildings** (the site has been cleared). (An earlier application, 173607, for four dwellings at Chapel Green House was refused and dismissed at appeal). The approved bungalows would have a broadly north-south orientation and rear gardens of around 10-15 metres in depth. The Provisional Layout illustrates a minimum front-flank separation of around 17 metres between the closest dwelling in parcel D and the approved bungalows. As with Chapel Green House, there would be about 17 metres between the northern boundary of the gardens of the approved bungalows and the southern boundary of the application site, sufficient to avoid an unacceptable relationship between the proposed and approved development.
159. **1 & 2 Chapel Green Cottages** are a pair of two-storey, semi-detached houses with broadly east-west orientation. A partially implemented and therefore extant planning permission for single-storey extension to No 1 has been referred to in representations. Proposed residential parcel D would be contiguous with the northern, flank boundary of No 1 and SANG is proposed to the east and south of the cottages. The parameters plans show a cul-de-sac extending southwards along the eastern edge of parcel D and the Provisional Layout indicates that the end house would have a staggered, flank-to-flank relationship with No 1. There would be approximately 19 metres minimum separation between the two buildings. Due to the staggered relationship, there would be some potential for overlooking and careful design, taking into consideration of the layout of the existing property (and any extensions), would be needed at the reserved matters stage to ensure an appropriate relationship but subject to this, an acceptable layout would be achievable. Informative 12 refers. There is no reason to anticipate that the new residential development would generate an unusual level or activity or, therefore, disturbance. The informal, recreational use of the land to the east and south as SANG would not generate significant disturbance.
160. **The Barns** is a two-storey conversion, approved as a live-work unit. The proposed SANG would lie immediately to the east of it and the quiet, informal

recreational use of this land would not result in undue disturbance. 1 and 2 Chapel Green Cottages would separate it from Parcel D, over 35 metres away.

161. **Chapel Green Farm** is a two-storey detached house with a two-storey detached outbuilding to the north and a generous garden to the south. There would be around 14 metres between its curtilage and Parcel D and the parameters Provisional Layout shows 35 metres separation from the nearest proposed dwelling, with the Public Right of Way, Wokingham Footpath 25 and an access road within the development in between. The separation would be sufficient to avoid an unacceptable impact. The land to the west is proposed to be allotments, a quiet, informal use that would not give rise to disturbance.
162. **Lucas Hospital** is a three-storey dwelling with outbuildings and a substantial walled garden to the north of it, separating it from the other properties in Chapel Green. Allotments are proposed to the west of it and SANG to the east; quiet, informal leisure uses that would not give rise to disturbance. The nearest residential parcel (Parcel D) would be over 90 metres away, far enough to avoid any adverse impact upon residential amenity. The Hospital is a grade I listed building and the impact of the proposals on its setting are considered separately in section 0.
163. **West Lodge** is two-storey house, to the south of the Application site. Its northern elevation is only about four metres from the boundary but it is separated from the application site by the lane that provides access to Chapel Green, giving an overall separation of approximately 12 metres. The land to the north is proposed to be used as public open space and its quiet, informal recreational use would not give rise to disturbance. The footpath and cycleway that is proposed to run north-south through the open space, connecting Parcel D to Luckley Road between West Lodge and the railway bridge, is also intended to serve as an emergency access. The use by pedestrians and cyclists would not generate significant disturbance. The emergency access would only be used if the northern access to the parcel were inaccessible, so would be infrequent and probably of short duration. While the increased level of activity may be noticeable in this relatively tranquil location, given the limited frequency and duration it would not result in unacceptable disturbance.
164. The flank of **54 Luckley Road** (a two-storey 'chalet bungalow' with second-floor accommodation within the roof space) would be 62 metres from the proposed public open space and separated from the site by the railway line, which is elevated at this point rising to a bridge over the road.
165. The closest properties to the west of the site are **24 a & 25-30 Gorrick Square**. 24a is a bungalow, and the remainder are two-storey houses that back onto the railway line. The rear of these properties is at least 21 metres from the railway boundary, the railway corridor provides about 30 metres separation and there is a further 23 metres between the railway and the application site boundary resulting in overall separation distances of over 75 metres, from the proposed public open space. There would be over 100 metres between existing dwellings to the west and the nearest residential parcel (Parcel D, proposed to be a maximum of two-storey). This separation would increase further to the north with separation distance of over 190 metres from the rear of the two-storey houses in **Tangley Drive** to the nearest residential parcels where the parameters would allow

development of up to 2.5 or three-storeys. The Tesco foodstore and car park separate the application site from the nearest dwellings in **Finchampstead Road**.

166. **76 A & 76 B Finchampstead Road** have been acquired by the council to facilitate delivery of the SWDR together with junction capacity works associated with the SDL and planning permission 192928 includes their demolition. Should this not take place, they would be over 240 metres from northern part of Parcel D and the main impact upon them would be from the use of the SWDR.
167. There would be over 80 metres between the boundary of **58 & 58A Finchampstead Road** and the nearest part of the application site - the SWDR where it runs through the site (the impact of which was considered when approving application 192928) - and the nearest residential parcel would be the northern part of Parcel D, over 230 metres away. The parameters would allow development of up to three-storeys (12.5 metres) in this location but the separation would be ample to prevent an adverse impact.
168. **Knoll Farm, Gipsy Lane** lies over 90 metres from the application site and about 110 metres from the nearest residential parcel (C2), an area identified by the building heights parameters plan for development of up to three-storey (12.5 metres). The separation would be ample to prevent an adverse impact.
169. The Reading-Waterloo railway line marks the northern boundary of the SDL and separates the proposed development from residential properties on **Gipsy Lane**. These are a mix of bungalows, two and occasional three-storey dwellings, which back onto the railway corridor. They are relatively elevated as the general trend is for levels to fall from north to south. There would be over 100 metres between the southern boundary of the nearest properties - **141-147, Duxmeer, Odimbo/St Clair, Rowans, Penlan and Satara, Gipsy Lane** – and the nearest residential Parcel (C2). Duxmeer, Odimbo/St Clair, Rowans, Penlan and Satara are unusual in that they have very little separation from the boundary with the railway line (as little as a metre in the case of Rowans) but more substantial amenity areas to the side of the buildings but – taking into account this and that the parameters would allow development of up to three-storeys (12.5 metres) in Parcel C2 - the separation would be sufficient to prevent an adverse impact.
170. **Ludgrove School**, a private boarding school, lies to the southeast of the application site. The south-eastern corner of the site is proposed to be SANG, continuing the SANG to the north of the school, within phase 2b. This informal leisure use would not give rise to disturbance and the closest residential development (parcel C2, over 260 metres from the nearest building within the school complex) would be far enough away to prevent an adverse impact on the school including the associated residential accommodation.
171. **In conclusion**, from the land use and building heights parameters plans that form part of the outline planning application, it is apparent that acceptable separation distances – in line with the guidance in the Borough Design Guide - can be achieved and, subject to the detailed design at the reserved matters stage, the proposed development would not have an adverse impact on existing neighbours.

Neighbouring properties within the development

172. The separation distances referred to in paragraph 153 apply equally to the relationships between dwellings within new development. Since layout is a reserved matter, this cannot be assessed at this stage. However, for outline planning permission to be granted, it is necessary to demonstrate that the amount of development proposed can be satisfactorily accommodated on the site and the application is supported by a Provisional Layout which shows how the site might be developed. These show that acceptable separation distances can be achieved at the density proposed.

Internal space standards

173. MDDLDP policy TB07 *Internal space standards* and Borough Design Guide design principle R17 establish that the size and layout of new homes should be suitable to serve the amenity requirements of future occupiers, although the standards set out in them have now been superseded by the DCLG Technical housing standards – nationally described space standard (March 2015).
174. The design of individual buildings cannot be assessed until the reserved matters stage but the applicant has confirmed that the proposed dwellings would conform to this guidance and that the Provisional Layout is based on policy compliant house types. Hence, it has been demonstrated that internal space standards are capable of being met without giving rise to other issues. Condition 24 refers.

Private amenity space

175. The Borough Design Guide design principle R16 establishes that dwellings should have access to some form of amenity space, preferably in the form of a private or communal garden. To be useable a garden should generally be broadly rectangular, receive sunlight of some of the day, be capable of accommodating a range of activities – sitting, play, clothes drying, storage - and have secure access for cycle storage. While occupants of upper-floor flats rarely have access to a garden, they should be provided with useable private outdoor space in the form of a balcony, roof terrace or winter garden, of at least one metre by three metres. The pandemic has reinforced the need for everyone to have access to private outdoor space.
176. The layout cannot be assessed until the reserved matters stage but the Provisional Layout demonstrates that amenity space standards are capable of being met at the density of development proposed. Condition 23 would ensure retention of appropriate amenity space post-development.

Access and Movement

177. Core Strategy Policies CP1 *Sustainable Development* and CP6 *Managing Travel Demand* require consideration of the travel impacts of development, emphasising the importance of reducing the need to travel, particularly by private car. Supported by CP10 *Improvements to the Strategic Transport Network* and MDDLDP policy CC08 *Safeguarding alignments of the Strategic Transport Network & Road Infrastructure* they require development to make provision for a choice of sustainable forms of transport, delivering improvements to existing road, rail, public transport infrastructure as well as facilities for pedestrians, cyclists and those with reduced mobility. The South Wokingham SPD, Section 4, part 5 in

particular, provides more detailed guidance on the Access and Movement Framework for the SDL.

Accessibility of the site: location

178. Core Strategy Policy CP6 b) directs development to locations which minimise the distance people need to travel and where there are choices of mode of transport available (or will be by the time of development).
179. Core Strategy policy CP9 *Scale and Location of Development Proposals* identifies Wokingham as a Major Development Location (one of the settlements which offer the greatest range of facilities and services as well as allowing residents the greatest choice of modes to access them) and the opportunities for easy access to the town centre were an important factor in designating the South Wokingham SDL as an extension to the existing settlement. The railway line restricts north-south movement to some extent but even the most remote parts of the SDL are within a 2km radius of the Town Centre and subject to the measures set out in the remainder of section 0 of the appraisal, especially 0, 0 & 0, good access can be provided to Wokingham Town Centre and also to destinations in Bracknell and Crowthorne.

The strategic transport network

180. Core Strategy Policy CP10 *Improvements to the Strategic Transport Network* identifies improvements required to ameliorate existing environmental and safety problems and to support new development. These include improvements on the A321 Finchampstead Road corridor and a cross reference to policy CP21 *South Wokingham Strategic Development Location* which requires improvements to transport capacity along the A321 Finchampstead Road and A329 London Road corridors, including a new connection between Coppid Beech Roundabout and Finchampstead Road (the South Wokingham Distributor Road (SWDR)) (MDDL policy CC08 *Safeguarding alignments of the Strategic Transport Network & Road Infrastructure* also refers). These policies also require delivery of measures to improve access by non-car modes which are considered in sections 0, 0 and 0 of this appraisal.
181. The improvements along the Finchampstead Road corridor are among the works to be delivered by the council through CIL funding, together with off-site mitigation at the junction with Molly Millars Lane and pedestrian and cycle improvements secured partly through the IDP and S106's. The highway works are the subject of separate applications (192928 & 203535).
182. The SWDR is a key element of the infrastructure required to support development in the SDL. It will form a continuous new route running broadly east-west through the SDL, south of the existing settlement, connecting the A329 London Road in the northeast to the A321 Finchampstead Road in the southwest. It should fulfil three important functions (Core Strategy paragraph A7.52 d)): to integrate with the existing street network, providing access to the new development as well as allowing dispersal of traffic, thus relieving some pressure on the historic town centre; to function as a corridor for sustainable travel accommodating bus routes (section 0) and catering for safe, comfortable pedestrian and cycle movement (section 0); and to be a street with civic quality, fronted by development serving as a location for commercial and community activity (section 0).

183. The first section of the SWDR – from London Road south to the Reading-Waterloo railway line - has already been delivered as part of the first phase of the SDL, at Montague Park (formerly Buckhurst Farm) and is called William Heelas Way.
184. The second section of the road, often referred to as the “Eastern Gateway” (planning permission 172934), opened in January 2022, extends William Heelas Way southwards, to Waterloo Road. The works included construction of a new bridge over the railway line, a new roundabout at the junction with Waterloo Road and stopping up of Waterloo Road between the existing level crossing and the new roundabout, to facilitate closure of the level crossing by Network Rail (for safety reasons) once the alternative route via between William Heelas Way and Waterloo Road was open.
185. The central section of the SWDR, from Waterloo Road to Finchampstead Road and associated works in the Finchampstead Road corridor (often referred to as the “Western Gateway”) were the subject of a separate planning applications (192928 & 203535) which were approved on 24 May 2021.
186. The S106 would secure land in the applicants’ control for the SWDR (including ecological mitigation and BNG) plus financial contributions for two, small parcels within the SDL boundary which the council has acquired from third parties for construction of the road. Condition 43 would control the number of SDL completions permitted before opening of the full SWDR.

Vehicular access to the site

187. The application is in outline with all matters, including access, reserved. Nevertheless, it is necessary to understand how the movement network within the site would connect into the wider, existing network. The following paragraphs consider vehicular access. Pedestrians and cyclists would share these routes, along with the additional connections described in section 0.
188. Vehicular access to the site is proposed to be via the SWDR (section 0) which will run east-west through the SDL from Finchampstead Road to London Road, crossing both Easthampstead Road and Waterloo Road. The connections between the SWDR with the existing adopted highway network were considered in the report on the SWDR (192928) and phase 2b (191068).
189. The largest, proposed residential parcel D (expected to be around 118 dwellings) would access the SWDR from a new roundabout, east of Tesco (approved as part of the SWDR application). The principles established by the Borough Design Guide and South Wokingham SDL SPD planning support an interconnected network of streets, providing a choice of routes but, in this case, site constraints limit the opportunities for vehicular access: the level of the SWDR and relatively narrow width of the northern edge of parcel D prevent a second connection to the SWDR while the narrow railway bridge on Luckley Road and the importance of maintaining the tranquillity of the southern part of the application site (see section 0) militate against access from the south. (This is also consistent with the SDP Transport and Movement Diagram). However, because the number of dwellings proposed in the parcel is over 100, it is necessary to provide an emergency access, as an alternative should the usual access be temporarily unusable (*Living Streets A Highways Guide for Developers in Wokingham* 2019). A footpath/cycleway is proposed to run north-south through the public open space to

the south of Parcel D, connecting it to Luckley Road. This would be a minimum of 3.7 metres wide and specified to facilitate occasional use by emergency vehicles or other traffic needing to access the parcel during an emergency, whilst avoiding detracting from the character of the open space or the setting of the grade I listed Lucas Hospital. Swept path analysis confirms that a safe connection could be made to the lane to the south and, while the necessary land is not all within the application site or adopted highway, the applicant has provided confirmation that the land is either within their landowner's control or unregistered. The landowner has had 40 years continuous, unchallenged use of the unregistered land and the applicant would insure against any future claim. Since access is a reserved matter, there is sufficient certainty to secure this access by means of a Grampian condition (condition **Error! Reference source not found.**). (The railway bridge can accommodate an emergency vehicle but not two-way traffic).

190. Priority junctions onto the SWDR would provide access to the smaller residential parcels, C1 and C2 (expected to be around 24 and 29 dwellings respectively). These accesses have also been approved as part of the SWDR application.
191. The proposed access arrangements are consistent with what has been approved under the SWDR application (192928) as well as the SPD Transport and Movement Diagram and are acceptable, subject to detailed design; secured by conditions **Error! Reference source not found.**, 47 & 53.

The transport network within the SDL

192. Core Strategy policies CP1 *Sustainable Development* and CP3 *General Principles for Development* are supportive of high quality design, requiring schemes to be functional, accessible, safe, secure and adaptable. The layout of development and how it facilitates movement is an important aspect of this and Core Strategy policy CP6 *Managing Travel Demand* expects development to improve the movement infrastructure network for all modes of transport.
193. These development plan policy requirements are amplified by the Borough Design Guide (Design Principles G3, R3, R4, S5, S6, S8, S10 & S11) and the South Wokingham SPD (Design Principles 2a, 5a and 5b in particular). Proposals should link into and strengthen the existing network of streets and spaces to create an inter-connected network with a clear hierarchy of streets (in terms of function and character). The South Wokingham SPD establishes a hierarchy of routes ranging from pedestrian and cycle routes, through mews lanes, tertiary streets and secondary streets to primary streets (the SWDR), to respond to different travel needs and allow easy and efficient movement, which should be organised around a robust and traditional pattern of streets and blocks and provide a clear distinction between public and private areas. The network should provide a choice of easy to navigate, safe, attractive, inclusive and convenient routes to neighbouring areas and to local destinations – these include schools, local shops and other services, public transport stops and open spaces - for all to use and for all modes but with a particular emphasis on promoting walking, cycling and public transport. The design of streets must meet the technical requirements for road layout but these requirements must not become dominant and streets should contribute to the character, identity and environmental quality of an area. The landscape design of the street should be governed by a detailed street design strategy, which should be prepared by the developer in advance of the Reserved Matters stage as part of a design code or design statement (condition 4).

194. Although site constraints limit the number of connections to the existing highway network that are achievable for motor vehicles (see paragraph 189), the Movement & Access Parameters Plan establishes a hierarchy of streets which reflects the SPD, described as major and minor access roads, shared surfaces and private drives and the DAS acknowledges the need to combine movement and place functions. Better connectivity can be achieved for pedestrians and cyclists due to the link from parcel D south to Luckley Road, the links from all three parcels to the SWDR, existing PRow and the proposed footpath network within the SANG.
195. While the SWDR is the subject of a separate planning application, its functions include providing access to development in the SDL and connecting the neighbourhoods and, as such its design is integral to the masterplanning of the development on either side. Consistent with the SPD and the character already been established by the first phase of the SWDR, William Heelas Way, the SWDR application proposes a tree lined street with three metre wide verges on either side, separating pedestrians and cyclists from other traffic. Secondary streets are intended to provide local access with a lower level of traffic movement and emphasis on pedestrians and cyclists. Footpaths should be provided on both sides and street trees should on at least one side, on-street parking may be provided in designated bays. Tertiary and shared surface streets are intended to provide access and have a more informal layout intended to give equal priority to pedestrians and slow moving vehicles.
196. It is of particular importance that this aspect of the design is considered comprehensively and condition 4 requires approval of a more detailed masterplan and design code (also including consideration of connectivity to the movement network within other parts of the SDL) before submission of the first reserved matters. Subject to this, an appropriate movement network could be delivered.

Pedestrian and cycle network

197. Core Strategy policy CP1 *Sustainable Development* establishes an overarching aim of reducing the need to travel by car. This can only be achieved by providing for alternative, sustainable forms of transport to allow choice: Core Strategy policies CP6 *Managing Travel Demand* and CP10 *Improvements to the Strategic Transport Network* both require improvements to pedestrian and cycle networks to improve access to services and facilities and increase use of cycling. The guidance in the Borough Design Guide and South Wokingham SPD's reinforce the importance of good connections to local destinations (paragraph 193) and Core Strategy policy CP21 *South Wokingham Strategic Development Location* refers to more site specific improvements: pedestrian links between Wokingham town centre and the countryside (including for those using mobility aids); improved access by non-car modes along the A321 and A329 corridors and to Wokingham Town centre (including the station interchange).
198. MDDL policy CC03 *Green Infrastructure, Trees and Landscaping* also seeks to promote accessibility for pedestrian and cyclists, with a specific focus on permeability between and within green corridors including public rights of way such as footpaths, cycleways and bridleways and identifies a specific aim of establishing a riverside footpath and cycleway along the Emm Brook.

199. SPD Design Principle 2a(ii) requires the layout of residential areas within the SDL to provide direct, easy, safe access to the neighbourhood centres, with first priority given to the safety, comfort and convenience of pedestrians followed by cyclists and public transport. The Movement and Access Parameters Plan and Provisional Layout shows the proposed network of routes and how it would tie into the wider network within the SDL and beyond - including Public Rights of Way (PRoW) - providing for both sustainable travel and recreational use. Integration with the PRoW network is considered in more detail in section 0. Further detail of the on-site sections would be secured by conditions the detailed masterplan and design code (conditions 4, 15, 38 & 39). At that stage connections between residential parcels and the footpath network would need to be identified (taking into consideration other factors such as the impact on trees and hedgerows). Condition 40 refers.
200. The Pedestrian and Cycle Strategy (developed by the consortium in collaboration with the applicant) identifies improvements along a number of off-site routes between the SDL and destinations in Wokingham including the town centre, Molly Millars Lane employment area and St Crispin's: the proposals include a pedestrian and cycle route from the application site (south of parcel D), along Luckley Road and Tangle Drive to Finchampstead Road; works along the Finchampstead Road corridor between Tangle Drive and the Carnival Pool roundabout; and a pedestrian and cycle path between the SWDR and Star Lane level crossing where Network Rail is intending to carry out safety improvements. Also a pedestrian and cycle path is proposed along Peacock Lane to provide connectivity between the eastern SDL and destinations in Bracknell. An off-road foot and cycle path would be provided through the SANGs (see paragraph 217). These works are identified in the IDP and would be secured either by conditions 15 & 46 or S106.
201. The key routes south, towards Crowthorne - Easthampstead Road, Heathlands Road and Honey Hill – are rural in character, with roadside ditches and vegetation. To provide footpaths along these routes would require significant widening and consequently drainage works, loss of trees and hedgerows, which would cause significant harm to the rural character of the area and ecology. However, there is an intention to upgrade Wokingham Without Byway 28 to a Greenway (see paragraph 205).
202. In addition to the sustainable travel measures outlined in sections 0 & 0 the S106 would secure contributions of £520.00 per dwelling towards My Journey, the borough-wide active and sustainable travel service.

Public Rights of Way (PRoW) and Greenways

203. The existing Public Right of Way (PRoW) network passing through the SDL south of the railway consists of two broadly north-south routes which form part of the wider network connecting Wokingham town with Gorrick Planation to the south and Crowthorne beyond. These are Wokingham/Wokingham Without Footpath 25 immediately east of the Reading-Guildford railway line and Wokingham Without Footpath 10 further to the east, connected by Wokingham Footpath 24, Wokingham Without Footpaths 9 and Wokingham Footpath 17. To the west of the SDL (within phase 2b), Wokingham Without Footpath 5 and Wokingham Without Byway 30 link Easthampstead Road and Waterloo Road.

204. Changes to the PRow network required to facilitate delivery of the SWDR were considered as part of the assessment of application 192928, together with the provision of appropriate crossing facilities where the road crosses Footpaths 10, 9/24 & 25.
205. As well as the changes to the routes of the existing PRow, described in section 0, the proposed development would change the setting and, therefore, the character of the PRow and increase use of them. The change in character would be particularly marked where Wokingham Footpaths 25 and 24 run through residential parcel D.
206. Consistent with the aim of enhancing links between Wokingham town centre and the surrounding countryside, Core Strategy Appendix A7.47 establishes that pedestrian and cycle movement should be supported by a network of **Greenways**, extending beyond the site to public green space and footpaths leading to Gorrick Plantation and Queens Mere. The council has since developed a Greenways Strategy and Implementation Plan (an appendix to the Rights of Way Improvement Plan 2020/2030) and Route D (Arborfield SDL-Barkham-South Wokingham SDL-Wokingham) runs through the SDL broadly on the route of the existing Footpaths 25-24-9-17, providing a pedestrian and cycle connection between Wokingham and Crowthorne, via the SDL. The Greenways Strategy establishes that these will generally be traffic free routes comprising a three metre wide path with a hard, permeable, surface suitable for commuting and leisure use by pedestrians, pedal cyclists, dog walkers, wheelchairs and mobility scooters in all weathers. (This specification is in line with the relevant standards for the expected nature and volume of traffic.)
207. Sections of the Greenways within the SDLs are to be delivered as part of the SDL development and are included in the IDP, to be secured by condition/S106; sections outside the SDLs are to be delivered by the council from CIL.
208. Surface improvements to Wokingham Without Byway 28, south of the SDL, in preparation for Greenway Route D are due to be delivered this year as a joint project between Wokingham Without Parish Council and the borough council.
209. The proposed route of the Greenway through the application site is north from byway 28, through the public open space in parcel D (see paragraph 189), continuing along minor access roads (partly on the alignment of Footpath 25) before turning north-east on the route of Wokingham Footpath 24 and Wokingham Without Footpath 9 to the Gipsy Lane Footbridge over the railway line. Where the route runs along new residential streets, the parameters and Provisional Layout establish a three metre wide footpath/cycleway would be provided, which are also likely to be quiet enough to allow cyclists to safely use the carriageway (condition 4 refers). Where the route runs through open space, a three metre wide flexipave, flexistone or equivalent path is proposed (conditions 15f), 15h), 38 & 44.b) refer). A replacement for the existing, substandard footbridge over the Emm Brook on the route of Footpaths 24 and 9 and associated diversions have already been approved (planning permission 192928). Funding for it is to be secured through the IDP & S106 (see section 0).
210. The report on the SWDR explained that, although the new bridge and the path under the road bridge would be accessible for the majority of the time, there may

be a few days each year when they would not be, due to flooding. An alternative route via the SWDR and the northern parcel within Phase 3 (parcel C2) would allow the route to remain accessible on these occasions (conditions 15j) & 39 refer and the S106 would secure rights of access, should the route not be over adopted highway or through adopted public open space; see section 0 iv)).

211. The character and function of the **northern section of Footpath 25** (Wokingham FP25 II, north of FP24) and **Wokingham Without footpath 24**, will inevitably change where they run through residential parcel D. These routes will be used for day-to-day walking and cycling journeys as well as longer recreational walks. The Crime Prevention Design Advisor highlighted the need for footpaths to be straight enough to provide clear sightlines, well-lit where they are intended to be used 24 hours a day and well surveilled, with defensive space separating them from dwellings. The parameters and Provisional Layout establish that, subject to detailed design, these minor access roads are capable of being designed to prioritise safe movement by pedestrians and cyclists, and landscaped to provide a pleasant, albeit more urban environment. Condition 4 refers.
212. The **central section of Footpath 25** (south of the junction with Footpath 24) would run through a landscaped area and then parallel to a shared surface, minor access road. The treatment of the interface between the shared surface access and the Public Right of Way will need careful consideration to ensure an efficient, functional and attractive layout. Condition 15 i) refers.
213. The **southern parts of Footpath 25** (Wokingham Without Footpath 25 and Wokingham Footpath 25I) lie just outside the application site, running along the Ludgrove School access before turning north and joining the lane that provides access to Chapel Green. The verdant, rural character of the lane is important to the setting of Lucas Hospital (section 0). Given the existing surfaces along this route and alternative Greenway route proposed (paragraph 209) no improvements are necessary to the southern part of this path. Use by cyclists could be discouraged through the design and signage of the Greenway.
214. Wokingham **Without Footpath 10** is contiguous with the eastern boundary of the application site but within Phase 2b. The southern section of Footpath 10 would be located in SANG, so retain a relatively rural setting and the northern section would form a green corridor between two residential parcels. These footpaths form an integral part of the network between Wokingham and Crowthorne, linking the two settlements as well as providing a link in circular walking routes and would be increasingly well used as a pedestrian and cycle link to the town centre. Upgrading of Footpath 10 to a three metre wide, flexipave, flexistone or equivalent surface is a condition of planning permission 191068 (phase 2b).
215. The IDP includes improvements to the specification of the PRoWs to make them fit for more intensive future use, to be secured by conditions 15 & 38 & 39 and the S106 would secure funding for the replacement footbridge and permissive rights for use by cyclists.
216. MDDL policy CC03 *Green Infrastructure, Trees and Landscaping* aspires to establish a riverside footpath and cycleway along the Emm Brook. The footpath network through the proposed SANG would deliver the section through the application site and a connection under the Emm Brook road bridge to the land north of the SWDR (within the SWDR application site).

217. The Rights of Way Improvement Plan (RoWIP) (2020) contains aspirations to formalise the permissive east-west route along the existing Ludgrove access, between Easthampstead Road and Luckley Road, as a restricted byway or bridleway and to upgrade Wokingham Without Footpath 10 to a bridleway. The proposals for Phase 2b make provision for an east-west route for pedestrians and cyclists through the Holme Park SANG, which would continue north-west through the proposed SANG to the SWDR. To fulfil this function the path should be at least three metres wide with a flexipave, flexistone or equivalent finish. Condition 15 g) refers.

Public Right of Way diversions, extinguishment & permissive use by cyclists

218. On 18 May 2021 the Planning Committee resolved to make orders for a number of PRoW diversions required to facilitate the comprehensive delivery SDL development south of the Railway as a whole: extinguishment the section of Wokingham Footpath 25 between the SWDR and Finchampstead Road (it would be fragmented by the SWDR which would also provide an alternative pedestrian route); diversion of Wokingham Footpaths 9 and 24 at their intersection with the Emmbrook (to provide an improved footbridge crossing); extinguishment of the section of Wokingham Without Footpath 10 where the SWDR crosses it (to incorporate pedestrian and cycle crossing facilities); and diversion of Wokingham Without Footpath 5 at either end (to accommodate the new junctions on Waterloo Road – approved under 172934 – and on Easthampstead Road).
219. Given the changing function of these routes, the status of Wokingham Without Footpath 10 and Wokingham Footpaths 9, 17 & 24 would need to be revised to permit cycle use: the S106 for phase 2b will secure permissive use of Footpaths 10 & 17 by cyclists and the current application would secure the same for Footpaths 9 and 24.
220. The Movement and Access parameters plan and Provisional Layout suggest additional diversions at the junction of Wokingham Footpaths 24 & 25 which would also be subject to a diversion, or if the new route would be part of the adopted highway potentially extinguishment (not yet secured.)

Public transport

221. Core Strategy policies CP6 *Managing Travel Demand*, CP10 *Improvements to the Strategic Transport Network* and SPD Design Principle 5c require development to provide for sustainable forms of transport, including delivery of public transport infrastructure and improvements to the quality and frequency of public transport services. One of the three functions of the SWDR identified in Core Strategy A7.42 D is as a corridor for sustainable travel including by bus. Policy CP21 *South Wokingham Strategic Development Location* also identifies the need to improve accessibility by non-car modes along the A321 and A329 corridors and to Wokingham Town Centre (including the station interchange).
222. Following discussions with operators there may be an opportunity to extend the existing Courtney's 108 bus service (currently a partly subsidised, 40-minute service between Bracknell Town centre and Jennett's Park). The developers' Public Transport Strategy currently proposes a 30-minute service (for the main part of the day, Monday to Saturday, hourly on a Sunday) connecting Wokingham

and Bracknell town centres (including both railway stations) via Jennett's Park and the SDL. The S106 would secure a proportionate contribution towards implementation of this (or an alternative should circumstances change in the meantime) and condition 41 would secure an interim strategy. The S106 would also secure proportionate contributions towards bus stops along the route together with additional bus stops in Wokingham Town Centre and on the route towards Bracknell, required to facilitate the revised service.

223. Accordingly, the SWDR (application 192928) has been designed to accommodate buses and includes five pairs of bus stops, located to be convenient to the neighbourhood centre and to residential areas. The majority of the new homes within the application site would be within 400 metres walk of a bus stop.

Traffic generation and mitigation of off-site transport effects

224. Core Strategy policy CP6 *Managing Travel Demand* establishes that development should not cause highway or traffic related environmental problems: any adverse effects upon the local and strategic transport network arising from development should be mitigated, road safety should be enhanced and development should not lead to highway problems or traffic related environmental problems.
225. The application is accompanied by a Transport Assessment, which uses the Wokingham Strategic Transport Model (WSTM4) to forecast the combined impact of development in the South Wokingham SDL and elsewhere, plus background growth for various scenarios including 2026 without development (for comparison purposes); 2026 with a realistic assumption of the full SWDR, 450 dwellings within phase 2 and 192 in Persimmon¹⁰ being complete; 2026 with an assumption of the full SWDR and full SDL development; and 2036 with the full SWDR and the full SDL development being complete. WSTM4 is a fully validated model in line with Department for Transport WebTAG guidance. Data from the strategic model is further validated by local traffic surveys, to ensure that the local junction modelling work carried out for the planning application is robust. This modelling has also informed the noise and air quality assessments which are considered in sections 0 & 0 of this appraisal and the HRA considered in section 0 .
226. While development in the SDL and elsewhere would generate additional traffic, the SWDR would provide an alternative route resulting in redistribution of traffic so, in some locations, a reduction may be experienced despite an overall increase in the amount of traffic on the network. The modelling has identified a number of junctions which are likely to require improvement to allow them to continue to function satisfactorily in future. These are:
- a) Barkham Road/Barkham Street;
 - b) Bearwood Road/ Barkham Road
 - c) Barkham Road/Molly Millars Lane;
 - d) Easthampstead Road/Heathlands Road;
 - e) Molly Millars Lane/Finchampstead Road;
 - f) Finchampstead Road/Oakey Drive/SWDR;

¹⁰ This number was based on proposals at the time the modelling took and has since been reduced.

- g) Peacock Lane/Waterloo Road/Old Wokingham Road¹¹ (to be secured by S106 contribution);
- h) Peacock Lane/Vigar Way¹²; and
- i) B3430 Nine Mile Ride/Heathlands Road (to be secured by S106 contribution).

Phased capacity improvements at these junctions would be secured by conditions 3 & 45 or, in the case of g) & i), by S106. These conditions would also secure coordinated delivery of new junctions within the SDL but outside the application site. A longstop date for the highway mitigation works would be included in the S106 agreement.

- 227. The modelling shows a reduction in movements at the Easthampstead Road/Old Wokingham Road junction, so no justification for mitigation. It also indicates capacity issues at the junction of the B3430 Nine Mile Ride and Honey Hill but any increase in capacity at the junction would draw additional traffic down Honey Hill which is unsuitable for increased use.
- 228. Construction traffic would be managed through a Construction Environmental Management Plan (CEMP) (Condition 33).

Intermediate railway station & public transport interchange

- 229. Core Strategy policy CP6 *Managing Travel Demand* requires improvements to the existing infrastructure network, including rail and Appendix 7 (paragraphs A7.42 f) & g)) suggests land should be safeguarded for future provision of an intermediate railway station at South Wokingham, together with a public transport interchange. This was subject to subsequent discussions with Network Rail (and the Department of Transport). It transpired a station would not be deliverable and, consequently, this element of the proposals has not been progressed. However, the proposed bus service (see section 0) would provide a regular service to both Wokingham and Bracknell railway stations.

Residential car and cycle parking

- 230. Core Strategy policy CP6 *Managing Travel Demand* and MDDLDP policy CC07 *Parking* require appropriate vehicle parking, in line with the council's standards (MDDLDP Appendix 2). For residential developments, demand for unallocated car parking is calculated, depending on the location (the SDL is categorised as 'urban'), size and tenure of property and the amount of unallocated parking (with garages counting as half a space).
- 231. Although the application is in outline and details of parking provision would not be fixed until the reserved matters stage it is necessary to establish an approach to parking provision in order to demonstrate that an appropriate amount of car parking can be provided and integrated into the development without harm to the character and amenities of the area.

¹¹ Within Bracknell Forest Borough

¹² Within Bracknell Forest Borough

232. Section 3 of the South Wokingham SPD (Design Principles 3a & e in particular) requires a comprehensive strategy for vehicle parking (cars, motorcycles and bicycles), which should be an integral part of the scheme to limit the impact on visual and residential amenity. Different approaches are recommended for each character area but parking should generally be on plot, in mews lanes or on-street, designed to be integral to the street. Parking courts should generally be avoided but may be necessary for apartment buildings and along the SWDR. This is consistent with the design principles in the Borough Design Guide (P1-3) which require parking to be safe and convenient without dominating the street scene, among other things.
233. The applicant's Design and Access Statement references the relevant parking standards and proposes a variety of provision including on-plot, courtyard and on-street parking. Allocated, on-plot parking would generally be located to the side of dwellings within parking bays or garages set back from the building line to allow ease of access to rear gardens and reduce the prominence of cars within the streetscape. Where courtyard parking is proposed it would serve a limited number of units and be conveniently located for users in a private, well-defined area with a single point of access and natural surveillance, to feel safe, be secure and promote use. (This is consistent with the Crime Prevention Design Advisor's advice that, where communal parking is unavoidable it should be provided in small groups with active surveillance from the dwellings and appropriate lighting to BS5489:2013.) Where unallocated visitor parking is provided on street, it would be in small groups, not exceeding five spaces without landscape intervention.
234. The provisional layout demonstrate that an appropriate quantum of parking could be achieved overall. Further details of the approach to parking delivery, the contribution it could make to differentiating the different character areas and how it could be incorporated into residential layouts without encroaching on adjacent green infrastructure (within and adjacent to the residential parcels) would be secured by conditions 4 & 49.
235. The amount of cycle parking also depends on the size of dwelling (MDDL Appendix 2). The Transport Assessment Addendum indicates provision of cycle storage in accordance with the Council's standards, within the curtilage of houses or a secure communal store for flats.
236. Cycle storage should be considered at an early stage of in design process to ensure that it is conveniently located and integrated into the development, rather than being provided as an afterthought (SPD Design Principle 3e(iv)) and Borough Design Guide Design principle R20). Larger garages (a 3 x 7 metre garages) can accommodate two bicycles as well as a parked car. Where sheds are used direct access to the garden should be possible (balanced with achieving a secure layout and sufficient private amenity space). (The Provision Layout demonstrates that - with refinement - this could be achieved). Drives should be at least 3.1 metres wide to allow a cycle to be pushed past a parked vehicle. Cycle storage for flats should either be within the building or in a purpose built structure, located where it would not encroach on amenity areas. Hence, details of cycle parking should be considered in the Design Code and at the reserved matters stage (conditions 4 & 52 and informative 14 refer).

Electric charging

237. The guidance in *Living Streets: a Highways Guide for Developers in Wokingham* (2019) anticipates a combination of 'passive' electric vehicle charging provision, which is a wired-in system that can be readily converted later; and 'active' provision, which includes a direct charging point ready for use. The current minimum standard is 35% passive, 5% active for on-plot parking and 25% passive, 5% active off-plot although this will rise from 2026.
238. The need to accord with these standards is acknowledged in the applicant's Transport Assessment Addendum. EV charging is also mentioned in the ES Air Quality chapter which recommends EV charging points for each on-plot parking space should be made where feasible and at least 1 Electric Vehicle (EV) rapid charge point per 10 residential dwellings for communal parking.
239. Condition 55 would secure provision in line with the council's policies, although it is worth noting that new government standards – expected to be announced soon – may well supersede current standards and transfer this matter to Building Regulations.

Flooding and drainage

240. The NPPF and National Planning Practice Guidance establish a framework for assessing the probability of flooding and the suitability of land for different uses, depending on their vulnerability to flooding. Core Strategy Policy CP1 *Sustainable Development* (and Appendix 7 A7.46 & A7.53) and MDDL Policy CC09 *Development and Flood Risk (from all sources)* follow the sequential approach established by the NPPF, directing development away from the areas at highest risk of flooding (from any source). Furthermore, development should incorporate Sustainable Drainage Systems (SuDS) to provide adequate drainage; avoid increasing - and where possible reduce - the risk of flooding, on the site and elsewhere; and limit adverse effects on water quality (including ground water). Climate change allowances were updated in July 2021 but the EA have advised that the previous (higher) standards will continue to applications submitted prior to that date. Accordingly, flood modelling and drainage systems should be designed to accommodate a 1 in 100 year flood event plus an allowance for climate change: 40% surface water (pluvial) flooding and 70% for fluvial flooding.
241. The South Wokingham SDL is allocated for development in the Core Strategy which was subject of a Strategic Flood Risk Assessment (SFRA). Furthermore, application is supported by a Flood Risk Assessment (FRA); required because the site area is more than one hectare and parts of it fall within Flood Zones 2 and 3 (where the probability of flooding is medium or high).

Risk of flooding

242. The FRA considers existing flood extents and how these would change as a result of construction of the SWDR and associated works as well as the development itself. (The bespoke hydraulic model that was prepared to support the SWDR application, 192928, has been submitted in support of this application and the EA flood risk maps are likely to be updated to reflect this in future).
243. The majority of the application site area falls within Flood Zone 1 where the probability of fluvial flooding is low (less than 0.1% annual probability/1 in 1,000)

and would remain so, following construction of the SWDR. However, the Emm Brook (a Main River, which flows northwest through the proposed SANG) and its tributaries traverse the site and land within the river valleys there is a risk of fluvial flooding: this includes land in Flood Zone 2 where the probability of flooding is medium (between 0.1% and 1% annual probability/1 in 1,000 and 1 in 100) and Flood Zone 3, including the functional floodplain (Zone 3b, where water flows or is stored in time of flood) and the probability of flooding is high (greater 1%/1 in 100). The overland flow routes that would result from surface water flooding (when excess rainwater cannot infiltrate the ground and is not intercepted) follow a similar pattern.

244. A comparison of the Baseline and Post Development (with mitigation) model results indicate that flood levels and extents are increased within the SANG area, as designed. There is also a slight increase in flood extent and levels immediately upstream of the proposed PROW footbridge and road bridge over the Emm Brook (Main River). However, there is no exacerbation of flood risk to existing properties in the vicinity. Overall, the mitigation measures would provide a net benefit in terms of reducing fluvial flood risk extents and flood levels downstream and ensuring that flood risk upstream remains unaffected. The results also indicate that the risk of flooding for existing properties within the study area would not change.

Vulnerability of the proposed uses

245. Residential uses are classified as 'more vulnerable' uses, which are appropriate in Flood Zones 1 & 2. SANG and other open space are 'water compatible' and are appropriate in any location.

The sequential test

246. Consistent with the approach anticipated by the Core Strategy and South Wokingham SPD, the landscape and green infrastructure strategies identify the land along the Emmbrook corridor as open space for outdoor recreation, nature conservation and biodiversity. These water compatible uses are acceptable in all zones including the functional floodplain, providing they can remain operational and safe for users in times of flood (see paragraph 209 and condition 39); do not result in a net loss of floodplain storage; and do not impede water flows and not increase flood risk elsewhere.
247. The residential parcels would be within Flood Zone 1 where all uses are appropriate (based on both the current EA flood maps and the post-SWDR mitigation and residual flood zones).
248. Thus all more vulnerable and less vulnerable uses would be located outside the areas at risk of flooding and only water compatible uses would take place within Flood Zones 2 and 3. Furthermore, the development parcels would be accessed from the SWDR, which has been designed to be above any predicted flood levels (fluvial and surface) for all events up to the 1 in 1,000 year event and/or the 1 in 100 year event with a 70% climate change allowance for peak river flows. The emergency access (see section 0) would also be through land in Flood Zone 1. Hence, the sequential test would be met.

Sustainable Drainage (SuDS) strategy

249. MDDL Policy CC10 *Sustainable Drainage* requires surface water to be managed in a sustainable manner, maintaining greenfield run-off rates and volumes, taking into account climate change. This is reinforced by policy CC03 *Green Infrastructure, Trees and Landscaping* which expects green infrastructure improvements within the River Valleys to help minimise flood risk.
250. Part H of the Building Regulations establishes a hierarchy for surface water disposal based on a SuDS approach: discharge should first be into the ground, then a surface water body, followed by a surface water drainage system or finally a combined sewer. It is proposed to follow this approach testing infiltration and the level of the water table to establish whether infiltration is feasible. Other SuDS features are likely to include attenuation basins (storing water and releasing it slowly); swales; and wetland corridors (within SANG), ultimately discharging to the Emm Brook at a rate equivalent to Greenfield runoff rates. Conditions 26- 28 refer.
251. As advocated by the MDDL (para 2.76) the applicants have worked with the SWDR application team to deliver a coherent approach to surface water management. In addition to swales and filter trenches in the verges alongside the SWDR, basins are proposed to be incorporated in the wider development. Overall the development would maintain greenfield run-off rates and improve drainage by holding water on the site, thus slowing discharge.
252. Incorporation of SuDS within the development parcels and the green infrastructure network is consistent with the approach envisaged by planning policy. Conditions 25-32 would ensure provision of appropriate drainage and condition 15 would ensure that SuDS features are appropriately incorporated in the wider landscape to support recreational use and biodiversity as well as fulfilling their drainage function.

Environmental Health

Noise

253. Core Strategy Policy CP1 *Sustainable Development* seeks to avoid development in areas where noise may impact on the amenity of future occupants and MDDL Policy CC06 *Noise* reinforces this, requiring proposals to demonstrate how noise impacts on sensitive receptors (both existing and proposed) have been addressed. Where there is no adverse impact noise would not be a material consideration. Where a significant effect could arise, a sequential approach should be taken first reviewing the layout of the site, then the internal layout of buildings and finally physical mitigation measures such as barriers and mechanical ventilation. Where there is still a significant adverse impact planning permission would normally be refused.
254. The Environmental Statement includes an assessment of the potential impact of both construction and operational noise on sensitive receptors within and close to the site.

Construction noise

255. Construction traffic is unlikely to generate a significant increase in noise levels on the local road network. Given the distance between the site and the nearest noise

sensitive properties, it is unlikely that noise and vibration from operations on site would have an adverse effect and any impact that did occur could be mitigated by application of the best practice measures identified in the Environmental Statement; condition 33 would secure appropriate measures.

Operational noise

256. Existing levels of road traffic and railway noise have been assessed together with potential changes following development.
257. The proposed development would result in negligible change in road traffic noise **outside the SDL**.
258. Acceptable daytime and night-time **internal noise levels** would be achieved in the majority of the proposed dwellings without mitigation (other than standard double glazing), due a combination of the distance from the road and shielding by other buildings. However, for a small proportion of dwellings nearest to the SWDR, additional mitigation is likely to be required to achieve acceptable internal noise levels. This could include a combination of planning the internal layout so habitable rooms are located away from noise sources (to be balanced with other considerations such as achieving active frontages), use of suitably-specified acoustic glazing products and alternative means of ventilation (such as acoustic trickle vents or mechanical ventilation). Where desirable internal noise levels can only be achieved with windows closed, they should remain openable for purge or rapid ventilation. With suitable mitigation appropriate internal noise levels could be achieved and condition 35 would require submission of Noise Mitigation Plan at the reserved matters stage detailing how this would be achieved.
259. Providing the layout of development is in accordance with the land use parameters and urban design principles established by this application, buildings would generally be sited between noise sources and **private amenity areas**, protecting them from road noise to some extent. Acceptable noise levels are predicted to be achieved in the majority of external private amenity areas (gardens) without the need for mitigation but there are exceptions, on the northern part of the site, adjacent to the SWDR. Noise levels could be reduced through careful consideration of the siting and spacing of buildings (dwellings and outbuildings such as garages) and boundary treatments (of at least 1.8 metres in height) at the reserved matters stage (Condition 35 refers). Furthermore, BS8233 acknowledges that external noise levels need to be balance against the benefits of development near to transport sources.

Air quality

260. Core Strategy policy CP1 *Sustainable Development* establishes that development should minimise the emission of pollutants into the wider environment. The Environmental Statement includes an assessment of the impact on air quality including nitrogen dioxide and PM₁₀.
261. Dust arising from **construction** and earthworks has the potential to cause a 'nuisance' if not properly controlled but the Environmental Statement identifies best practice measures, demonstrating that this nuisance can be adequately controlled. Condition 33 would secure implementation of these measures.

262. The Environmental Statement also includes an assessment of the **operational impact** of the development on air quality, taking into account changes in traffic flow on roads potentially affected by the development. Modelling for the site concludes that air pollution is not likely to exceed health objective levels for nitrogen dioxide or PM₁₀ and it is not predicted to cause any worsening of local air quality: no mitigation is required. Nevertheless, implementation sustainable travel initiatives and provision of charging points for electric vehicles could make a positive contribution to improving local air quality. Section 0 refers.

Contamination

263. Core Strategy policy CP1 *Sustainable Development* requires development requires development to minimise the emission of pollutants, limit any adverse effects on water quality (including ground water) and avoid areas where pollution may impact upon the amenity of future occupants.
264. A phase 1 desk study has been carried out which identifies a plausible soil and gas risk: a further (phase 2) intrusive site investigation is necessary to confirm whether there is any contamination and, in the event that contamination is found, to secure a remediation strategy - setting out mitigation measures, testing and verification - to make the site suitable for use. This would address protection of human health, controlled waters, structures and the surrounding ecological environment. Condition 36 refers.

Sustainable design and construction

265. Core Strategy policy CP1 *Sustainable Development*, amplified by MDDL policy CC05 *Renewable energy and decentralised energy networks* requires development to contribute towards the goal of zero-carbon development by minimising energy consumption and incorporating on-site renewable energy features: at least a 10% reduction in carbon emissions should be achieved through renewable energy or low carbon technology.
266. Building Regulations Part L (conservation of fuel and power) currently require new residential development to achieve a 6% CO₂ reduction compared to the equivalent 2010 standards. However, these standards and Part F (ventilation) are under review and the current government proposes a steppingstone approach which would require new homes in 2021 to produce 31% less carbon dioxide emissions compared to current standards. The aim is to reduce carbon emissions and improving energy efficiency without stifling growth.
267. It is proposed to submit a detailed Energy Statement, based on a Standard Assessment Procedure (SAP) assessment, at the reserved matters stage, to demonstrate that minimum Building Regulation standards would be met and a 10% reduction in carbon emissions through renewable energy provision within the development compared with Part L Building Regulations baseline in accordance with Policy CC05 as part of the detailed design. This is likely to be achieved through use of solar solutions, given the constraints on alternative renewable/low-carbon solutions as recognised within the South Wokingham SDL SPD. Condition 58 refers.

Water consumption

268. The Environment Agency has identified the Thames region as an area of Water Stress and Core Strategy policy CP1 *Sustainable Development* requires development to reduce water consumption.
269. It is proposed that new dwellings would be designed to the building regulations optional higher water efficiency standards of 110 litres/person/day as defined by national Planning Policy Guidance. Condition 58 refers.

Recycling and refuse storage

270. Core Strategy policy CP1 *Sustainable Development*, Appendix A7.46 and MDDLDP policy CC04 *Sustainable Design and Construction*, amplified by the Sustainable Design and construction SPD and Borough Design Guide design principal R20 require early consideration of how provision for waste sorting storage (internal and external), including on-site recycling and collection can be incorporated in new development.
271. The Planning Statement indicates that reserved matters would incorporate refuse and recycling facilities to meet standards (within the curtilage of each dwelling or communal stores) and that the layout could accommodate suitable access for refuse vehicles to within recommended carry distances: condition 6 would secure this and informative 15 directs the applicant to the council's guidance.

Employment skills plan (ESP)

272. MDDLDP Policy TB12 *Employment Skills Plan* indicates that proposals for major development should be accompanied by an Employment and Skills Plan to show how the proposal accords opportunities for training, apprenticeship or other vocational initiatives to develop local employability skills required by developers, contractors or end users of the proposal.
273. The Socio-economic Chapter of the ES anticipates in the region of 39 direct construction jobs on-site could be supported per annum during the estimated four-year build period, construction costs are estimated at approximately £29million over the build programme (at current prices) the Proposed Development could generate an additional £8million of gross value added (GVA) per annum or £30.5 million during the four-year construction period. It is estimated that the proposed scheme would provide homes for 241 economically active residents supporting additional expenditure.
274. Based on the projected value and Construction Industry Training Board (CITB) benchmarks, the ESP should secure 13 community skills support places, eight apprenticeship starts and creation of eight jobs. However, this calculation could be refined once detailed proposals are available.
275. The S106 would secure an Employment Skills Plan or alternatively a financial contribution of £60,000 to enable equivalent delivery by the council.

Infrastructure impact mitigation

276. In accordance with Core Strategy policy CP4 *Infrastructure Requirements* infrastructure, services, community and other facilities should be improved to meet

the requirements of new development, taking into account cumulative impact. Specific requirements for the South Wokingham SDL are set out in Core Strategy policy CP21 *South Wokingham Strategic Development Location*, Core Strategy Appendix 7 (paragraph A7.53 in particular) and the Infrastructure Delivery and Contributions SPD.

277. In April 2015 the council adopted a Community Infrastructure Levy (CIL) charging schedule, allowing it to collect a contribution towards infrastructure delivery for each new market house built: £320/m² for residential development within the South Wokingham SDL. This rate is lower than for development outside the SDLs, reflecting the amount of on-site infrastructure that is expected to be delivered in conjunction with a large-scale, strategic development.
278. The council's Infrastructure Funding Statement and Capital Programme set out the infrastructure that CIL is expected to cover (to be delivered by the council according to its priorities and overall funding availability). In addition, development specific mitigation not covered by CIL can still be secured through a combination of direct delivery (on or off-site) and financial contributions, secured by condition or S106 legal agreement, depending on the circumstances.
279. Planning policy and guidance also establish that there should be a comprehensive approach to the planning and the delivery of infrastructure for the SDL, with each development making a proportionate contribution towards the infrastructure required for the SDL as a whole. Accordingly, each application must be accompanied by an Infrastructure Delivery Plan (IDP) listing the necessary infrastructure, each developers' proportionate share and how it is to be secured. The first phase of the SDL - up to 650 dwellings, north of the railway at Montague Park – secured its proportionate share of the SDL infrastructure (26%). Using the Core Strategy, Infrastructure SPD and now historic CIL Regulation 123 list as a basis, the consortium, Charles Church and the SWDR team have jointly prepared a comprehensive Infrastructure Delivery Plan (IDP) for the remainder of the SDL.
280. The need for mitigation and how it would be secured has been assessed throughout this appraisal and the IDP which provides a summary of the full package of mitigation. This demonstrates that the infrastructure requirements for the SDL would be met.

S106 Heads of Terms

281. The infrastructure and impact mitigation to be secured through the S106 (rather than by condition or through CIL contributions) is summarised below.

Land

- i) Land for allotments (with soil to an agreed specification) (section 0);
- ii) Land for the SWDR (section 0).
- iii) Land for the SWDR which is within the SDL boundary and the Persimmon options but not the application site boundary;
- iv) A right of access over any unadopted land within parcel C2 (as identified on Drawing No P18-2684_06W *Provisional Layout*) to permit pedestrian and cycle access between footpath/cycleway on the north side of the SWDR to Wokingham Without Footpaths 9 & 10 (section 0).

- v) Provision for access to Knoll Farm to facilitate future closure of the Smiths Private User Worked Level Crossing by Network Rail;

On-site provision

- vi) 35% Affordable housing (sections 0 & 0);
- vii) Lifetime homes (section 0);
- viii) On-site provision of amenity open space (land and laying out to an agreed specification) (section 0);
- ix) On-site provision of play areas (land and laying out to an agreed specification) (section 0);
- x) Suitable Alternative Natural Greenspace including a 2.7km walk extending into the proposed Holme Park SANG, east of the application site (section 0);
- xi) Natural semi-natural greenspace (section 0);
- xii) Permissive use of PRoW by cyclists (section 0 & 0);
- xiii) Car club;
- xiv) No fewer than 20% of dwellings occupied on or before 31 March 2026 to be flats and no fewer than 14% in overall (this was the basis of the modelling that supports the HRA – section 0); and
- xv) Provision for the adoption of the roads as highways or as privately maintainable roads constructed to council's adoptable standards.

Financial contributions

- xvi) Land for the SWDR (paragraph 186);
- xvii) Land for outdoor sport (section 0);
- xviii) Strategic Access Monitoring and Management (SAMM) (section 0);
- xix) Commuted sums for future management and maintenance of on-site public open space (SANG, play areas, amenity open space) (paragraph 114);
- xx) Commuted sum for off-site biodiversity net gain if not secured on site (section 0);
- xxi) My Journey contribution (paragraph 202);
- xxii) Off-site highway works (section 0);
- xxiii) Some elements of the Pedestrian and cycle strategy (section 0) including land north of the Star Lane crossing;
- xxiv) Public Transport Strategy (section 0);
- xxv) Bus stops (section 0);
- xxvi) Any necessary Traffic Regulation Orders;
- xxvii) Any necessary Public Right of Way diversions (section 0); and
- xxviii) Employment Skills Plan or alternatively a contribution of £60,000 (section 0).

Overhead power lines

282. The 132KV and 33kV overhead power lines which run broadly east-west across the SDL are a constraint and Core Strategy Appendix 7 (paragraph A7.48 d)) reinforced by the South Wokingham SPD (pages 11, 25 and 27) establishes that this issue should be appropriately resolved, preferably by undergrounding the power lines.
283. Following discussions with Southern Electric Power Distribution (SEPD), who own and operated the power lines, it is proposed to underground the 33kV line across the whole site (within the service margins on the north side of the SWDR) and the 132kV line east of Easthampstead Road but it would remain overground through the application site.
284. Given the need for a comprehensive approach, a condition to secure a scheme for retention or reconfiguration of the electricity distribution apparatus, reflecting that imposed on phase 2 is recommended (condition 57 refers).

CONCLUSION

Consistent with Development Plan policy, this application provides for the coordinated delivery of new homes and the supporting infrastructure required in the South Wokingham SDL. The application can be supported.

The Public Sector Equality Duty (Equality Act 2010)

In determining this application the Council is required to have due regard to its obligations under the Equality Act 2010. The key equalities protected characteristics include age, disability, gender, gender reassignment, marriage and civil partnership, pregnancy and maternity, race, religion or belief. There is no indication or evidence (including from consultation on the application) that the protected groups identified by the Act have or will have different needs, experiences, issues and priorities in relation to this particular planning application and there would be no significant adverse impacts upon protected groups as a result of the development.

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TARGET NOTES

- ① Emm Parkland SANG
- ② Local Landscaped area of play (LLAP)
- ③ Boardwalk (as required) through wet woodland & pedestrian link to adjacent SANG
- ④ Allotments
- ⑤ Pocket Park
- ⑥ Equipped play area
- ⑦ Wildlife Corridor
- ⑧ Feature tree within shared space
- ⑨ Brook crossing and path junction beneath roadway overpass
- ⑩ SDR footpath boardwalk
- ⑪ Formalised dog dip

KEY

- Site boundary
- Proposed boundary of Suitable Accessible Natural Greenspace (SANG)
- Existing trees/vegetation to be retained *
- Existing trees/vegetation to be removed (subject to AIA)
- Existing Emm Brook watercourse
- Areas of ecological value to be retained and enhanced (lowland dry acid grassland, Fens, Purple more grass rush pasture) *
- Proposed Southern Distributor Road
- Proposed areas of residential development
- Proposed wetland trees and wet meadow grassland/wildflowers
- Proposed native trees and meadow grassland/wildflowers
- Proposed feature tree
- Proposed hedgerow trees as a semipermeable layer
- Proposed native tree and shrub screening buffer to development edges
- Proposed indicative plot tree
- Proposed native hedgerow
- Proposed informal surfaced path and seating
- Proposed mown path
- Proposed timber boardwalk through wet woodland
- Maintenance access to west bank of Emm Brook under SDR with dog proof fencing
- Pedestrian bridge over existing ditch
- Formalised dog dip
- Proposed attenuation ponds
- Proposed swales
- Wildflower grassland
- New ecological enhancement area (lowland dry acid grassland and Fens) *
- Proposed fence

121

* Based on The Ecology Partnership Habit Condition Assessment (May 2021)

- SANG walk (within site boundary)
- Existing public right of way
- Alternative flood route
- Existing PROW to be diverted
- Proposed PROW diversion route

E: 11/02/2022 WMI Updated to include a footpath link to the SANG opposite plot 44.
 D: 10/01/2022 WMI Updated SDR balancing pond and footpath alignments.
 C: 13/12/2021 WMI Updated to the latest site layout.
 B: 05/06/2021 WMI Updated to the latest site layout.
 A: 19/04/2021 WMI Updated to the latest residential layout and drainage strategy.
 Revisions:



LEGEND

Green Infrastructure Provision

- Parks and gardens
- Natural/Semi-Natural Green space (excluding Country parks)
- Amenity Greenspace
- Provision for children and young people (LAP, LEAP and NEAP)
- Allotments
- SANG

E- [02/22/2022 WM] Updated area for natural/semi natural green space
 D- [11/10/2021 WM] Updated to LPA comments
 C- [06/07/2021 WM] Updated to latest layout
 B- [28/04/2021 WM] Updated to client comments
 A- [19/04/2021 WM] Updated to landscape masterplan
 Revisions:

Land south-east of Finchampstead Road
Green Infrastructure Provision

Typology	MDD DPD Standard	On Site Provision Requirement (Ha)	On Site Provision (Ha)	Notes
 Parks and gardens	1.1 ha/1000	0.451	0.495	
 Natural/Semi-Natural Green space (excluding Country parks)	2.84 ha/1000	1.166	3.202	Excludes SANG
 Amenity Greenspace	0.98 ha/1000	0.4	0.4	
 Provision for children and young people (LAP, LEAP and NEAP)	0.25 ha/1000	0.103	0.15	
 Allotments	0.52 ha/1000	0.213	0.587	
 SANG	8 ha/1000	3.28	8.114	
Number of dwellings:171				
2.4 people per dwelling:410.4				

Note: Civic space located within the Consortium's site covers the site cumulatively with the Consortium's.

Drawing Ref: P18-2684_40E

Client: Persimmon Homes

Date : 12/04/2021

Drawn by : WM

Scale : 1 : 2000 @ A2

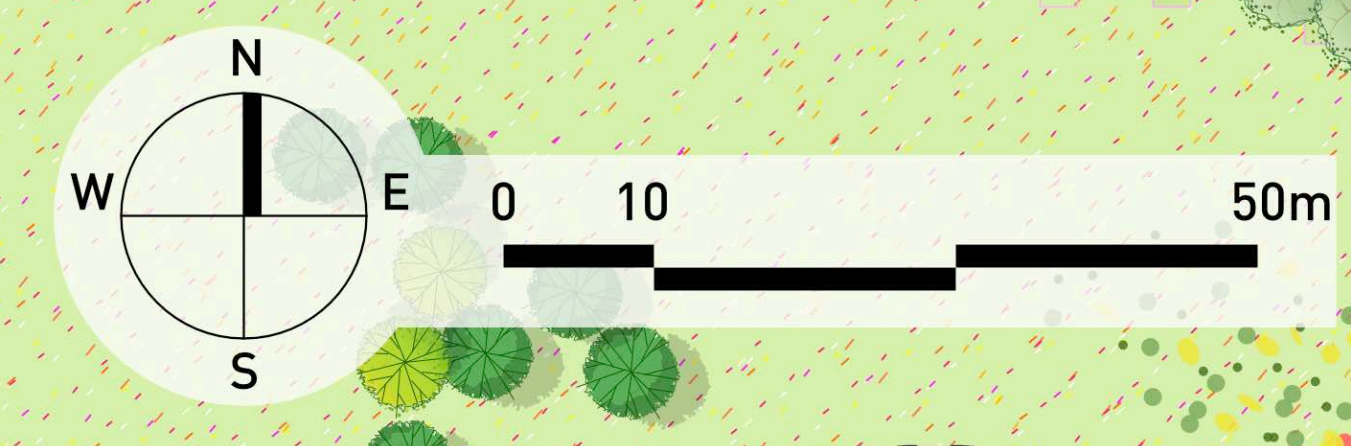
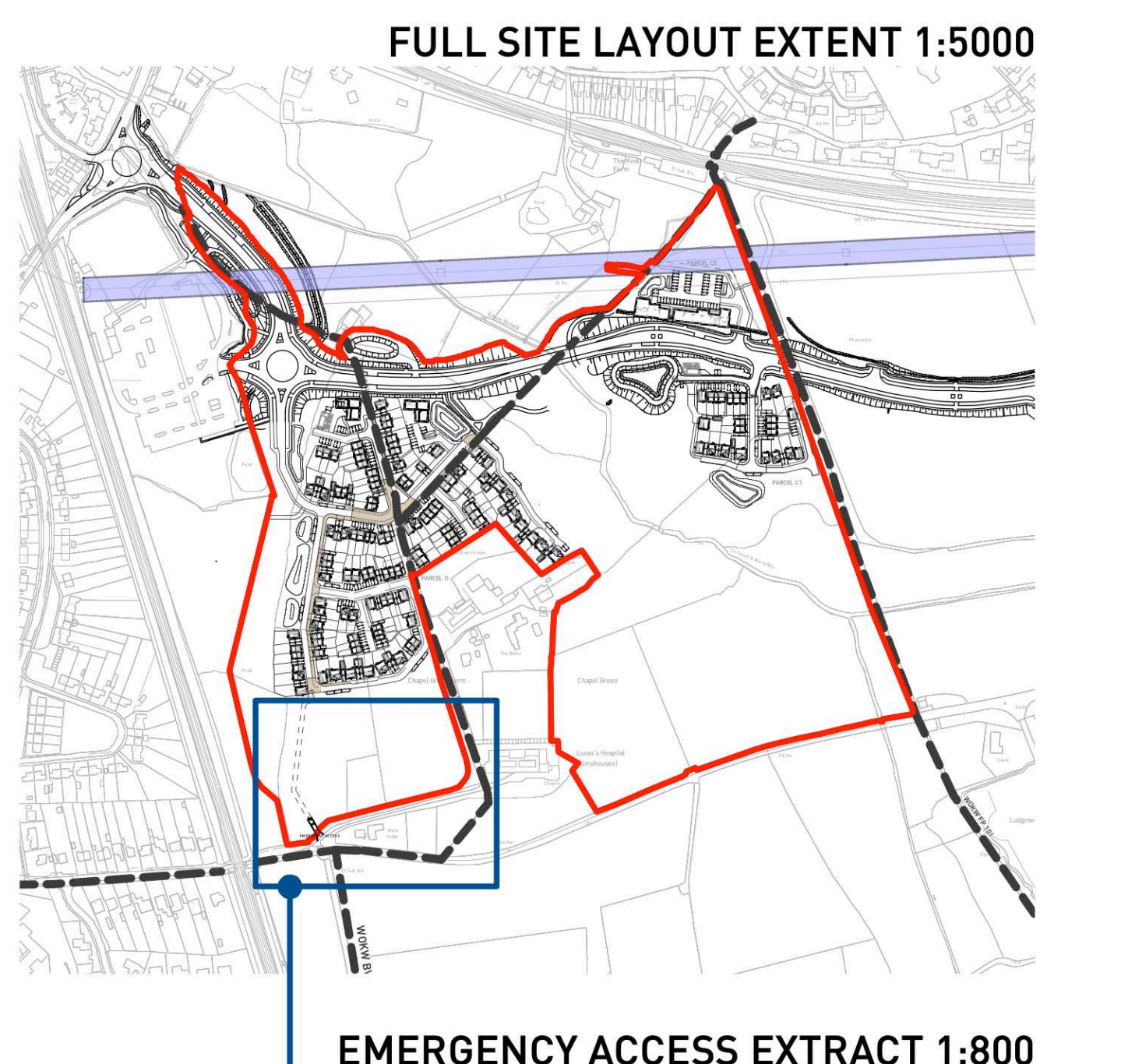
0 100m

DESIGN ENVIRONMENT PLANNING ECONOMICS HERITAGE





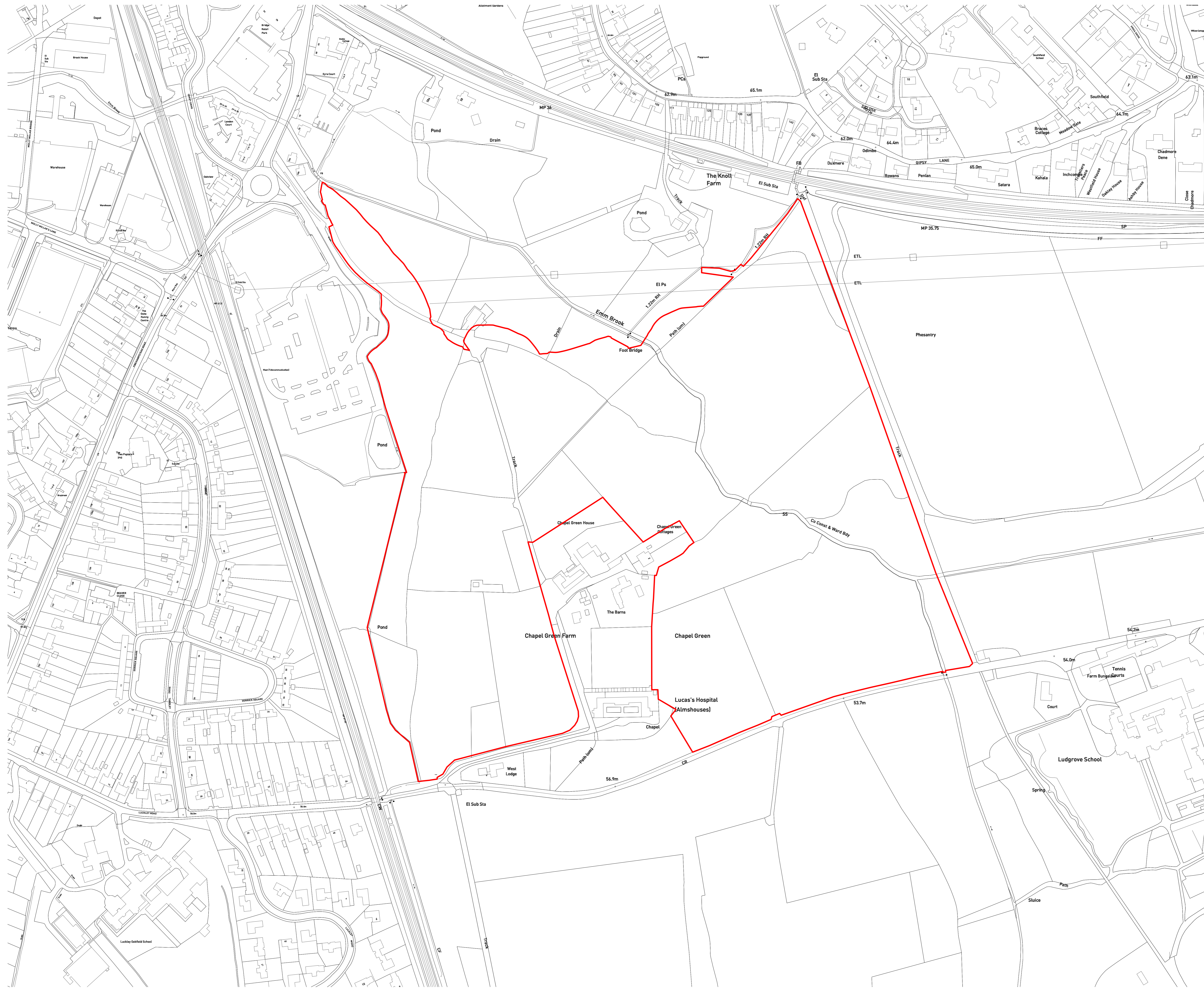
- KEY: SITE LAYOUT**
- SITE BOUNDARY
 - 105 PARKING ALLOCATION
 - 105 PLOT NUMBER
 - UNALLOCATED PARKING
 - VISITOR PARKING
 - EXISTING PROW
 - EXISTING PROW TO BE DIVERTED
 - PROPOSED PROW DIVERSION ROUTE
 - ALTERNATIVE FLOOD ROUTE
 - EXISTING TREE GROUPS/HEDGEROWS
 - PROVISIONAL DRAINAGE BASIN
 - AFFORDABLE DWELLING
 - GAS MAIN/ELECTRICITY CABLES TO BE GROUNDED
 - ELECTRICITY CABLES & ASSUMED 10M OFFSET
 - MODELLED FLOODPLAIN
 - EXISTING TREE CANOPIES
 - SERVICE MARGIN
 - INDICATIVE TREE PLANTING



Note
This layout is prepared in support of the Outline Planning Application pertaining to this site.

LAND SOUTH EAST OF FINCHAMPSTEAD ROAD, SOUTH WOKINGHAM SDL - PROVISIONAL LAYOUT

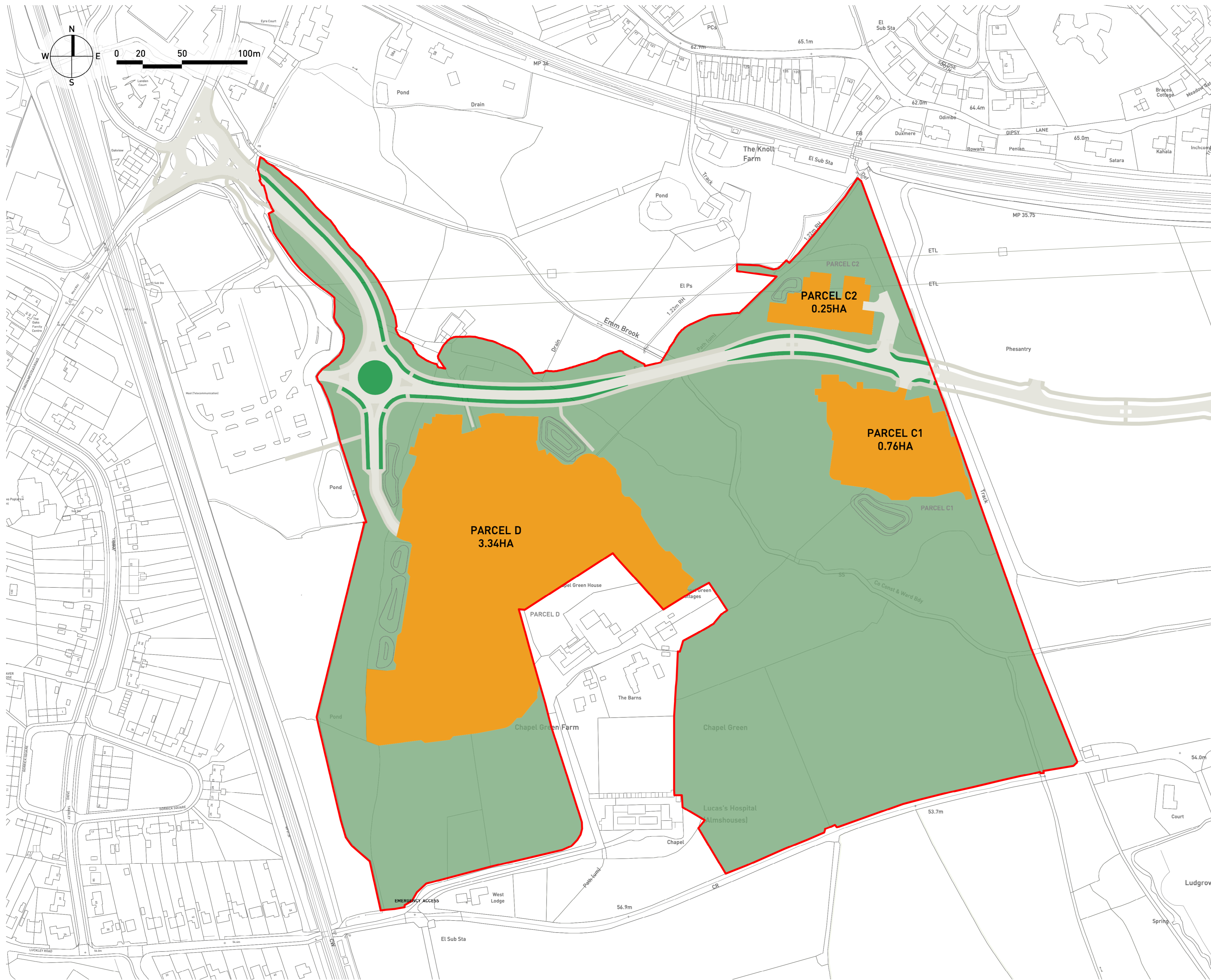
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KEY: SITE LOCATION PLAN
 [Red Outline] APPLICATION BOUNDARY (18.75Ha)

LAND SOUTH EAST OF FINCHAMPSTEAD ROAD, SOUTH WOKINGHAM SDL - SITE LOCATION

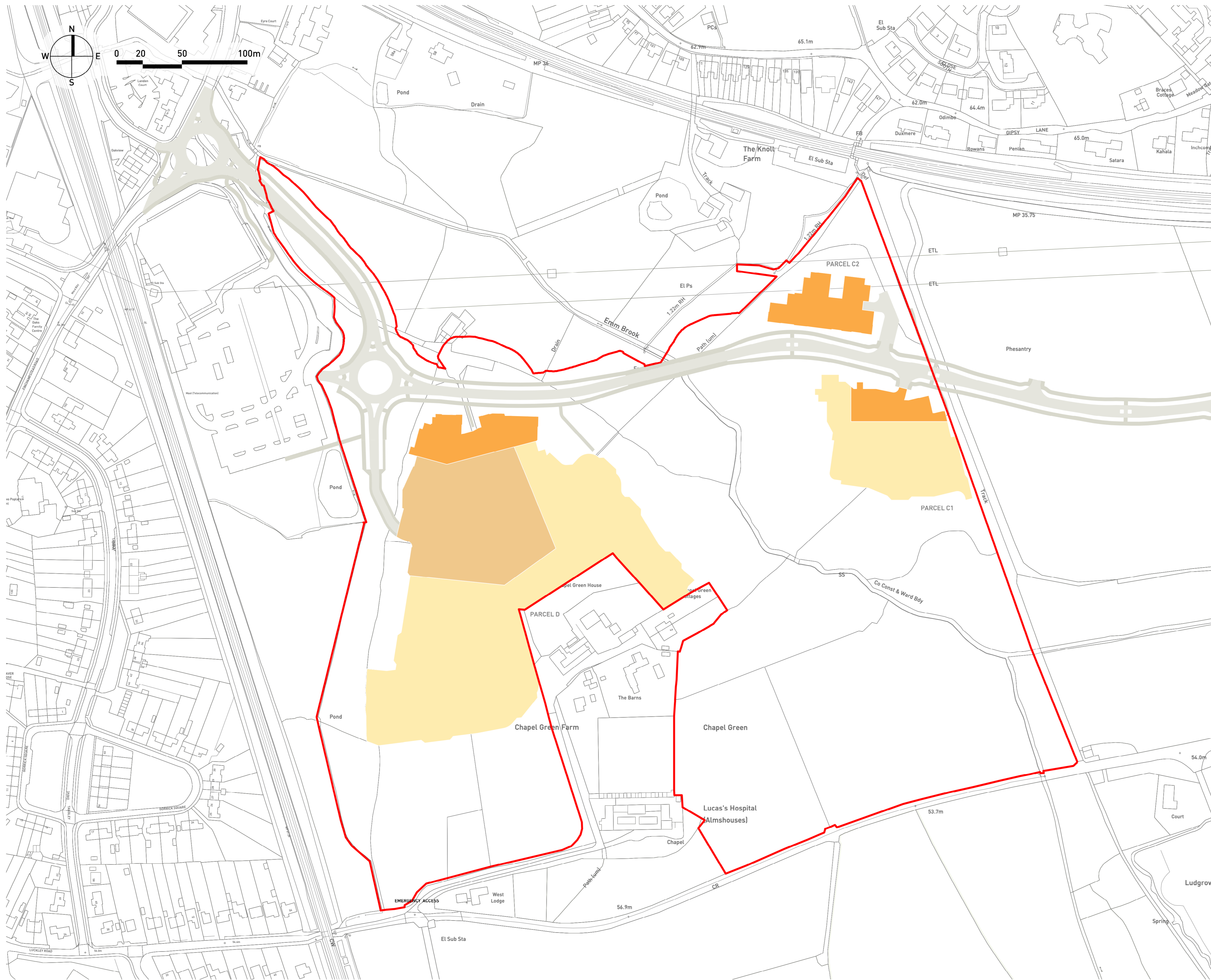




- KEY: PARAMETERS
LAND USE**
- SITE BOUNDARY
 - PROPOSED RESIDENTIAL AREA
 - PROPOSED GREEN INFRASTRUCTURE & PUBLIC OPEN SPACE
 - EMERGING SOUTHERN DISTRIBUTOR ROAD ALIGNMENT (NOT FORMING PART OF THIS APPLICATION)

LAND SOUTH EAST OF FINCHAMPSTEAD, SOUTH WOKINGHAM SDL - PARAMETERS - LAND USE



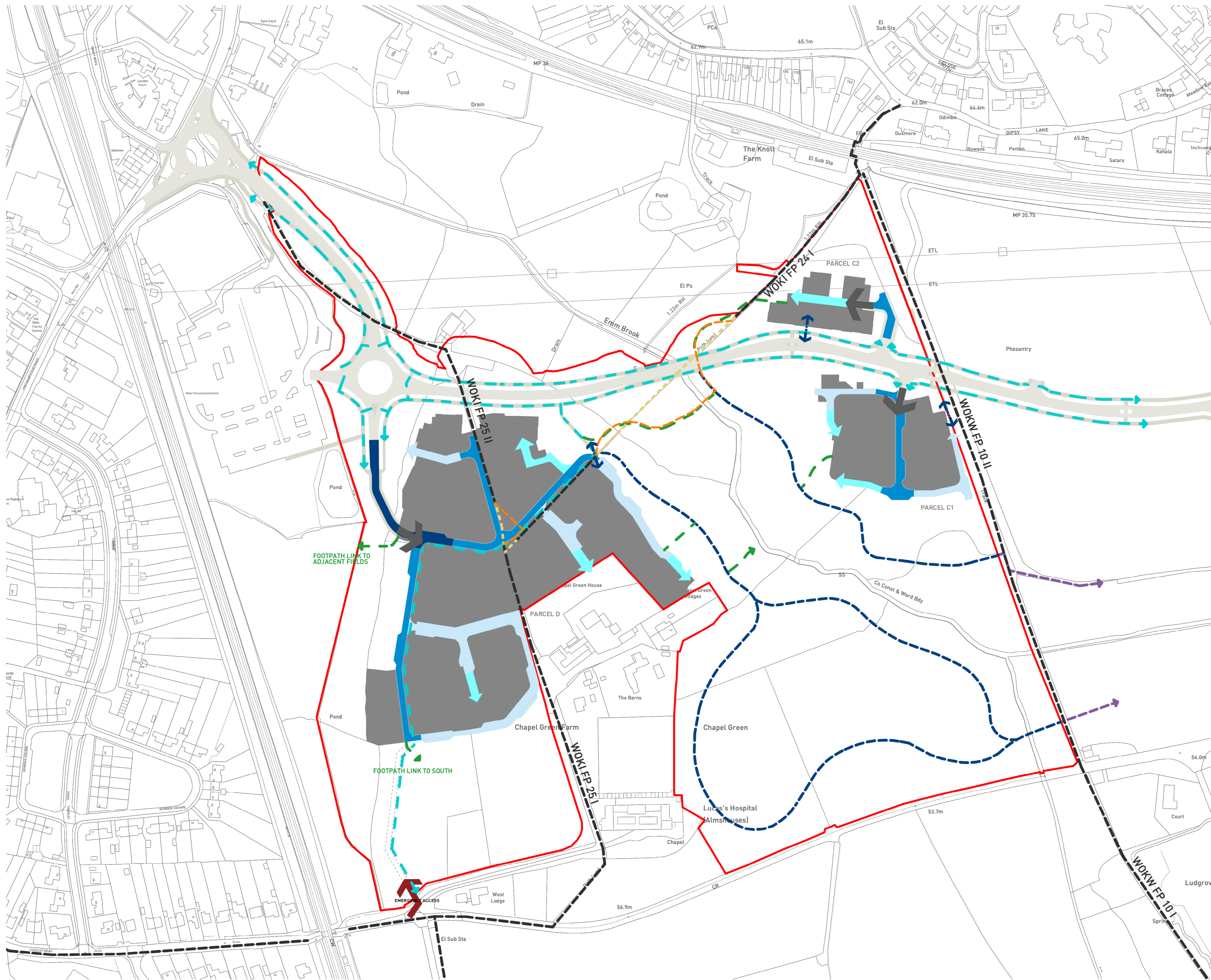


**KEY: PARAMETERS
BUILDING HEIGHTS**















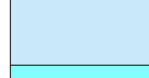


- SITE BOUNDARY**
- UP TO 2 STOREY**
(10M TO RIDGE MAX)
- UP TO 2.5 STOREY**
(11M TO RIDGE MAX)
- UP TO 3 STOREY**
(12.5M TO RIDGE MAX)
- EMERGING SOUTHERN
DISTRIBUTOR ROAD
ALIGNMENT (NOT FORMING
PART OF THIS APPLICATION)**

LAND SOUTH EAST OF FINCHAMPSTEAD, SOUTH WOKINGHAM SDL - PARAMETERS - BUILDING HEIGHTS





KEY: PARAMETERS ACCESS & MOVEMENT










-  SITE BOUNDARY
-  PROPOSED RESIDENTIAL AREA
-  PROPOSED ACCESS TO RESIDENTIAL AREA
-  EXISTING PROW
-  EXISTING PROW TO BE DIVERTED
-  PROPOSED PROW DIVERSION ROUTE
-  PEDESTRIAN ACCESS TO PARCELS FROM SDR
-  SANG PEDESTRIAN ROUTE
-  CONNECTION TO ADJACENT SANG
-  SECONDARY PEDESTRIAN ROUTE
-  PROPOSED SHARED PEDESTRIAN/CYCLE ROUTE
-  EMERGENCY ACCESS
-  MAJOR ACCESS ROAD
-  MINOR ACCESS ROAD
-  SHARED SURFACE ROAD
-  PRIVATE DRIVE
-  EMERGING SOUTHERN DISTRIBUTOR ROAD ALIGNMENT (NOT FORMING PART OF THIS APPLICATION)

LAND SOUTH EAST OF FINCHAMPSTEAD, SOUTH WOKINGHAM SDL - PARAMETERS - MOVEMENT & ACCESS





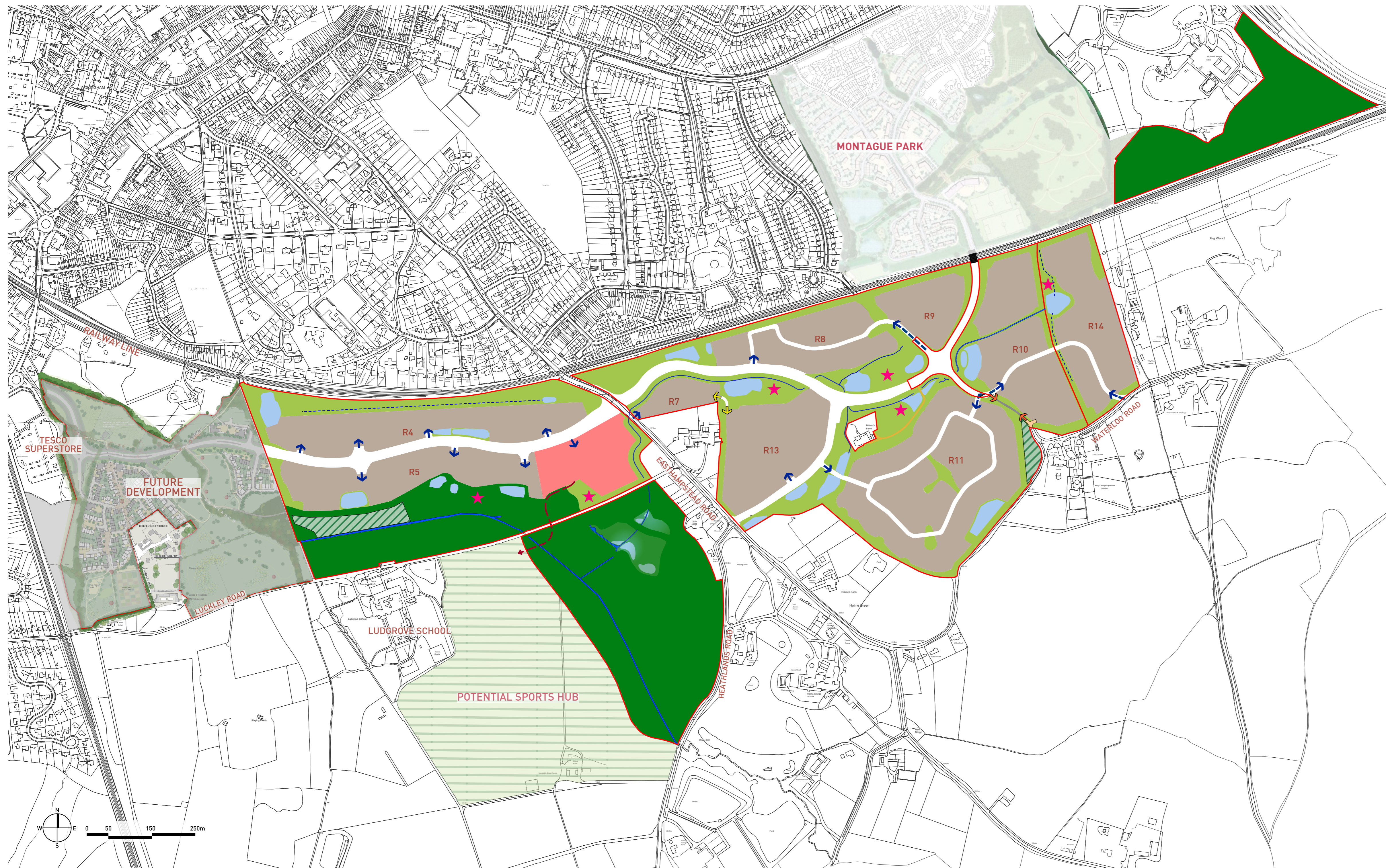
**KEY: PARAMETERS
POS TYPOLOGY**

-  **SITE BOUNDARY**
-  **PROPOSED RESIDENTIAL AREA (4.35HA)**
-  **PARKS AND GARDENS (0.495HA)**
-  **NATURAL/SEMI-NATURAL GREEN SPACE (EXCLUDING COUNTRY PARKS) (3.202HA)**
-  **AMENITY GREENSPACE (0.40HA)**
-  **PROVISION FOR CHILDREN AND YOUNG PEOPLE (LLAP, LAP, LEAP) (0.15HA)**
-  **ALLOTMENT (0.587HA)**
-  **SANG (8.114HA)**
-  **EMERGING SOUTHERN DISTRIBUTOR ROAD ALIGNMENT (NOT FORMING PART OF THIS APPLICATION)**

LAND SOUTH EAST OF FINCHAMPSTEAD, SOUTH WOKINGHAM SDL - PARAMETERS - POS TYPOLOGIES



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KEY

- THE PROJECT DEVELOPMENT
- INDICATIVE ALIGNMENT OF SWSDR
- INDICATIVE RESIDENTIAL PARCEL
- INDICATIVE OPEN SPACE
- INDICATIVE LOCAL CENTRE/ PRIMARY SCHOOL SITE
- INDICATIVE LOCATION OF ATTENUATION BASINS
- SUITABLE ALTERNATIVE NATURAL GREENSPACE (SANG)
- ANCIENT WOODLAND
- EQUIPPED AREA OF PLAY
- PARCEL ACCESS LOCATIONS
- KEY INTERNAL STREETS
- PROPOSED ACCESS TO BRITTON'S FARM
- SAFEGUARDED ROUTE TO SPORTS HUB
- DITCHES
- SWALES
- EMERGENCY/ PEDESTRIAN/ CYCLE ACCESS
- POSSIBLE TURNING HEAD LOCATION AND PEDESTRIAN/ CYCLE/ EQUESTRIAN ACCESS ALONG ADOPTED HIGHWAY

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PLANNING REF : 192325
PROPERTY ADDRESS : Gorse Ride North
: Finchampstead
: RG40 3ES
SUBMITTED BY : Councillor Finchampstead Parish Council
DATE SUBMITTED : 01/05/2020

COMMENTS:

We accept the development in principle but have concerns about the adverse impact on the Finchampstead Rd. The distributor road and bridge widening should be completed before development commences.

Other areas of concerns are flooding, the disruption to wildlife and removal of trees.

The proposal could have a harmful impact on the former Henry Lucas Hospital a Grade 1 listed building.

We support the allocation of 35% affordable housing and the comments made by Wokingham Town Council.

From: [Marcia Head](#)
To: [Planning Enquiries](#)
Cc: [Emy Circuit](#); [Finchampstead Parish Council](#)
Subject: FW: WBC Planning Application 192325 - Land South East of Finchampstead Road, South Wokingham (revised plans) ~[UNCLASSIFIED]~
Date: 24 September 2021 11:21:55
Attachments: [RSR03319.Pdf](#)

Please can you at the Parish Council's comments on the revised plans to the web site.

Thanks

Marcia Head BSc (Hons), MA, DMS, MRTPI Service Manager, Place and Growth, Wokingham Borough Council, Shute End, Wokingham, RG40 1WR, mobile 07825420651

-----Original Message-----

From: Caroline Cordell <Planning@finchampstead-pc.gov.uk>

Sent: 23 September 2021 13:01

To: Marcia Head [REDACTED]

Cc: Steve Bromley [REDACTED]

Subject: FW: WBC Planning Application 192325 - Land South East of Finchampstead Road, South Wokingham (revised plans)

External Email

CAUTION: This is an EXTERNAL EMAIL - STOP & THINK before clicking a link or opening attachments.

Hi Marcia,

Please could you add the following comments on behalf of Finchampstead Parish Council in relation to the above planning application (revised plans) before the cut off date of 27th Sept? I did try via the portal but for some reason it's not recognising the number.

Our existing comments are still valid, particularly with regard to the additional traffic going under the narrow bridge up to Molly Millars Lane.

Many thanks & kind regards,
Caroline

Clerk: Mrs K. Dagnall FSLCC

Planning Committee Clerk: Mrs C. Cordell Finchampstead Parish Council FBC Centre, Gorse Ride North Finchampstead Berkshire RG40 4ES

0118 908 8164

WBC Planning Application 192325

"The Council has no objections to the revised plans. Please refer to our comments in relation to the original plans dated 01/05/20".

Many thanks & kind regards,

Caroline.

Clerk: Mrs K. Dagnall FSLCC

Deputy Clerk: Mrs C. Warke

Planning Committee Clerk: Mrs C. Cordell Finchampstead Parish Council FBC
Centre, Gorse Ride North Finchampstead Berkshire RG40 4ES

PLANNING REF : 192325
PROPERTY ADDRESS : Market Place Wokingham
:
: RG40 1AS
SUBMITTED BY : The Wokingham Town Council P&T Committee
DATE SUBMITTED : 16/04/2020

COMMENTS:

There were major concerns regarding flooding, increased traffic on local roads and the disruption to wildlife, removal of trees and lack of infrastructure to support the development. The distributor road should be complete before development.

There are also concerns regarding impact on the Grade 1 listed building formerly Henry Lucas Hospital, that has major historic value.

Trees removed should be replaced with mature trees.

Objections as follows:

CP1 ? Sustainable development

CP 1 ? 8 Pollution (including noise)

CP 1 ? 4 Drainage

CP 1 ? 9 Flooding

CP 111 Need to travel

CP3 General Principles for development

CP 3 ? D Flora & Fauna

CP 4 ? Infrastructure Requirements

CP6 ? Managing Traffic Demand

CP6 ? C Improve existing infrastructure network

CP6 ? E adverse effects on transport network

CP7 Biodiversity

CP7 A harm county designated sites

The committee however, saw as a positive that there is to be 35% allocated affordable homes within the development.

CP5 Housing mix, density and affordability

PLANNING REF : 192325
PROPERTY ADDRESS : Town Hall Market Place
: Wokingham
: RG40 1AS
SUBMITTED BY : The Wokingham Town Council P&T Committee
DATE SUBMITTED : 14/10/2021

COMMENTS:

The previous comments that the Committee made objecting to this application still stand along with the following new comments.

?The Greenway proposed through this development does not separate cyclists from pedestrians.

?The planned bicycle Storage is by the bins and is also too far away from the buildings.

?There are formal amenity areas but there should be an area to allow children to play and kick a football around.

?There should be allocation of public exercise equipment.

?There is planned removal of a 400m hedge.

?The lack of infrastructure to deal with increased numbers in the Community. In particular concern is access to GP and Doctors Surgeries.

PLANNING REF : 192325
PROPERTY ADDRESS : Town Hall Market Place
: Wokingham
: RG40 1AS
SUBMITTED BY : The Wokingham Town Council P&T Committee
DATE SUBMITTED : 02/02/2022

COMMENTS:

The Committee discussed the parking for the SANG. As there are cars already parking along Luckley Road and this will be increased, causing damage to verges, then an idea would be to introduce wooden stakes along this stretch of road to discourage parking. The Committee would like this implemented.

The Committee's previous objections still stand, particularly regarding flooding.

The Committee would like to add an additional objection regarding parking.

CP 6D appropriate vehicular parking.

PLANNING REF : 192325
PROPERTY ADDRESS : The Lodge
: Pinewood Leisure Centre, Old Wokingham Road, Crowthorne, Wokingham, RG40 3AQ
SUBMITTED BY : Wokingham Without Parish Council
DATE SUBMITTED : 17/04/2020

COMMENTS:

The site is allocated in the Core Strategy for housing and is acceptable in principle. It is expected that the development will be assessed against the adopted policy and the adopted Supplementary Development Document for the South Wokingham SDL.

There have been a number of objections from our residents referencing increased traffic, noise and pollution, and the Council is requested to take these fully into account when determining the application.

PLANNING REF : 192325
PROPERTY ADDRESS : The Lodge
: Pinewood Leisure Centre, Old Wokingham Road, Crowthorne, Woki
: RG40 3AQ
SUBMITTED BY : Wokingham Without Parish Council
DATE SUBMITTED : 22/09/2021

COMMENTS:

In addition to our comments submitted on 17th April 2020, WWPC would like to note that concerns have been raised with respect to the location and infrastructure for the additional new housing required to compensate for the reduction on this site.

PLANNING REF : 192325
PROPERTY ADDRESS : The Lodge
: Pinewood Leisure Centre, Old Wokingham Road, Crowthorne, Woki
: RG40 3AQ
SUBMITTED BY : Wokingham Without Parish Council
DATE SUBMITTED : 27/09/2021

COMMENTS:

In addition to our comments submitted on 17th April 2020, WWPC would like to note that concerns have been raised with respect to the location and infrastructure for the additional new housing required to compensate for the reduction on this site.

PLANNING REF : 192325
PROPERTY ADDRESS : The Lodge
: Pinewood Leisure Centre, Old Wokingham Road, Crowthorne, Woki
: RG40 3AQ
SUBMITTED BY : Wokingham Without Parish Council
DATE SUBMITTED : 07/01/2022

COMMENTS:

Wokingham Without Parish Council have nothing further to add to our comments of 17th April 2020 and 22nd September 2021.

Agenda Item 84.

Application Number	Expiry Date	Parish	Ward
214183	5 April 2022	Winnersh	Winnersh;

Applicant	Stage Fifty
Site Address	Land At 1040 And 1100 Series Eskdale Road Winnersh Triangle Business Park Wokingham RG41 5TS
Proposal	Full application for the proposed temporary erection of 2no. sound stage buildings for commercial filming and 8no. workshops, with associated access, parking, landscaping and infrastructure for a period of five years (retrospective).
Type	Full
Officer	Joanna Carter
Reason for determination by committee	Major application (floor area >1000m2)

FOR CONSIDERATION BY	Planning Committee on Wednesday, 9 March 2022
REPORT PREPARED BY	Assistant Director – Place & Growth

SUMMARY
<p>The proposal seeks temporary permission for the period of five years for the erection of a film studio comprising two sound stage buildings and eight workshops along with ancillary structures, including welfare facilities, bin stores, security fencing, security gate huts. Two sound stage buildings and four workshops are under construction at 1100 Series Eskdale Road and four workshops are under construction at 1040 Series, Eskdale Road.</p> <p>The application site is located within Winnersh Triangle Business Park, which is designated as a Core Employment Area. The scheme would create 7,242m² of floorspace in Use Class E(g)(iii) (industrial process incidental to film, video or sound recording) within an area where the principle of the development is accepted and the proposal is in accordance with the aim of the Core Strategy (Core Strategy Policy CP15) to concentrate the majority of employment growth within Core Employment Areas. The economic market has changed considerably since the park came into use and as such the existing plots have been vacant for some time. As such there are no in principle objections to the proposed use.</p> <p>The pre-production works have already commenced elsewhere on site as part of this project and include occupation of the existing offices (E2 building located between 1040 and 1100 Series) as well as of Units C1-C3 on the opposite side of Eskdale Road. Building 820 Series, previously Units C4-C8 also located south of Eskdale Road, is currently under construction (application ref 193285). This area is within applicant's control as intended to accommodate operations associated with the proposal. The proposal would create approximately 250 direct and 250 indirect jobs in the short term with the ultimate aim of creating overall 500 direct and 500 indirect jobs as part of the long-term aspirations of the applicant for this area should the application be accepted.</p> <p>No objections were raised by the ward members or neighbours and Winnersh Parish Council was supportive of the proposal.</p>

The design of the proposed buildings is dictated by their function, whether to accommodate sound stages or to offer flexible work space to create sets, costumes and props. Given the temporary nature of this application, the appearance of temporary buildings is considered to be in keeping with the character of the surrounding commercial area and there are no concerns on these grounds. However it should be noted that should a permanent proposal be submitted, officers would want to consider the appearance of the buildings in more detail. There are no residential uses in close proximity to the site and the impact on the existing landscape would be limited as no trees are proposed to be removed as part of this scheme. The proposal would not have significant impact on the highway network nor would it impact upon the function or safety of the A329(M) which runs directly to the north. The site is located in close proximity to A329(M), as well as in a highly sustainable location, close to Winnersh Train Station and P&R 500 bus service. As such there is no objection in terms of parking and the changes in the levels of parking resulting from this proposal in the short term. In addition to offering sustainability benefits, the proposal would also ensure ecological enhancements and contribute to the creation of opportunities for training or vocational initiatives, which would be secured through a Section 106 Legal Agreement. Therefore, given the local economic benefits of the proposals, it is recommended that the application is approved subject to conditions outlined below and subject to the Section 106 Legal Agreement being completed.

PLANNING STATUS

- Major Development Location of Winnersh (CS Policy CP9 and MDD LP Policy CC01)
- Core Employment Area – Winnersh Triangle (CS Policy CP15 and MDD LP Policy TB11)
- Heathrow Aerodrome Safeguarding Zone
- Minerals Consultation Zone (Replacement Minerals Local Plan for Berkshire incorporating the alterations adopted in December 1997 and May 2001)
- Nuclear Site Consultation Zone – Burghfield Atomic Weapons Establishment 12 km (MDD LP Policy TB04)
- Electricity Sub Station Consultation Zone – SSE Power Distribution
- Overhead Electricity Cable Consultation Zone – SSE Power Distribution
- Contaminated Land Consultation Zone (south/west section of 1040 Series)
- Flood Zone 2 (MDD LP Policy CC09)
- Bat Roost Habitat Consultation Zone (MDD LP Policy TB23)
- Site of Special Scientific Interest Impact Risk Zone (MDD LP Policy TB23)

RECOMMENDATION

That the Planning Committee authorise the GRANT OF PLANNING PERMISSION subject to the following:

A. Prior completion of a S106 legal agreement inclusive of the following Heads of Terms:

- i) Biodiversity Net Gain off-site provision/contribution
- ii) Employment Skills Plan contribution

iii) Section 106 Monitoring contribution

B. Conditions and informatives as below (subject to any additions and updates agreed with the Assistant Director – Place and Growth between the date of the resolution and the issue of the decision):

Conditions and Reasons

Approved Drawings

1. This permission is in respect of the submitted application plans and drawings as listed in the table below. The development hereby permitted shall be carried out in accordance with the approved details unless otherwise agreed in writing by the Local Planning Authority.

Drawing/Document Title	Drawing Number	Revision
<i>Proposed Sound Stages 1 & 2 Floor Plans and Elevations</i>	<i>2697-TP-102</i>	<i>P1</i>
<i>Proposed Workshops 1 & 2 Floor Plans and Elevations</i>	<i>2697-TP-103</i>	<i>P1</i>
<i>Proposed Workshops 3 & 4 Floor Plans and Elevations</i>	<i>2697-TP-104</i>	<i>P1</i>
<i>Proposed Workshops 5 & 6 Floor Plans and Elevations</i>	<i>2697-TP-105</i>	<i>P1</i>
<i>Proposed Workshops 7 & 8 Floor Plans and Elevations</i>	<i>2697-TP-106</i>	<i>P1</i>
<i>1040 Security Hut Proposed Plans and Elevations</i>	<i>2697-TP-107</i>	<i>/</i>
<i>1100 Security Hut Proposed Plans and Elevations</i>	<i>2697-TP-108</i>	<i>/</i>
<i>Site Layout</i>	<i>2697-TP-101</i>	<i>P7</i>
<i>Transport Statement prepared by Motion dated 11.02.2022</i>	<i>/</i>	<i>v.1.3</i>

Reason: for the avoidance of doubt and to ensure that the development is carried out in accordance with the application form and associated details hereby approved.

Temporary Permission

2. The development hereby permitted shall be discontinued, the associated buildings and structures removed and the land restored to its former condition no later than five years from the date of this planning permission.

Reason: The applicant has sought a temporary planning consent for five years and the Council would wish to consider more permanent proposals in greater detail. Relevant Policy: Core Strategy Policies CP1, CP3 and CP11.

Construction Method Statement

3. The construction of development hereby permitted shall adhere to the Framework Construction Method Statement dated 11.02.2022 prepared by Motion throughout the period of construction. The method statement shall be reviewed and updated if necessary on an ongoing basis and any updated details shall be submitted to and approved in writing by the Local Planning Authority.

Reason: In the interests of highway safety & convenience and neighbour amenities. Relevant policy: Core Strategy policies CP3 and CP6.

Delivery and Service Strategy

4. The development hereby permitted shall be carried out in accordance with the Delivery and Servicing Plan dated 11.02.2022 prepared by Motion. The plan shall be reviewed and updated if necessary on an annual basis and any updated details shall be submitted to the Local Planning Authority for a written approval.

Reason: To ensure a satisfactory form of development and to avoid adverse impact on the public highway in the interests of highway safety. Relevant Policy: Core Strategy Policies CP3 and CP6.

Vehicle Parking and Turning

5. The development hereby permitted shall not become operational until the vehicle parking and turning spaces serving it have been provided in accordance with the approved plans. The vehicle parking and turning spaces shall be retained and maintained in accordance with the approved details and the parking spaces shall remain available for the parking of vehicles at all times during the life of the planning permission. The turning space shall not be used for any other purpose other than vehicle turning.

Reason: To provide adequate off-street vehicle parking and turning space and to allow vehicles to enter and leave the site in a forward gear in the interests of road safety, convenience and providing a functional, accessible and safe development, and in the interests of amenity. Relevant Policy: Core Strategy Policies CP3 and CP6 and Managing Development Delivery Local Plan Policy CC07.

Electric Vehicle Charging

6. Within three months of the date of this planning permission full details of electric vehicle charging points shown in the approved drawing "Site Layout Plan 2697-TP-101 Rev P7" shall be submitted to and approved in writing by the Local Planning Authority. The electric vehicle charging points shall be installed in accordance with the approved details within three months of the Local Planning Authority's approval and shall be retained and maintained

thereafter for the lifetime of the planning permission, unless otherwise agreed in writing by the Local Planning Authority.

Reason: In order to ensure the development contributes towards achieving a sustainable transport system. Relevant Policy: Core Strategy Policies CP1 and CP6, and Policy CC07 of Managing Development Delivery Local Plan.

Cycle Parking

7. Before the development hereby approved is brought into operation, details of secure and covered parking for cycles serving this development shall be submitted to in writing for approval to the Local Planning Authority. The cycle parking shall be provided in accordance with the approved details within three months of the Local Planning Authority's approval and be permanently so-retained for the parking of bicycles and used for no other purpose for the life of the planning permission.

Reason: In order to ensure that secure weather-proof bicycle parking facilities are provided so as to encourage the use of sustainable modes of travel. Relevant Policy: NPPF Section 9 (Sustainable Transport), Core Strategy Policies CP1, CP3 and CP6, and Managing Development Delivery Local Plan Policy CC07.

Access

8. The proposed vehicular accesses and egress shall be formed and provided with visibility splays as shown on the approved plans. The land within the visibility splays shall be cleared of any obstruction exceeding 0.6 meters in height and maintained clear of any obstruction exceeding 0.6 meters in height at all times.

Reason: In the interests of highway safety and convenience. Relevant Policy: Core Strategy Policies CP3 and CP6.

Travel Plan

9. Within three months of the date of this planning permission, a Travel Plan shall be submitted to the Local Planning Authority for a written approval. The submitted Travel Plan shall be in general accordance with the approved Framework Travel Plan dated 17 February 2022 and prepared by Motion and include a programme of implementation and proposals to promote alternative forms of transport to and from the site, other than by the private car and provide for periodic review for monitoring purposes. The approved Travel Plan shall be fully implemented within three months of the Local Planning Authority's approval, maintained thereafter and reviewed as so-approved.

Reason: To encourage the use of all travel modes. Relevant Policy: NPPF Section 9 (Sustainable Transport) and Core Strategy Policy CP6

Parking Management Plan

10. Within three months of the date of this planning permission, a Parking Management Strategy for the management of the parking arrangements shall be submitted to the Local Planning Authority for a written approval. The submitted Parking Management Strategy shall include details of management of all parking spaces and the monitoring and the delivery of the passive electric vehicle charging spaces. The Parking Management Strategy shall be implemented in accordance with the approved details within three months of the Local Planning Authority's approval.

Reason: To ensure satisfactory development in the interests of amenity and highway safety in accordance with Wokingham Borough Core Strategy Policies CP1 and CP6, and MDDL P Policies CC07.

Boundary Treatment

11. Within three months of the date of this planning permission, details of all boundary treatments and other means of enclosure shall be submitted to and approved in writing by the Local Planning Authority. The approved scheme shall be implemented within one month of the Local Planning Authority's approval and shall be retained and maintained in the approved form for the lifetime of the planning permission.

Reason: In the interests of amenity and highway safety, in accordance with Core Strategy Policies CP1, CP3 and CP6, and Managing Development Delivery Local Plan Policy TB23.

Flood Risk

12. Development hereby permitted shall be carried out in accordance with the Flood Risk Assessment ref: 11574 dated 14.02.2022 prepared by GTA Civils & Transport.

Reason: To prevent the increased risk of flooding and to protect water quality. Relevant policy: NPPF Section 14 (Meeting the Challenge of Climate Change, Flooding and Coastal Change), Technical Guidance on the NPPF (Flood Risk), Core Strategy Policy CP1 and Managing Development Delivery Local Plan Policy CC09.

Sustainable Drainage

13. No part of development hereby permitted shall be occupied until the sustainable drainage scheme for the site has been completed in accordance with Surface Water Drainage Maintenance & Management Plan ref 11518 dated February 2022 prepared by GTA Civils & Transport. The sustainable drainage scheme shall be managed and maintained thereafter in accordance with the approved details.

Reason: To prevent increased flood risk from surface water run-off. Relevant policy: NPPF Section 14 (Meeting the Challenge of Climate Change,

Flooding and Coastal Change), Core Strategy Policy CP1 and Managing Development Delivery Local Plan Policies CC09 and CC10.

External Lighting

14. No external lighting shall be installed until a report detailing the lighting scheme has been submitted to and approved in writing by the Local Planning Authority. The report shall include the following:
- i) A layout plan with beam orientation;
 - ii) A schedule of equipment;
 - iii) Measures to avoid glare;
 - iv) An isolux contour map showing light spillage to 1 lux both vertically and horizontally and areas identified that could be of importance for commuting and foraging bats; and
 - v) An assessment how the proposed lighting scheme will not adversely affect wildlife and how it will not have an adverse impact on highway safety.

The approved lighting scheme shall thereafter be implemented as agreed and maintained in accordance with the approved details for the lifetime of the planning permission.

Reason: In the interests of biodiversity and highway safety. Relevant Policy: NPPF Section 15, Core Strategy Policies CP1, CP3, CP6 and CP7, and Managing Development Delivery Local Plan Policy TB23.

Sustainability

15. Within three months of the date of this planning permission, an Energy and Sustainability Statement shall be submitted to the Local Planning Authority for a written approval. The Energy and Sustainability Statement include details of measures identified in "Renewable Energy Contribution - Technical Note" dated 15.02.2022 prepared by Wardell Armstrong to achieve 10% reduction in carbon emissions produced by the development hereby approved. The measures so-approved shall be implemented and operational within three months of the Local Planning Authority's approval.

Reason: Although the sustainability objectives weigh in favour for supporting the development, details are required in order to reduce the environmental impact of the facility. Relevant Policy: Core Strategy Policies CP1 and CP3, Managing Development Delivery Local Plan Policies CC05 and CC04 and the Sustainable Design and Construction SPD

Retention of trees and shrubs

16. No trees, shrubs or hedges present along the frontage of 1100 Series plot on Eskdale Road, showing as being retained in the approved plan "Site Layout 2697-TP-101 Rev 7 shall be felled, uprooted, wilfully damaged or destroyed, cut back in any way or removed without previous written consent of the Local Planning Authority; any trees, shrubs or hedges removed without consent or dying or being severely damaged or becoming seriously diseased within 5 years from the completion of the development hereby permitted shall be

replaced with trees, shrubs or hedge plants of similar size and species unless the Local Planning Authority gives written consent to any variation.

Reason: To secure the protection throughout the time that development is being carried out, of trees, shrubs and hedges growing within the site which are of amenity value to the area and to ensure that wildlife is not adversely affected by the proposed development. Relevant Policy: Core Strategy Policies CP3 and CP7, and Managing Development Delivery Local Plan Policies CC03 and TB21.

Informatives:

Planning Obligation

- 1) This permission should be read in conjunction with the legal agreement under Section 106 of the Town and Country Planning Act (yet to be finalised) the contents of which relate to this development.

Highways

- 2) The Corporate Head of Environment at the Council Offices, Shute End, Wokingham should be contacted for the approval of the access construction details before any work is carried out within the highway. This planning permission does NOT authorise the construction of such an access
- 3) If it is the developer's intention to request the Council, as Local Highway Authority, to adopt the proposed access roads etc. as highway maintainable at public expense, then full engineering details must be agreed with the Corporate Head of Environment at the Council Offices, Shute End, Wokingham. The developer is strongly advised not to commence development until such details have been approved in writing and a legal agreement is made with the Council under S38 of the Highways Act 1980.
- 4) Any works/events carried out either by, or at the behest of, the developer, whether they are located on, or affecting a prospectively maintainable highway, as defined under Section 87 of the New Roads and Street Works Act 1991, or on or affecting the public highway, shall be coordinated under the requirements of the New Roads and Street Works Act 1991 and the Traffic management Act 2004 and licensed accordingly in order to secure the expeditious movement of traffic by minimising disruption to users of the highway network in Wokingham.
- 5) Any such works or events commissioned by the developer and particularly those involving the connection of any utility to the site, shall be co-ordinated by them in liaison with Wokingham Borough Council's Street Works Team, (telephone 01189 746302). This must take place at least three months in advance of the works and particularly to ensure that statutory undertaker connections/supplies to the site are coordinated to take place wherever possible at the same time.
- 6) Adequate precautions shall be taken during the construction period to prevent the deposit of mud and similar debris on adjacent highways. For

further information contact the Local Highway Authority on tel: 0118 9746000.

Waste

- 7) The provision for refuse storage shall have regard to the advice on the council's website at <https://www.wokingham.gov.uk/rubbish-and-recycling/collections/waste-information-for-developers/>.

Thames Water – Groundwater Risk Management Permit

- 8) A Groundwater Risk Management Permit from Thames Water will be required for discharging groundwater into a public sewer. Any discharge made without a permit is deemed illegal and may result in prosecution under the provisions of the Water Industry Act 1991. We would expect the developer to demonstrate what measures he will undertake to minimise groundwater discharges into the public sewer. Permit enquiries should be directed to Thames Water's Risk Management Team by telephoning 020 3577 9483 or by emailing wqriskmanagement@thameswater.co.uk. Application forms should be completed on line.

Thames Water – Mains Water

- 9) If you are planning on using mains water for construction purposes, it is important you let Thames Water to avoid potential fines for improper usage. More information and how to apply can be found online at thameswater.co.uk/buildingwater. Thames Water will aim to provide customers with a minimum pressure of 10m head (approx 1 bar) and a flow rate of 9 litres/minute at the point where it leaves Thames Waters pipes. The developer should take account of this minimum pressure in the design of the proposed development.

Advertisement Consent

- 10) This permission does not convey or imply any approval or consent that may be required for the display of advertisements on the site for which a separate Advertisement Consent application may be required. You should be aware that the display of advertisements without the necessary consent is a criminal offence liable to criminal prosecution proceedings through the courts.

Discussion

- 11) The Local Planning Authority has acted positively and proactively in determining this application by assessing the proposal against all material considerations, including planning Policies and any representations that may have been received. This planning application has been the subject of positive and proactive discussions with the applicant in terms of planning issues relating to ecology, employment skills plan, addressing concerns relating to highway safety and surface water drainage.

The decision to grant planning permission in accordance with the presumption in favour of sustainable development as set out in the NPPF is considered to be a positive outcome of these discussions.

C. Alternative recommendation:

That the Planning Committee authorise the Head of Development Management to refuse planning permission in the event of a S106 legal agreement not being completed within three months of the date of the committee resolution (unless longer period is agreed by the chairman of the Planning Committee and confirmed in writing by the Local Planning Authority) for the following reasons:

- 1) In the absence of a habitat biodiversity impact assessment calculator showing otherwise, the proposal is considered to result in a net loss for biodiversity. Moreover, the proposal does not provide biodiversity enhancement through design, layout and landscaping. As such, the proposal is contrary to paragraphs 174(d) and 180(a) of the National Planning Policy Framework and Policy TB23 of the Managing Development Delivery Local Plan.
- 2) In the absence of a Section 106 legal agreement, the Local Planning Authority is unable to secure the implementation of an Employment Skills Plan or the Employment Skills Contribution in lieu that would otherwise enable the Council to secure the equivalent employment outcomes. Therefore, the applicant failed to provide a suitable mechanism to encourage the use of local labour and ensure that local people have the skills and abilities to compete for local jobs, contrary to Policy TB12 of the Managing Development Delivery Local Plan.

RELEVANT PLANNING HISTORY		
Application Number	Proposal	Decision
214088	Full application for soft landscaping works including seating areas and pergolas (1180 Series).	Under consideration
193285	Full application for the proposed change of use from warehouse (Use Class B8) to mixed Use Class B1 and B8 following part demolition of existing building (800 Series).	Approved 13 February 2020
193253	Full application for the external alterations following demolition of units C4 & C5 and parts of C6- C7 & C8 and associated works (800 Series).	Approved 9 January 2020
193125	Full application for the erection of a sports hub (adjoining 1100 Series)	Approved 27 February 2020
162308	Variation of condition 7 of O/2006/9071 (as varied by condition 2 of VAR/2009/0943) relating to highway works	Approved 9 January 2017

VAR/2009/0943	Variation of condition 7 of O/2006/9071 relating to highway works	Approved 12 October 2010
RM/2008/0778	Reserved Matters application in respect of Zone 4 pursuant to Outline Planning Permission O/2006/9071	Approved 2 June 2008
RM/2007/2428	Reserved Matters application in respect of Zone 1 pursuant to Outline Planning Permission O/2006/9071	Approved 30 November 2007
O/2006/9071	Outline planning application for up to 191,000m ² new floorspace for Class B1(a)-(c) (including data centre use), D1, D2, C1 and A1-A5	Approved 2 August 2007
SO/2006/7259	Scoping opinion application for the redevelopment of Winnersh Triangle	Replied 3 May 2006
308308	Outline planning application for the redevelopment of Winnersh Triangle	Approved 1979

SUMMARY INFORMATION																							
Site Area	1.97 hectares																						
Previous land use(s)	Hardstanding, overflow car park																						
Proposed floorspace of each use	7,242m ² of E use class comprising																						
Change in floorspace (+/-)	<table border="0"> <tr> <td>Sound Stage 1</td> <td>1,800m²</td> </tr> <tr> <td>Sound Stage 2</td> <td>1,800m²</td> </tr> <tr> <td>Workshop 1</td> <td>450m²</td> </tr> <tr> <td>Workshop 2</td> <td>450m²</td> </tr> <tr> <td>Workshop 3</td> <td>450m²</td> </tr> <tr> <td>Workshop 4</td> <td>450m²</td> </tr> <tr> <td>Workshop 5</td> <td>450m²</td> </tr> <tr> <td>Workshop 6</td> <td>450m²</td> </tr> <tr> <td>Workshop 7</td> <td>450m²</td> </tr> <tr> <td><u>Workshop 8</u></td> <td><u>450m²</u></td> </tr> <tr> <td>Total</td> <td>7,200m²</td> </tr> </table>	Sound Stage 1	1,800m ²	Sound Stage 2	1,800m ²	Workshop 1	450m ²	Workshop 2	450m ²	Workshop 3	450m ²	Workshop 4	450m ²	Workshop 5	450m ²	Workshop 6	450m ²	Workshop 7	450m ²	<u>Workshop 8</u>	<u>450m²</u>	Total	7,200m ²
Sound Stage 1	1,800m ²																						
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Workshop 3	450m ²																						
Workshop 4	450m ²																						
Workshop 5	450m ²																						
Workshop 6	450m ²																						
Workshop 7	450m ²																						
<u>Workshop 8</u>	<u>450m²</u>																						
Total	7,200m ²																						
Number of jobs created/lost	250 direct jobs (including those arising from associated occupation of other buildings) and 250 indirect jobs																						
Proposed parking spaces	95 proposed, 59 lost																						

CONSULTATION RESPONSES	
Berks, Bucks and Oxon Wildlife Trust	No comments received
Crime Prevention Design Officer	No comments received
Environment Agency	No comments received
ESP Utilities	No objection
Highways England	No objection subject to condition (Condition 9 refers)
National Grid	No comments received
Royal Berkshire Fire and Rescue	Initial observation stated: " <i>The planning Statement mentions - The workshops and sound stages will comprise aluminium frames with tensile fabric canopies. The plans provided show that the roof of</i>

each building will be inflatable. Without knowing the fire resistance of the of the roof material there is the potential that the proposed separation distance between buildings will not be sufficient. In the worst case scenario the distance to the relevant boundary between buildings could be 20m.”

[Officer comment: The applicant is liaising with the RBFRS in relation to fire resistance of materials used. Fire resistance of building materials is covered by Building Regulations, therefore this aspect of the proposal does not amount to a material planning consideration.]

Southern Gas Networks	No objection
SEE Power Distribution	No objection
Thames Water	No objection
WBC Biodiversity	No objection subject to S106 agreement
WBC Economic Prosperity and Place (Community Infrastructure)	No objection subject to S106 agreement
WBC Flood Risk & Drainage	No objection
WBC Environmental Health	No objection subject to condition (please see Paragraph 25)
WBC Highways	No objection subject to conditions (Conditions 4, 5, 6, 7, 8, 9, 10 and 14 refer)
WBC Tree & Landscape	No objection subject to conditions (Conditions 10 and 14 refer)
WBC Cleaner & Greener (Waste Services)	No comments received
WBC Growth and Delivery (Policy)	No objection

REPRESENTATIONS

Winnersh Parish Council:	Supportive of application, no objections to revised plans
Local Members:	No comments received
Neighbours:	No comments received

PLANNING POLICY

National Policy	NPPF	National Planning Policy Framework
	NPPG	National Planning Policy Guidance
Core Strategy (adopted January 2010)	CP1	Sustainable Development
	CP2	Inclusive Communities
	CP3	General Principles for Development
	CP4	Infrastructure Requirements
	CP6	Managing Travel Demand
	CP7	Biodiversity
	CP9	Scale and Location of Development Proposals
	CP15	Employment Development

Managing Development Delivery Local Plan (adopted February 2014)	CC01	Presumption in Favour of Sustainable Development
	CC02	Development Limits
	CC03	Green Infrastructure, Trees and Landscaping
	CC04	Sustainable Design and Construction
	CC05	Renewable energy and decentralised energy networks
	CC06	Noise
	CC07	Parking
	CC09	Development and Flood Risk (from all sources)
	CC10	Sustainable Drainage
	TB11	Core Employment Areas
	TB12	Employment Skills Plan
	TB20	Service Arrangements and Deliveries for Employment and Retail Use
	TB21	Landscape Character
TB23	Biodiversity and Development	
Other	BDG	Borough Design Guide Supplementary Planning Document (adopted June 2012)
	SDC	Sustainable Design and Construction Supplementary Planning Document (adopted May 2010)

PLANNING ISSUES

Proposal Description:

1. This is a retrospective application for a temporary period of five years for the erection of two sound stages and eight workshops for the purpose associated with film production. The location of the proposal is within Winnersh Triangle Business Park which is designated as Core Employment Area within the Core Strategy. The proposed buildings, along with supporting ancillary infrastructure, such as welfare facilities and security huts, would be located across two plots of land: 1040 Series and 1100 Series, Eskdale Road. Landscaping within both sites is very limited given that both plots are largely covered by hardstanding due to having been used to provide overspill car parking.
2. Both plots take access off Eskdale Road at present. The proposal would also include associated works to the access, parking and landscaping. Works have already commenced on site, therefore the proposal is retrospective in nature.

Principle of Development:

3. The National Planning Policy Framework has an underlying presumption in favour of sustainable development which is carried through to the local Development Plan. The Managing Development Delivery Local Plan (MDD LP) Policy CC01 states that planning applications that accord with the Policies in the Development Plan for Wokingham Borough will be approved without delay, unless material considerations indicate otherwise.

Sustainability

4. Policy CC02 of the MDD LP sets out the development limits for each settlement as defined on the Policies map and therefore replaces the proposals map adopted through the Core Strategy, as per the requirement of the Core Strategy Policy CP9. The Core Strategy Policy CP9 sets out that development proposals located within development limits will be acceptable in principle, having regard to the service provisions associated with the major, modest and limited categories.
5. As the site is within a major development location, the proposal is acceptable in principle. It is located in close proximity to Winnersh Triangle Railway Station, which is served by London Waterloo line offering connections to Reading and London, and a Wharfdale Road bus stop which is served by Park & Ride 500 providing connection to Reading (both located approximately 300m south). The site is in a highly sustainable location and raises no concerns on grounds of sustainability of location.

Employment Land

6. Core Strategy Policy CP15 and MDD LP Policy TB11 identify Winnersh Triangle Business Park as a Core Employment Area, where the majority of employment growth would occur in the borough. Core Strategy Policy CP15 permits business, industry or warehouse uses falling within B1, B2 or B8 Use Classes, now superseded. The proposed development would fall within the new Use Class E(g)(iii) which broadly mirrors the use within the B1(3) Use Class (now superseded). Therefore the proposal is considered to fall within a use class permitted by the Core Strategy Policy CP15 and would achieve its aims through the provision and intensification of existing employment uses. The proposal would assist in meeting some of the identified shortfall of industrial floorspace and would not be in conflict with development plan. It should also be noted that the demand for office space has reduced since the adoption of the Policies and even before the onset of the pandemic. This is evidenced by the fact the site has laid empty since 2007.
7. In terms of the number of jobs created, the applicant anticipates this would be approximately 250 jobs on-site as well as 250 indirect jobs. The applicant acknowledges that there is further potential to create 500 direct and indirect jobs in the film and TV production sector through wider 'Winnersh Studios' longer-term proposals.

Local Plan Update

8. The Local Plan Update is at a fairly early stage of preparation and, at the time of writing, has limited weight in the decision-making process. Notwithstanding, the Draft Plan continues to recognise the role Winnersh Triangle Business Park has

within the local and regional economy. Policy ER1 proposes to designate Winnersh Triangle Business Park as a Core Employment Area.

9. The Employment Land Needs Study (2020) supported the Draft Plan and acknowledges that Winnersh Triangle is one of three specialised office parks of a scale and nature in the borough that attract firms across a range of sectors that are particularly distinctive to this part of England and the Thames Valley (paragraph 3.11). According to the study, Winnersh Triangle is one of Wokingham's prime business parks with modern purpose built of high-quality office space.
10. The approach of Policy ER1 encourages evolution of Core Employment Areas, including Winnersh Triangle Business Park, in accordance with economic needs. As set out in foregoing paragraphs, the need for studio space within South-East England has been evidenced.

Need for Facility/Economic Benefits

11. A further consideration is the rapid growth in creative industries as a percentage of UK GVA and specifically increased consumer demand for online film and TV moving image content. Evidence submitted with the application (in section 5 of the Planning Statement) suggests there is a global and national need for the type of development proposed, with an increasing demand for content due to delays in production caused by the Covid-19 pandemic and insufficient stage space to cope with demand. The proposed development would therefore contribute towards a significant evidenced global and national demand.
12. The Planning Statement further makes a case for need within the south-east and close to London, which is where over half of the UK's studio space is already located. Proximity to London allows for benefits including access to high skilled workforce and proximity to Heathrow airport which affords international connectivity, alongside access to the M4.
13. In summary on the issue of need, there is evidence of national and global demand for studio space and workshops as proposed, as well as demand within the wider south-east and London area. The proposed development is therefore capable of helping to address this evidenced need and providing a range of benefits to the local economy. A further consideration is that the proposed development could potentially allow a world-leading multimedia production company to locate and deliver entertainment products within the borough, which would bring associated prestige to the area. It is noted that similar issues were considered as part of the Shinfield Studios proposal at Thames Valley Science Park (210387 and 211841).

Character of the Area:

14. Core Strategy Policies CP1 and CP3 set out the requirement for the development to achieve high quality of design that respects its context and maintains or enhances the quality of the environment. This includes the way development integrates with its surroundings and the use of appropriate landscaping. MDD LP Policy CC03, Trees and Landscaping requires that new development should protect and enhance green infrastructure networks, promoting connectivity between different parts of the network and integrating with adjacent open space.

15. The proposed two sound stages are rectangular, temporary buildings of a footprint of 1,800m² (45m x 40m) each. Together the sound stages would read as one building with its southern elevation facing Eskdale Rd being 45m wide. These temporary structures would have shallow pitched roof covered with inflatable tensile fabric. The maximum height of the stages would be 19.7m (pitch height) with the eaves height being 15.1m. Sound stages would be located along the eastern boundary of parcel 1100 Series
16. Each of the proposed eight workshops is a rectangular structure of a footprint of 450m² (20m x 22.5m). As with sound stages, these would be sited in pairs, with workshops 1-4 located within the western section of 1100 Series plot, parallel to sound stages, and workshops 5-8 located at 1040 Series plot, parallel to Eskdale Rd. The proposal at 1040 Series would be partially obscured from views by the existing ramp between the site and Eskdale Road, which provides access to office buildings at 1010-1030 Series. Workshops would have an aluminium A-frame construction and would have the maximum height of 7.5m (roof pitch) and eaves height of 4.2m, with tensile fabric roof.
17. In terms of the surrounding area, the proposal is sited in a location where office, light industrial and business uses prevail and influence the site surroundings. The type and appearance of buildings varies and can be broadly described as ranging from 6-storey Crowne Plaza hotel at the entrance to Winnersh Triangle Business Park, which measures 27m in height, through 4-5 storey office buildings along the eastern and north-eastern perimeter of the Business Park (e.g. Jacobs) to 2-storey buildings, which are generally located within the central and western sections.
18. The two plots which form the application site (1040 and 1100 Series) sit to the east and west of E2 Series plot (Manhattan Associates). The E2 plot comprises two blocks and atrium offering office space. E2 buildings are approximately 8.7m in height (11m including roof plant) with flat roof and glazed elevations. The E2 building would be over 1m above the pitch of the workshop roofs situated to either site of E2 plot. The applicant has already taken occupation of part of E2 Series as part of their long-term plans for this area (as indicated in the Location Plan).
19. Opposite the application site, there are currently under construction buildings at part of 800 Series site which offers a mix of office, warehouse and distribution uses (application references 193253 and 193285). These buildings will be 9m high (11m including roof plant) with red brick and glazing to office areas and metal cladding across warehouse and distribution elevations. Part of 800 site (units C1-C3) have been occupied by the applicant to assist with pre-production work as indicated by a blue line in the Location Plan.
20. To the west of 1040 Series there is a cluster of four buildings (comprising office and retail uses) and a hotel – together these form Zone 1 of the Winnersh Triangle redevelopment project (outline planning permission ref: O/2006/9071). The building nearest to 1040 Series is a five storey 1030 Series office building, which measures approximately 22m.
21. To the east of 1100 Series there is located an office building at 1080 Series (occupied by Jacobs), which was constructed as part of site-wide redevelopment (RM/2008/0778). The building incorporates office use and measures approximately 20m in height (23m including roof plant) and sits at some 25m above site levels of 1100 due to underground/basement parking. It has a flat roof and glazed elevations.

It is taller than the proposed sound stages, which will be located to the east of 1100 Series building, by approximately 5m.

22. In between 1100 Series and 1180 Series there is a newly erected sports hub which includes a netball/tennis court and a football pitch, together with changing and classroom facilities (193125). The existing structure, which incorporates the changing and classroom facilities, is single storey and located approximately 21m from the eastern elevation of sound stages. The sports hub is for the use by occupiers of the business park and not open to the wider public.
23. The proposal, given its temporary nature, is considered acceptable in terms of its impact on the character of the area given that industrial use units are present (and anticipated) within a mixed-use business park. The proposed sound stages, whilst taller than E2 buildings, would be set back from the from Eskdale Rd by at least 15.5m, i.e. some 7m further than E2 buildings. The single-storey classroom building within the sports hub, whilst lower than sound stages, does not represent the prevailing or anticipated heights in this area (which is dominated by multistorey buildings and warehouse-type structures). It is also acknowledged that the height and massing of the sound stages is heavily dictated by their function. In any event, the sound stages (and workshops) would not exceed the height parameters for buildings within this location set out in previous applications (O/2006/9071). Whilst no proposal has come forward for 1100 site following the outline permission being granted in 2007, an office building (identified as 1D) and a decked car parking were approved within 1040 Series plot. However, these have not been required and both sites remained covered by hardstanding, with very limited landscaping present. It is noted that the frontage of 1100 would retain the avenue planting along Eskdale Road as approved under 193125 and now implemented as part of proposals for a sports hub. It should be noted that should a permanent proposal come forward in the future, the Council would assess the appearance of these and the boundary fencing in more detail and expect high quality proposals.

Neighbour Amenities:

24. The building forming part of the current proposal which is sited in the greatest proximity to an existing building is the pair of workshops 1&2 at 1100 Series, which are located some 27m from the corner of the office building at E2 Series. Due to the height of the proposed buildings and the distance to existing buildings, there are no concerns on loss of light or overbearing grounds. There are no residential dwellings located in close proximity to the site and the nearest one is located over 300m away.
25. In relation to noise generation, the workshops comprise sandwich panel cladding and they will not be subject to noise insulation. The WBC Environmental Health Officer has reviewed the proposal and recommended a condition requiring that doors and windows are shut during filming. Due to their function, sound stages are designed to minimise noise pollution and are soundproofed. It is noted that the proposed use is in line with business park uses and some level of noise generation is expected. Therefore, there are no concerns on grounds of noise pollution that would warrant imposing a condition restricting hours of operation, noise insulation details or filming with doors and windows shut.
26. The proposal does not provide details of on-site lighting that may be required. Given the proximity to Dinton Pastures and A329(M), it is considered that any potential

lighting scheme should be controlled by condition should it be required. Condition 14 refers.

Access and Movement:

27. Core Strategy Policies CP1 and CP6 require consideration of the travel impacts of development, emphasising the importance of reducing the need to travel, particularly by private car. Policy CC07 and Appendix 2 of the MDD LP stipulate minimum parking standards

Access

28. 1100 Series take access directly from Eskdale Road whilst 1040 Series is accessed via a private road connecting to Eskdale Road. The existing access to 1100 Series has been widened by 1m to accommodate movement of HGV vehicles in and out of the site. In addition, a separate pedestrian/cycle access is proposed. 1040 Series is accessed via a one-way system with site entrance located in the south-east corner of the site and exit situated in the north-west section. Both parcels will include security huts located near site entrances to allow security staff to carry out safety checks. Sufficient space has been provided to allow safe pedestrian movement whilst vehicles are waiting to enter the 1100 Series site. Deliveries and Servicing Plan has been secured by Condition **Error! Reference source not found.**

Traffic Impact

29. The applicant submitted a Transport Statement which included an assessment of an impact on the proposal on highway network. The applicant followed the methodology which was accepted for Shinfield Studios application (210387). WBC Highways Officer has reviewed the information submitted and considered that the proposal would not have an adverse impact on highway network.
30. National Highways recommended that a Travel Plan is secured by condition. It is acknowledged that a site-wide Travel Plan is currently being prepared as part of redevelopment of the wider Winnersh Triangle Business Park and that it would need to include the application proposal. However given the temporary nature of the scheme and its location close to the station, the current proposals are considered acceptable until further work has been carried out on the wider Travel Plan. Condition 9 secures a submission of a detailed Travel Plan that will be in general compliance with the approved Framework Travel Plan.
31. The applicant submitted a Framework Construction Method Statement, however, given the advanced progress of construction works, the Method Statement is considered redundant at this stage. Moreover, it is noted that the Council received no complaints in relation to the construction activities associated with this proposal.

Parking

32. Core Strategy Policy CP6 Managing Travel Demand and MDD LP Policy CC07 Parking require appropriate vehicle parking, in line with the Council's standards set out at Appendix 2 of the MDD LP. Principles P2 and P3 of the BDG SPD require that cycle parking is conveniently located, secure and undercover and provided where it is compatible in the streetscene.

33. In establishing their car parking requirements, the applicant adopted the methodology agreed in Shinfield Studios application (210387). It is proposed that there would be 95 parking spaces measuring at least 2.5m x 5m (65 at 1100 plot and 30 at 1040 plot), of which 6 would be accessible. There would also be 5 active and 5 passive electric vehicle charging spaces and 6 motorbike parking spaces. Parking for this use is expected to be far less than what is required for an office building, so the use would improve conditions over what has been consented by the outline. The WBC Highways Officer has reviewed the parking provision and raised no objections subject to parking management plan being secured by condition (Condition 10 refers).
34. It is noted that the proposals would remove some existing parking spaces however this is not considered to result in significant impacts upon local parking, the functioning of the site or the local highway network given the extent of public transport that serve the site and the current need. Any future permanent proposals would be expected to address this. In any event, the applicant confirmed that these spaces previously were used as overflow parking and are no longer required.
35. Cycle parking would be well distributed between both parcels with 4 cycle stands installed at plot 1040 and 8 cycle stands installed at plot 1100. The total of 12 cycle parking spaces has been considered by the WBC Highways Officer and is acceptable given that the use of cycle parking will be monitored and additional spaces provided should there be additional demand, as set out in the Framework Travel Plan (Condition 9 refers). Details of cycle parking would be secured under Condition 7.

Flooding and Drainage:

36. Policy CC09 of the MDD LP requires consideration of flood risk from historic flooding. MDD LP Policy CC10 requires surface water to be managed in a sustainable manner.
37. The site is located within Flood Zone 2. A Flood Risk Assessment and Surface Water Drainage Maintenance & Management Plan have been submitted as part of the application. The WBC Drainage Officer has reviewed the proposal and raised no objections.

Landscape and Trees:

38. Policy CC03 of the MDD LP aims to protect green infrastructure networks, retain existing trees and establish appropriate landscaping, and Policy TB21 requires consideration of the landscape character.
39. Both sites contain very limited landscaping and no trees are proposed to be removed as part of this application, although it is noted that some trees have been removed as part of separate planning application (193125) which involved upgrading works to landscaping south and east of 1100 Series (as part of a new sports hub proposal east of the 1100 Series). A site visit confirmed that landscaping works associated with the sports hub proposal were being implemented.
40. WBC Trees and Landscape Officer has reviewed the proposal and raised no objections, subject to conditions (Conditions 11 and 16 refer).

Environmental Health:

41. A portion of the 1040 Series site (south/west corner) is located within an area of potentially contaminated land. However the proposal would result in minimal ground intrusion given that it utilises ground anchors and precast interlocking concrete blocks for foundations. The WBC Environmental Health Officer has reviewed the proposal and raised no concerns on contamination ground. Therefore, it is considered the proposal would not result contamination issues.

Ecology:

Protected Species

42. Core Strategy Polices CP3 and CP7 establish that proposals should not have a detrimental impact on ecological features. Species and habitats of conservation value should be protected and the ability of a site to support fauna and flora, including protected species, should be maintained and enhanced. Where the need for development outweighs the need to safeguard nature conservation importance and there are no less harmful alternatives available harm should be mitigated or compensated for. In addition, MDD LP Policy TB23 requires proposals to enhance and incorporate new biodiversity features, provide appropriate buffer zones between development and designated sites as well as habitats and species of principle importance for nature conservation and ensure ecological permeability.
43. This application is supported by a short ecological appraisal report which contains a summary of a walkover survey and desktop survey.
44. WBC Ecology Officer has reviewed the proposal and commented that due to the design of the proposed structures, the proposal is unlikely to have a detrimental impact (light spill) on the Dinton Pastures Country Park (in particular on the light-sensitive bat species) in the present form. Condition 14 ensures that any proposed external lighting scheme is first approved by Local Planning Authority.

Biodiversity Net Gain

45. It is appropriate under local plan Policy TB23 and NPPF paragraph 174 for the Local Planning Authority to seek a biodiversity net gain (ecological enhancement) in the course of development.
46. The ecological appraisal report identified a relatively small area of bare ground with a covering of woodchip and occasional ruderal plant. Bare ground represents habitat of low distinctiveness, nevertheless, it would, as a result of conversion into sealed surface, represent a net biodiversity loss. The applicant proposes to address this through the provision of biodiversity net gain off-site. The off-site provision is considered appropriate in this instance and is supported by the WBC Ecology Officer. The biodiversity net gain would be secured via a Section 106 Legal Agreement.

Sustainable Design/Construction:

47. Core Strategy Policy CP1 requires that proposals contribute towards the goal of achieving zero carbon development by including on-site renewable energy features and minimising energy and water consumption. This is further emphasised in MDD LP Policies CC04 and CC05, and the Sustainable Design and Construction SPD.
48. Notwithstanding this, the applicant submitted a Technical Note which confirms that the 10% reduction in carbon emissions is achievable through the use of solar photovoltaic system. At the time of writing of this report, the applicant confirmed they are exploring options to mount these on sound stages 1 and 2 and it has been confirmed there is sufficient space to install enough panels to meet Policy requirements. Condition 15 secures further details and the implementation of the measures proposed therein along with other sustainability measures.
49. Area intended for waste storage is provided within both parcels. Sound stages and workshops are demountable and as such, they can be reused, thereby contributing to providing sustainable design and construction benefits. Additional sustainability measures would be secured by Condition 15.

Employment Skills:

50. MDD LP Policy TB12 indicates that proposals for major development should be accompanied by an Employment and Skills Plan (ESP) to show how the proposal accords opportunities for training, apprenticeship or other vocational initiatives to develop local employability skills required by developers, contractors or end users of the proposal.
51. The proposed scheme amounts to major development given the gross increase in floorspace exceeding 1,000m², therefore the scheme is required to provide seven opportunities for community skills support, four apprenticeships and three jobs created. As an alternative, the applicant may choose to pay Employment Skills Contribution (ESC) in lieu of delivering ESP which would be used to meet costs of the Council overseeing and supporting each of employment target. The requirement to deliver ESP or pay ESC in lieu would be secured through a Section 106 Legal Agreement.

Community Infrastructure Levy (CIL)

52. The application is not liable for CIL payments because it is for a commercial use only.

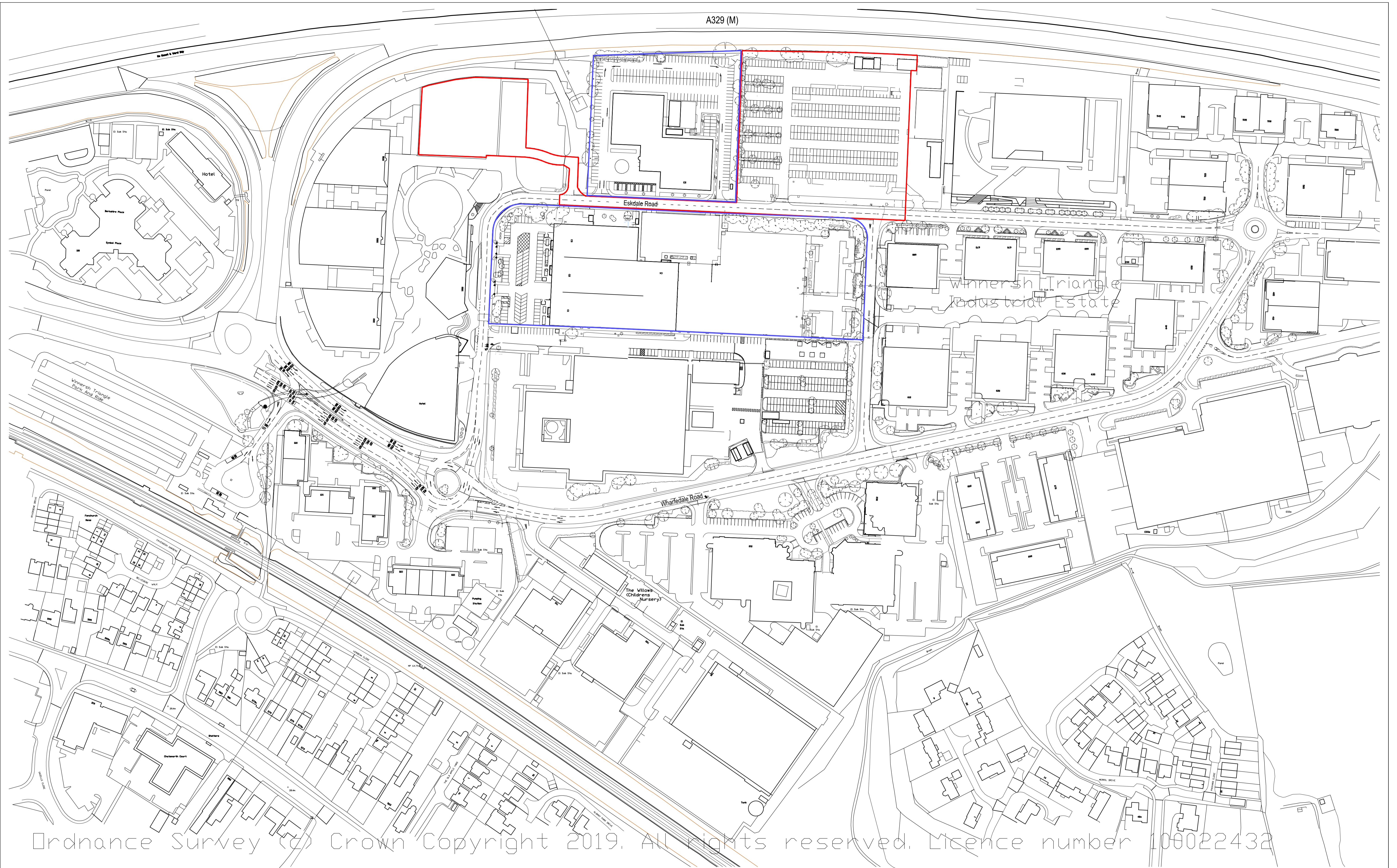
The Public Sector Equality Duty (Equality Act 2010)

53. In determining this application, the Council is required to have due regard to its obligations under the Equality Act 2010. The key equalities protected characteristics include age, disability, gender, gender reassignment, marriage and civil partnership, pregnancy and maternity, race, religion or belief. Policy CP2 of the Core Strategy also seeks to ensure that new development contributes to the provision of sustainable and inclusive communities, including for the disabled.
54. There is no indication or evidence (including from consultation on the application) that persons with protected characteristics as identified by the Act have or will have

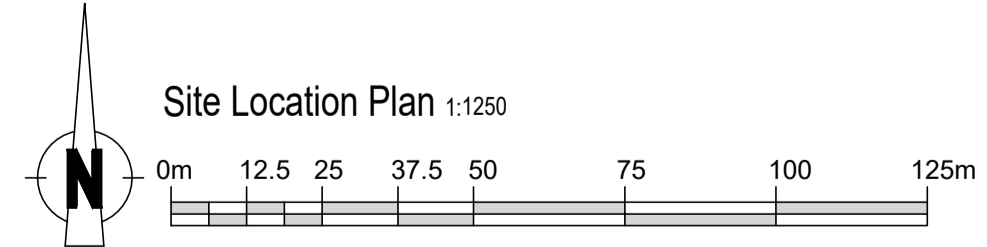
different needs, experiences, issues and priorities in relation to this particular planning application and there would be no significant adverse impacts as a result of the development. Within the development, level access is provided to the workshops and sound stages and a total of 6 parking spaces, or 6% of the total spaces, are assigned for disabled use.

CONCLUSION

The National Planning Policy Framework has an underlying presumption in favour of sustainable development which is carried through to the local Development Plan. The Managing Development Delivery Local Plan Policy CC01 states that planning applications that accord with the Policies in the Development Plan for Wokingham Borough will be approved without delay, unless material considerations indicate otherwise. The Core Strategy supports the proposed use in the location of Winnersh Triangle Business Park and the scheme would bring substantial economic and social benefits to the local area. There are no material considerations that would indicate that the planning application should be refused, therefore the planning application is recommended for approval.



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Revisions	Drawings Notes
P - 25.11.2021 - Draft Issue for review.	1. this is not a survey drawing - all dimensions to be checked on site.
PT - 21.12.2021 - Planning Issue.	2. do not scale off this drawing.
	3. the copyright of this drawing is the property of TPA Design Ltd
	4. all discrepancies to be reported to the architect
	5. please ensure that this drawing is read together and cross referenced with all other relevant architectural drawings, schedules and specifications.
	6. please ensure that this drawing is read in conjunction with all other consultants/specialist contractors relevant drawings, schedules and specifications.
	7. TPARCHITECTS is a trading name of TPA Design Ltd Registered address 33a St Lukes Road, Maidenhead, Berkshire SL67DN.

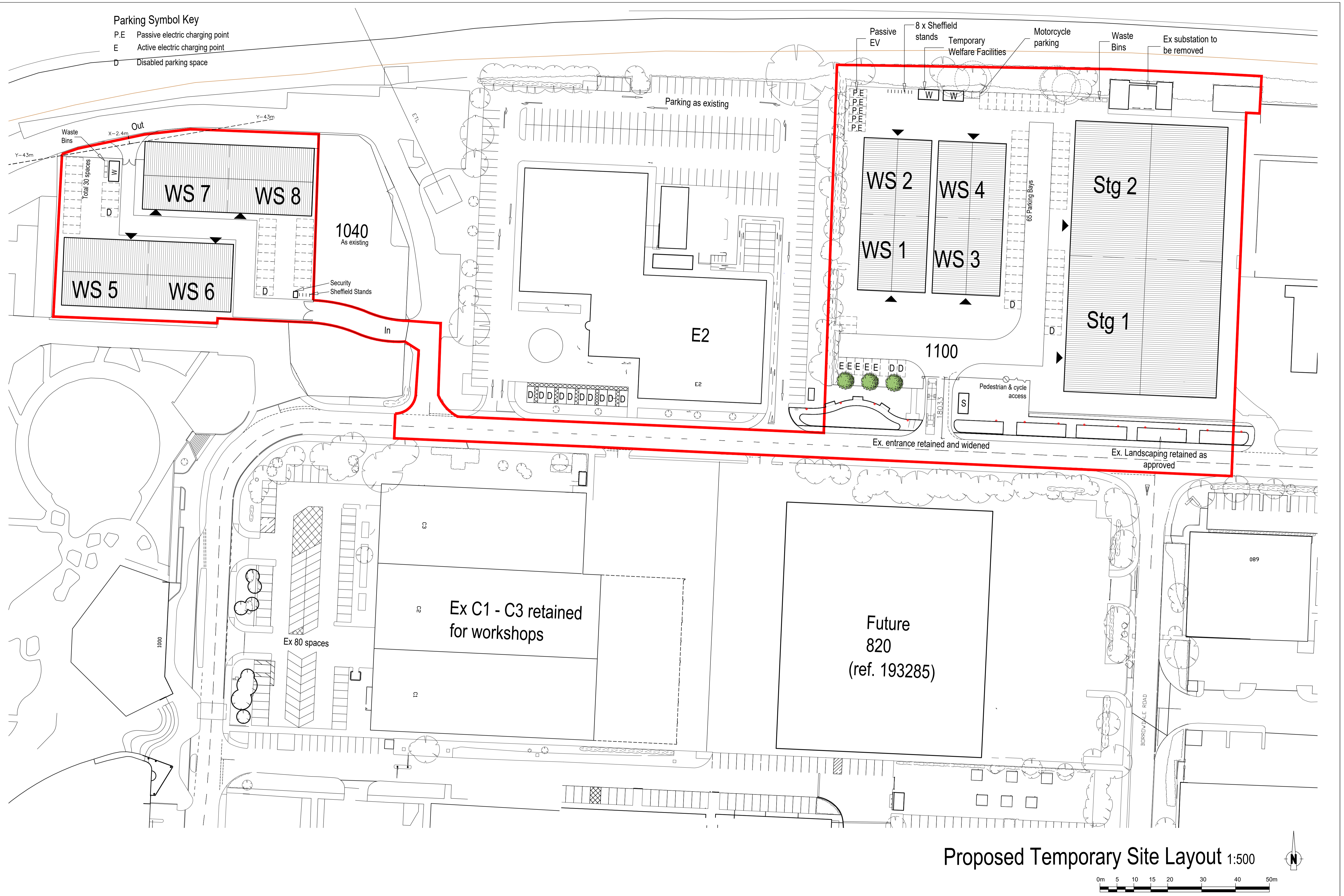
Project		Winnersh Triangle Business Park Eskdale Road Reading, RG41 5TS	
Title		Winnersh Film Studio Location Plan	
Drg. No.	Rev	Drawn	Checked
2697-TP-100	P1	GVL	ED
Date	Scale	Status	
Dec'21	As Shown	T.Planning	

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Parking Symbol Key

- P.E Passive electric charging point
- E Active electric charging point
- D Disabled parking space



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Proposed Temporary Site Layout 1:500



Revisions	Drawings Notes
P - 25.11.2021 - Draft Issue for review.	1. this is not a survey drawing - all dimensions to be checked on site.
P1 - 21.12.2021 - Planning Issue.	2. do not scale off this drawing.
P2 - 07.02.2022 - Minor amendments	3. the copyright of this drawing is the property of TPA Design Ltd
P3 - 14.02.2022 - Minor amendments	4. all discrepancies to be reported to the architect
P4 - 15.02.2022 - Minor amendments	5. please ensure that this drawing is read together and cross referenced with all other relevant architectural drawings, schedules and specifications.
P5 - 16.02.2022 - Minor amendments	6. please ensure that this drawing is read in conjunction with all other consultants/specialist contractors relevant drawings, schedules and specifications.
P6 - 18.02.2022 - Officer comments	7. TPARCHITECTS is a trading name of TPA Design Ltd Registered address 33a St Lukes Road, Maidenhead, Berkshire SL67DN.
P7 - 23.02.2022 - Minor amendments	

Project	Winnersh Triangle Business Park Eskdale Road Reading, RG41 5TS
Title	Winnersh Film Studio Proposed Temporary Site Layout
Drg. No.	2697-TP-101
Rev	P7
Drawn	GVL
Checked	ED
Date	Dec'21
Scale	1:500
Status	T.Planning

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Eskdale Road- Eastern Street View

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Eskdale Road- Western Street View

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PLANNING REF : 214183
PROPERTY ADDRESS : Winnersh Community Centre
: New Road, Sindlesham, Wokingham
: RG41 5DX
SUBMITTED BY : Winnersh Parish Council
DATE SUBMITTED : 16/02/2022

COMMENTS:

Winnersh Parish Council has no observations to make on the revised plans, other than having no objections.

PLANNING REF : 214183
PROPERTY ADDRESS : Winnersh Community Centre
: New Road, Sindlesham, Wokingham
: RG41 5DX
SUBMITTED BY : Winnersh Parish Council
DATE SUBMITTED : 18/01/2022

COMMENTS:

Given the significant economic benefits, both direct (including employment) and indirect through the supply chain, that would arise from the development of facilities to facilitate the filmingcreative arts sector, Winnersh Parish Council is supportive of the application.

Agenda Item 85.

Application Number	Expiry Date	Parish	Ward
214108	24 March 2022	Wargrave	Remenham, Wargrave and Ruscombe

Applicant	Hare Hatch Sheeplands
Site Address	Hare Hatch Sheeplands, London Road, Twyford RG10 9RS
Proposal	Full application for the proposed redevelopment of existing mixed-use site to a new garden centre with associated play area, allotments, reconfigured parking and servicing, landscaping and other associated infrastructure, replacement cafe, demolition of existing glasshouses, and retention of existing farm shop, site office/toilet block
Type	Full
Officer	Simon Taylor
Reason for determination by committee	1) Major application (site area >1 hectare and floorspace >1000sqm) 2) Assistant Director decision that committee determination is in the public interest

FOR CONSIDERATION BY	Planning Committee on Wednesday 9 March 2022
REPORT PREPARED BY	Assistant Director – Place

SUMMARY
<p>Hare Hatch Sheeplands is situated in the Green Belt on a prominent corner location at the intersection of Bath and London Roads, Hare Hatch.</p> <p>It comprises a large collection of greenhouses and buildings that were originally used as a horticultural nursery with ancillary sales but through a long history of planning applications, now includes retail floorspace, reptile display and exhibition space (all temporary), events use (periodic and temporary), reptile display, farm shop and café with outdoor seating. There is no planning permission for use as a garden centre. A gravel car park and service yard dominate the centre of the site and a residential dwelling with an occupancy condition tied the horticultural use is in the south western corner (but outside of the red line area).</p> <p>The proposal represents a change of use of the site from its horticultural/mixed use to create a retail garden centre. It includes a wholesale redevelopment of the site to include a much larger retail component in a new building, retention of a farm shop and café and provision of open space and allotment gardens.</p> <p>There is demolition of a large amount of partly dilapidated greenhouses (lightweight structures, some of which are required to be demolished when no longer required for horticultural use) and construction of a permanent modern building with a net reduction in building footprint and volume across the site. The children’s play equipment is open to the public, additional woodland planting is proposed in the north western corner of the site, net biodiversity gain is achieved and the allotment gardens are intended for the sale of produce in the farm shop. There are 381 representations in support and one against. The representations come from a very wide area and are not all local or from within the Borough. Consultee responses have not raised objection.</p> <p>However, the proposed garden centre building has a more readily apparent height and form when viewed from New Bath Road and from views within the site. The retail use represents an intensification in the use of the site. In NPPF terms, the proposal is inappropriate development in the Green Belt with the application turning on the</p>

consideration of whether the site is previously developed land. This forms the basis for the refusal of the application in Reasons for Refusal 1 and 2.

PLANNING STATUS

- Green Belt
- Green Route (Bath Road)
- Landfill gas consultation zone
- Potentially contaminated land consultation zone
- Radon affected area
- Groundwater zone 3
- Nitrate vulnerable zone (surface water)
- AWE Burghfield nuclear consultation zone (special case zone)
- Bat consultation zone
- Non-classified road
- Heathrow Aerodrome consultation zone

RECOMMENDATION

That the committee authorise the REFUSE TO GRANT OF PLANNING PERMISSION for the following reasons:

1) Inappropriate development in the Green Belt

The increase in permanent retail and café floorspace represents inappropriate development within the Green Belt without sufficient very special circumstances. It is an unacceptable and unsustainable form of development resulting in harm contrary to Section 13 of the National Planning Policy Framework 2021, Policies CP1, CP3, CP11 and CP12 of the Core Strategy 2010 and Policies TB01 and TB21 of the Management Development Delivery Local Plan 2014.

2) Harm to the character of the area

By virtue of the intensified activity associated with the change of use to retail and the increased bulk closer to New Bath Road, the harm to the character and spatial openness of the Green Belt and countryside is contrary to Section 13 of the National Planning Policy Framework 2021, Policies CP1, CP3, CP11 and CP12 of the Core Strategy 2010 and Policies TB01 and TB21 of the Management Development Delivery Local Plan 2014.

3) Lack of employment skills plan

In the absence of a legal agreement, the proposal does not make provision for an employment skills plan, contrary to Policy TB12 of the Managing Development Delivery Local Plan 2014.

Informatives

1) Refused plans

This decision is in respect of the plans numbered M07.188.D.017 Rev K, M07.188.D.018 Rev D, M07.188.D.023 Rev K, M07.188.D.024 Rev H, M07.188.D.025 Rev G, M07.188.D.026 Rev B, M07.188.D.027 Rev I, M07.188.D.029

Rev E, M07.188.D.030 Rev B, M07.188.D.031, RSE_5316_BIA Proposed Rev V1 and RSE_5316_BIA Baseline Rev V1, received by the Local Planning Authority between 17 December 2021 and 5 January 2022.

2) Discussion

The Council engages with all applicants in a positive and proactive way through specific pre-application enquiries and the detailed advice available on the Council's website. On this particular application, pre-application advice was sought and some discussion took place in trying to find a solution, but no solution was possible on account of the unacceptable nature of the principle of development.

PLANNING HISTORY

App No.	Description	Decision/Date
03604	Erection of glasshouses	Approved 15 December 1976
39618	Demolition of glasshouses and replacement with polytunnels and barn	Approved 2 September 1993
F/1995/63187	Erection of barn for horticultural use	Approved 27 September 1996
F/1997/66297	Erection of several greenhouses for horticultural use	Approved 3 November 1997
F/2001/4820	Erection of barn, offices, glasshouses and toilet block	Withdrawn 6 November 2001
F/2001/5225	Erection of glasshouses and toilet block with office and staff room	Approved 9 January 2002
F/2002/7504	Erection of one detached dwelling	Approved 22 January 2003
F/2003/0195	Construction of basement below the detached dwelling	Refused 5 November 2003
A/04/1136843	Appeal against refusal of F/2003/0195	Upheld 23 July 2004
F/2003/8706	Erection of building for Class A1 use (retail) with a floor area of 195m ²	Refused 28 April 2003
F/2004/2238	Erection of building of 180m ² for retailing of pet foods and accessories	Refused 27 August 2004
F/2007/0225	Change of use of barn to farm shop (retail)	Approved 25 May 2007
A/07/2054755	Appeal against Condition 5 of F/2007/0225 (produce is to be within a 10-mile radius)	Upheld 23 April 2008
F/2007/0226	Change of use of display area to a restaurant (156m ²) with 80 covers	Refused 23 April 2007
F/2007/1428	Change of use of display area to café/coffee shop with dry goods storage.	Refused 27 July 2007
A/07/2054755	Appeal against refusal of F/2007/1428	Upheld 23 April 2008
A/2007/2038	Retrospective approval for four free standing advertisement signs	Part approved/ refused 17 October 2007
F/2008/0038	Change of use from 2200m ² floor area of the glasshouses to retail sales and display (A1)	Refused 20 March 2008
F/2008/2295	Extension to farm shop to provide butchery (part retail)	Approved 2 February 2009

App No.	Description	Decision/Date
03604	Erection of glasshouses	Approved 15 December 1976
A/2008/0444	Two freestanding sign boards (one double sided)	Approved 24 April 2008
F/2008/2766	Conversion of basement in bungalow to one unit of residential accommodation.	Refused 10 March 2009
A/09/2104930	Appeal against refusal of F/2008/2766	Dismissed 2 September 2009
F/2011/0648	Redevelopment of horticultural site including increase in retail space, replacement buildings, revised parking and landscaping	Refused 19 December 2011
A/12/2171958	Appeal against refusal of F/2011/0648	Withdrawn 7 November 2012
F/2011/2117	Two display conservatories	Refused on 28 November 2011
CLE/2014/1462	Certificate of existing lawful development for sale of retail goods	Refused 31 March 2015
152747	Change of land and building to play area and recreational farm	Declined to determine
152748	Change of use of part of existing building to retail	Declined to determine
160677	Certificate of existing lawful development for sale of retail goods	Refused 13 May 2016
171478	Four non-illuminated free-standing boards	Approved 7 July 2017
173316	Temporary CoU of part glasshouse and outdoor area to retail sales	Declined to determine
W/18/3193969	Appeal against non-determination of 173316	Upheld 14 March 2019
172850	CoU of part glasshouse and outdoor area to retail sales	Declined to determine
172161	Variation of F/2008/2295 for use of farm shop as fishmonger as well as butchery	Approved 4 September 2017
191517	Canopy and support struts to farm shop	Refused 5 September 2019
191518	Expansion of café floor area	Refused 10 September 2019
191520	Extension of car park, use of service yard for all uses and mobile dinosaur	Approved 10 September 2019
191519	Use of a greenhouse as a residential timber store	Approved 17 September 2019
192841	Replacement advertising signage	Approved 21 November 2019
192912	Change of use of land for storage of cars	Withdrawn 15 January 2020
192018	CoU of three existing nursery glasshouses into events area relating to existing nursery	Approved 11 March 2020
192841	Two illuminated entrance boards and three illuminated boards	Approved 21 November 2019
201047	Temporary (3 years) change of use to house Quirks' Animal Roadshows Exotic Animals	Approved 23 June 2020

App No.	Description	Decision/Date
03604	Erection of glasshouses	Approved 15 December 1976
201693	Replacement greenhouse with café kitchen, butcher storage and public toilets	Approved 10 September 2020
201777	Two canopies and AC unit supports to the farm shop and a CoU of outdoor plant storage for use as an outdoor café seating area	Approved 16 September 2020
203533	Five non illuminated banner signs	Approved 9 February 2021
211084	CoU of greenhouse from horticulture to exhibition space for display of National Collection of Hawthornia and other plans	Approved 11 August 2021
211085	Application to vary condition 1 of 192018 to extend temporary permission for two years	Approved 11 August 2021
211086	Continued use of existing sales area in 173316 and CoU to include additional sales	Approved 11 August 2021
A/21/3282720	Appeal against condition in 211085	Not yet commenced
A/21/3287734	Appeal against condition in 211086	Not yet commenced

SUMMARY INFORMATION

Note: Due to the scale of the site and the variable condition and location of buildings, there is variance between the applicant's figures and the Council's figures. Nothing in the assessment turns on this aspect.

	Existing	Proposed
Site Area	2.85 hectares (current site)	3.57 hectares (includes fields to the south)
Land use	Mixed use with horticultural origins (includes farm shop, retail, café and other uses)	Retail garden centre with ancillary uses
Floorspace	9,973m ² (applicant's figure) 11,190m ² (Council's figure)	4,154m ² (applicant's figure) 5,050m ² (Council's figures)
External space	3,557m ² (applicant's figure) 4,260m ² (Council's figure)	2,000m ² (applicant's figure - retail only)
Employment	32 FTE (12 FT and 20 PT)	69 FTE (25 FT and 44 PT)
Car parking spaces	201 (plus unimplemented approval for 40)	192 (with overflow area of 91)

CONSULTATION RESPONSES

Internal

Drainage	No objection.
Highways	No objection, subject to conditions relating to provision of car parking and turning, EV charging, parking management, delivery servicing, cycle parking, lighting, Travel Plan and pedestrian and vehicle access.
Ecology	Although there are departures with respect to calculations of biodiversity net gain, there is still a satisfactory biodiversity net gain for the site and subject to conditions, no objection is raised.
Trees and Landscaping	No objection, subject to further details relating to woodland planting, play equipment and allotment treatments by condition.

	The proposed species in the woodland planting is opposed as noted in paragraph 106.
Employment Skills	No objection, subject to employment skills provisions secured by legal agreement.
Environmental Health	No objection.
Growth and Delivery	No objection to the consideration of the sequential test and retail impact assessment.
Economic Prosperity	No comments received.
External bodies	
Thames Water	No objection.
Thames Valley Police	No in-principle objection, subject to further details relating to external lighting and CCTV, as required by condition.
Natural England	No objection.
Fire and Rescue	No objection, subject to observations.
Environment Agency	No comments received.
National Grid	
Southern Gas	
SEE Power	
Wildlife Trust	

REPRESENTATIONS	
Wargrave Parish Council	No objection, subject to improved visibility splay to the right of the existing access. <u>Officer comment:</u> The Council's Highways Officer has agreed that the sightlines at the entrance are satisfactory. Refer to paragraph 96.
Ward Members	No ward member representations were received. However, two members from adjacent or nearby wards have written in support. Cllr Lindsay Ferris (member for neighbouring ward of Twyford) supports the application, noting the following comments: <ul style="list-style-type: none"> • Improvements to the site • Increased biodiversity • Supportive of the Council's climate emergency • Suitable use in the Green Belt • Improved services Cllr David Hare (member for Hawkedon) supports the application, noting the following comments: <ul style="list-style-type: none"> • Will enable continued community support • Improved openness <u>Officer comment:</u> The above points form part of the planning balance consideration at paragraph 133-139.
Neighbours	The application was consulted to neighbours, a site notice was installed and an advertisement placed in the local paper, advising of the consultation period to 2 February 2022. In addition to the two ward member submissions noted above, 379 submissions of support were received and one submission against.

Support

- 1) 1 Berrylands Road, Caversham RG4 8NU
- 2) 1 Chittering Close, Lower Earley RG6 4BE
- 3) 1 Clifton Rise, Wargrave RG10 8BN
- 4) 1 Cumberland Drive, Bracknell RG12 2PJ
- 5) 1 Fontwell Drive, Reading RG30 4QR
- 6) 1 Grassy Lane, Maidenhead SL6 6AU
- 7) 1 Harvest Place, Wargrave RG10 8AQ
- 8) 1 Hill Cottages, Milley Lane, Hare Hatch RG10 9TJ
- 9) 1 Ilex Close, Sonning Common RG4 9LG
- 10) 1 Maple Bank, Ruscombe RG10 9AZ
- 11) 1 Mendip Close, Charvil RG10 9QE
- 12) 1 Royal Station Court, Station Road, Twyford RG10 9NF
- 13) 1 Snowdrop Grove, Winnersh RG41 5UP
- 14) 1 Stephenson Close, Twyford RG10 9FG
- 15) 1 Sycamore Drive, Twyford RG10 9HP
- 16) 10 Ashbourne Grove, Maidenhead SL6 3ER (x3)
- 17) 10 Cromwell Road, Maidenhead SL6 6BJ
- 18) 10 Pembroke Place, Caversham RG4 5HU
- 19) 10 Spruce Road, Woodley RG5 4BB (x3)
- 20) 10 The Hedgerows, Woodley RG5 3BQ
- 21) 10 Woodfield Court, Woodley RG5 4JE
- 22) 10 Woodmere Close, Earley RG6 5QU
- 23) 10 Yew Tree Way, Woodley RG5 4FF
- 24) 105 Fleetham Gardens, Lower Earley RG6 4BZ
- 25) 105 The Hawthorns, Charvil RG10 9TT
- 26) 106 Pinkneys Road, Maidenhead SL6 5DN
- 27) 108 Galsworthy Drive, Caversham RG4 6PP
- 28) 108 Highfield Park, Wargrave RG10 8LE
- 29) 11 Herald Way, Woodley RG5 4PB (x2)
- 30) 11 Kibblewhite, Twyford RG10 9AX
- 31) 11 Rock Farm Lane, Sandford On Thames OX4 4YL
- 32) 11 St Bartholomews Road, Reading RG1 3QA
- 33) 11 Tamarisk Rise, Wokingham RG40 1WG (x3)
- 34) 11 The Hawthorns, Charvil RG10 9TS
- 35) 11 Thistleton Way, Lower Earley RG6 3BD
- 36) 111 Howth Drive, Woodley RG5 3DJ
- 37) 116 Broomhill, Cookham SL6 9LQ
- 38) 12 Bayliss Road, Wargrave RG10 8DR (x3)
- 39) 12 Bramber Mews, Caversham RG4 6NN
- 40) 12 Chiltern Drive, Charvil RG10 9QF
- 41) 12 Corbett Gardens, Woodley RG5 4JY
- 42) 12 Maiden Erlegh Drive, Earley RG6 7HP
- 43) 12 Malvern Way, Twyford RG10 9PX
- 44) 12 Northbury Avenue, Ruscombe RG10 9LG
- 45) 12 Sunderland Close, Woodley RG5 4XR (x3)
- 46) 12 Tiverton Close, Woodley RG5 3BE
- 47) 12 Weavers Way, Twyford RG10 9GX
- 48) 120 Barkham Road, Wokingham RG41 2RP
- 49) 127 St Saviours Road, Reading RG1 6EP
- 50) 14 Greenacres Avenue, Winnersh RG41 5SX

- 51) 14 Manners Road, Woodley RG5 3EA
- 52) 14 Rothwell Gardens, Woodley RG5 4TJ
- 53) 14 Wessex Gardens, Twyford RG10 0AY
- 54) 142 Antrim Road, Woodley RG5 3NZ
- 55) 143 Broad Hinton, Twyford RG10 0XJ
- 56) 15 Eynsham Close, Woodley RG5 4LF (x3)
- 57) 15 Foxes Walk, Charvil RG10 9TX
- 58) 15 Orpington Close, Twyford RG10 0AD
- 59) 15 Plover Close, Wokingham RG41 3JD
- 60) 15a Braybrooke Road, Wargrave RG10 8DU
- 61) 16 Church End Lane, Tilehurst RG30 4UT
- 62) 16 Milton Road, Wokingham RG40 1DB
- 63) 16 Orchard Estate, Twyford RG10 9JY
- 64) 16 Orwell Drive, Arborfield Green RG2 9US
- 65) 16 Radcot Close, Woodley RG5 3BG
- 66) 16 Rosedale Crescent, Earley RG6 1AS
- 67) 16 Sandhills Way, Calcott RG31 7PQ
- 68) 16 Spring Meadow, Bracknell RG12 2JP
- 69) 16 The Hydes, Tilehurst RG316GD
- 70) 160 Broad Hinton, Twyford RG10 0XH
- 71) 162 Victoria Road, Wargrave RG10 8AJ (x2)
- 72) 169 Pitshanger Lane, London W5 1RQ
- 73) 17 Budes Road, Wokingham RG40 1PL
- 74) 17 Bythorn Close, Lower Earley RG6 3BH
- 75) 17 Hollins Walk, Reading RG30 2BU
- 76) 17 Home Farm Close, Reading RG2 7TD
- 77) 17 Spruce Road, Woodley RG5 4BB
- 78) 177 Broad Hinton, Twyford RG10 0XA
- 79) 18 Butts Hill Road, Woodley RG5 4NH
- 80) 18 Church View, White Waltham SL6 3JQ
- 81) 18 Gingells Farm Road, Charvil RG10 9DJ
- 82) 18 Lismore Close, Woodley RG5 3RT
- 83) 18 Lowfield Road, Caversham RG4 6PA
- 84) 18 Oaklands, Bulmershe Road, Reading RG1 5RW
- 85) 18 Orchardville, Burnham Village SL17BD
- 86) 184 Victoria Road, Wargrave RG10 8AJ (x2)
- 87) 19 Elizabeth Court, Victoria Road, Wargrave RG108BP
- 88) 19 Fern Drive, Taplow SL6 0JS
- 89) 19 Moor Copse Close, Earley RG6 7NA
- 90) 19 Saxon Close, Wallingford OX10 0SR
- 91) 19 THornbers Way, Charvil RG10 9DW
- 92) 196 Kingfisher Drive, Woodley RG5 3LQ (x5)
- 93) 198 Kingfisher Drive, Woodley RG5 3LH
- 94) 2 Allenby Road, Maidenhead SL6 5BB
- 95) 2 Chaffinch Close, Wokingham RG41 3HN
- 96) 2 Cheviot Drive, Charvil RG10 9QD
- 97) 2 Corby Close, Woodley RG5 4TL
- 98) 2 Denmark Avenue, Woodley RG5 4RS
- 99) 2 Diamond Cottages, Warren Row RG10 8QT
- 100) 2 Greenwood Grove, Wokingham RG41 5LH
- 101) 2 Headley Close, Woodley RG5 4SF
- 102) 2 Nursery End, Whistley Green RG10 0ED

- 103) 2 Old Bath Road, Charvil RG10 9QR
- 104) 2 Seymour Place, 36 Warwick Road, Beaconsfield HP9 2PE
- 105) 2 Twynham Road, Maidenhead SL6 5AS
- 106) 20 Lower Armour, Tilehurst RG31 6HH
- 107) 20 Bass Mead, Cookham SL6 9DJ
- 108) 20 Ravensbourne Drive, Woodley RG5 4LH
- 109) 20 Ridgeway, Wargrave RG10 8AS
- 110) 21 Capercaillie Close, Bracknell RG12 8AX (x2)
- 111) 21 Dowding Close, Woodley RG5 4NL
- 112) 21 Hearne Drive, Holyport SL6 2HZ
- 113) 21 High Street, Wargrave RG10 8BU
- 114) 21 Nelson Road, Caversham RG4 5AT
- 115) 21 St Michael's Court, Ruscombe RG10 9UF
- 116) 212 Wensley Road, Coley Park RG1 6DP
- 117) 22 Meadow Road, Wokingham RG41 2TD
- 118) 22 Palmerstone Road, Earley RG6 1HL
- 119) 22 The Croft, Maidenhead SL6 4BA
- 120) 22 Vauxhall Drive, Woodley RG5 4DU
- 121) 23 Derwent Close, Wokingham RG41 3UD
- 122) 23 Summer Field Close, Wokingham RG41 1PH
- 123) 23 Telford Crescent, Woodley RG5 4QT
- 124) 24 Bellway, Woburn Sands MK17 8UG
- 125) 24 Chiltern Crescent, Earley RG6 1AN
- 126) 24 Wentworth Crescent, Maidenhead SL6 4RW
- 127) 25 Victoria Road, Wargrave RG10 8AD
- 128) 26 Pound Lane, Sonning RG4 6XE
- 129) 27 Edgcumbe Park Drive, Crowthorne RG45 6HU (x2)
- 130) 27 Patten Ash Drive, Wokingham RG40 1SH
- 131) 27 Pitts Lane, Earley RG6 1BX
- 132) 27 Telford Crescent, Woodley RG5 4QT
- 133) 27 Wessex Way, Maidenhead SL6 3BP
- 134) 28 Anthian Close, Woodley RG5 4XA
- 135) 28 Burnside Road, West Bridgford NG2 7HW
- 136) 28 Jarvis Drive, Twyford RG10 9EW
- 137) 28 New Road Twyford RG10 9PT
- 138) 282 Kidmore Road, Caversham RG4 7NF
- 139) 29 Oxford Avenue, Burnham SL1 8HR
- 140) 29 Paddock Heights, Twyford RG10 0AP (x2)
- 141) 29 Winchcombe Road, Twyford RG10 0AS (x3)
- 142) 3 Gables Close, Maidenhead SL6 8QD
- 143) 3 Humber Close, Fareham PO14 3RH
- 144) 3 Kingfisher Court, Twyford RG10 0BD (x2)
- 145) 3 Newfield Road, Sonning Common RG4 9TB
- 146) 3 Skilton Road, Tilehurst RG31 6SA (x3)
- 147) 3 Strand Way, Lower Earley RG6 4BU
- 148) 3/57-59, West End Road, Morecambe LA4 4DR
- 149) 30 All Saints Avenue, Maidenhead SL6 6NA
- 150) 30 Faygate Way, Lower Earley RG6 4DA
- 151) 30 Hurst Road, Twyford RG10 0AJ
- 152) 30 Manor Road, Wokingham RG41 4AH
- 153) 30 New Road, Twyford RG10 9PT
- 154) 30 Park View Drive South, Charvil RG10 9QX

- 155) 30 Rushington Avenue, Maidenhead SL6 1BZ (x3)
- 156) 31 Bideford Close, Woodley RG5 3SE
- 157) 31 Hurst Road, Twyford RG10 0AG
- 158) 31 New Road, Ruscombe RG10 9LN (x4)
- 159) 32 Furze Platt Road, Maidenhead SL6 7NN (x2)
- 160) 32 Northumberland Avenue, Reading RG2 7PW
- 161) 33 Finmere North Lake, Bracknell RG12 7WF
- 162) 33 Hermitage Drive, Twyford RG10 9HT
- 163) 33 Hermitage Drive, Twyford RG10 9HT
- 164) 33 Loxwood, Earley RG6 5QZ
- 165) 33 Telford Crescent, Woodley RG5 4QT
- 166) 34 Beechwood Avenue, Woodley RG5 3DG
- 167) 34 Huntingdon Close, Lower Earley RG6 3AB
- 168) 34 Hurst Park Road, Twyford RG10 0EY
- 169) 34 Norden Road, Maidenhead SL6 4AY (x2)
- 170) 35 Carrick Gardens, Woodley RG5 3JD
- 171) 35 Grange Road, Bracknell RG12 2HY
- 172) 35 Lawrence Road, Tilehurst RG306BH
- 173) 35 Loosen Drive, Maidenhead SL6 3UT (x2)
- 174) 36 Orchard Avenue, Sonning Common RG4 9LT
- 175) 36 Wessex Gardens, Twyford RG10 0AY (x5)
- 176) 37 Barn Drive, Maidenhead SL6 3PR (x3)
- 177) 37 Fountains Garth, Bracknell RG12 7RH
- 178) 37 Nightingale Road, Woodley RG5 3LS
- 179) 37 Ravensboure Drive, Woodley RG5 4LH
- 180) 38 Badminton Road, Maidenhead SL6 4QT
- 181) 38 Bodmin Road, Woodley RG5 3RZ
- 182) 38 Millbank Crescent, Woodley RG5 4ER
- 183) 38 The Drive, Earley RG6 1EG
- 184) 39 Kidmore End Road, Enmer Green, Reading RG4 8SN (x2)
- 185) 39 Lees Gardens, Maidenhead SL6 4NN (x2)
- 186) 39 Wroxham, Bracknell RG12 8QP
- 187) 4 Avery Close, Wokingham RG40 5QY
- 188) 4 Cherington Gate, Maidenhead SL66RU
- 189) 4 Corby Close, Woodley RG5 4TL
- 190) 4 Fallowfield Close, Enmer Green RG4 8NQ
- 191) 4 Hazel Close, Marlow SL7 3PW
- 192) 4 Lawrence Court, The Grove, Twyford RG10 9JQ
- 193) 4 Mulberry Close, Twyford RG10 0GJ
- 194) 4 Springfield Park, Twyford RG10 9JH
- 195) 4 Tallis Lane, Reading RG30 3EB
- 196) 40 Western Avenue, Woodley RG5 3BH
- 197) 42 Braybrooke Road, Wargrave RG10 8DT
- 198) 42 Orchard Aven Woodham KT15 3EA
- 199) 43 Academy House, Woolf Drive, Wokingham RG40 1EZ
- 200) 43 Ridgeway, Wargrave RG10 8AS
- 201) 43 Swanpool Street, Falmouth TR11 3HT (x2)
- 202) 43 Woodlands Avenue, Woodley RG5 3HN
- 203) 44 Palmers Close, Maidenhead SL6 3XF
- 204) 44 Purfield Drive, Wargrave RG10 8AR
- 205) 45 School Close, Downley HP13 5TR
- 206) 46 Ashton Road, Wokingham RG41 1HL

207) 47 Colleton Drive, Twyford RG10 0AU
 208) 47 Eastfield Road, Burnham SL1 7EL
 209) 47 Fidlers Walk, Wargrave RG10 8BA (x2)
 210) 479 Basingstoke Road, Reading RG2 0JG
 211) 48 Kibblewhite Crescent, Twyford RG10 9AX (x2)
 212) 49 Wessex Gardens, Twyford RG10 0BA
 213) 5 Chalcraft Close, Henley-On-Thames RG9 1QZ
 214) 5 Jack Court, 63 Rosemary Lane, Blackwater GU17 0LS
 215) 5 Kennedy Gardens, Earley RG6 5RN
 216) 5 Oak Way, Woodley RG5 3QS (x2)
 217) 5 Spencer Close, Goring RG8 0DW
 218) 5 Sycamore Drive, Twyford RG10 9HP
 219) 5 Wessex Gardens, Twyford RG100BA
 220) 50 Fidlers Walk, Wargrave RG10 8BA
 221) 51 Fidlers Walk, Wargrave RG10 8BA (x2)
 222) 51 New Road, Bourne End SL8 5BT
 223) 53 Lillibrooke Crescent, Maidenhead SL63XJ
 224) 53 Ridgeway, Wargrave RG10 8AS
 225) 56 Millbank Crescent, Woodley RG5 4ER
 226) 56 The Hawthorns, Charvil RG10 9TS (x2)
 227) 57 Antrim Road, Woodley RG5 3NY
 228) 57 Derwent Drive, Maidenhead SL6 6LE
 229) 57 Orchard Estate, Twyford RG10 9LA
 230) 574 Wokingham Road, Earley RG6 7JD
 231) 59 Colemansmoor Lane, Woodley RG5 4BT
 232) 59 Mays Close, Earley RG6 1JY
 233) 59 Mays Close, Earley RG6 1JY
 234) 6 Greenhow, Bracknell RG12 7RJ
 235) 6 Grenadier Close, Shinfield RG2 9EZ
 236) 6 Hurricane Way, Woodley RG5 4UX
 237) 6 Kennedy Close, Maidenhead SL6 4RL
 238) 6 Longcross Road, Longcross KT16 0DJ
 239) 6 Melody Close, Wokingham RG41 5LJ
 240) 6 Paddick Close, Sonning RG4 6XQ
 241) 6 Yewhurst Close, Twyford RG10 9PW (x2)
 242) 60 Pennfields, Ruscombe RG10 9BG (x2)
 243) 62 Oakley Road, Caversham RG4 7RN
 244) 62 The Hawthorns, Charvil RG10 9TS
 245) 63 Falstaff Avenue, Earley RG6 5TG
 246) 64 High Street, Wargrave RG10 8BY
 247) 64 Roslyn Road, Woodley RG5 3HS
 248) 64 Westleigh Drive, Sonning Common RG4 9LB
 249) 65 New Wokingham Road, Crowthorne RG45 6JG
 250) 66 Beechwood Avenue, Woodley RG5 3DG
 251) 66 Carisbrooke Avenue, Fareham PO14 3PR
 252) 68 Frensham Road, Crowthorne RG45 6QH (x2)
 253) 69 Ashbrook Road, Old Windsor SL4 2NE
 254) 69 Farm Road, Maidenhead SL6 5JB
 255) 7 Alben Road, Binfield RG42 4HU
 256) 7 Altwood Bailey, Maidenhead SL6 4PQ (x2)
 257) 7 Arnside Close, Twyford RG10 9BS
 258) 7 Pound Lane, Sonning RG4 6XD

- 259) 7 Reading Road, Woodley RG5 3DA (x2)
260) 7 Sussex Gardens, Woodley RG5 4JN
261) 7 The Copse, Wargave RG10 8AU
262) 7 Walnut Tree Close, Ruscombe RG10 9PF
263) 71 Pound Lane, Sonning RG4 6XD
264) 71 Antrim Road, Woodley RG5 3NY (x2)
265) 72 Arundel Road, Woodley RG5 4JT
266) 75B Wargrave Road, Twyford RG10 9PD
267) 77 Springfield Park, Twyford RG10 9JG
268) 77 Vale Farm Road, Woking GU21 6DN (x2)
269) 78 Broadwater Road, Twyford RG10 0EU
270) 78 Reading Road, Woodley RG5 3AD (x2)
271) 8 Bathurst Road, Winnersh RG41 5JB
272) 8 Bayliss Road, Wargrave RG10 8DR (x2)
273) 8 Link Way, Arborfield Cross RG2 9PD
274) 8 Mallard Close, Twyford RG10 0BE (x2)
275) 8 Morton Place, Theale RG7 5QW
276) 8 Munro Avenue, Woodley RG5 3QY
277) 8 Northbrook Road, Caversham Park RG4 6PW
278) 8 Ribstone Road, Maidenhead SL6 3HJ
279) 8 Tiggall Close, Earley RG6 7ES
280) 80 Elm Road, Earley RG6 5TR
281) 83 Windrush Way, Maidenhead SL6 8AP
282) 84 Loddon Bridge Road, Woodley RG5 4AN
283) 86 Acorn Drive, Wokingham RG40 1EQ
284) 88 Woodward Close, Winnersh RG41 5UU
285) 9 Delmeade Road, Chesham HP5 1JS
286) 9 Dunnock Way, Wargrave RG10 8LR
287) 9 Fitzroy Crescent, Woodley RG5 4EU (x3)
288) 9 Mansfield Road, Wokingham RG41 2TP
289) 9 Orchard Estate, Twyford RG10 9JY
290) 9 Paddock Heights, Twyford RG10 0AP
291) 9 Tiverton Close, Woodley RG5 3BE (x3)
292) 9 Victoria Way, Reading RG1 3HD
293) 94 Eastern Avenue, Reading RG1 5SF
294) 94 Linden Road, Reading RG2 7EH
295) 96A London Road, Ruscombe RG10 9HH
296) 98a The Brow, Waterlooville PO7 5DA
297) 99 Beechmount Avenue, Hanwell W7 3AF
298) Anonymous
299) Antibes, Ridge Close, Woking GU22 0PU
300) April Cottage, 13 Butts Hill Road, Woodley RG5 4NJ
301) Ashingdon, School Road, Hurst RG10 0DR
302) Ashley View, Star Lane, Knowl Hill RG10 9XY
303) Badgers Rise, Woodley RG5 3AJ (no number supplied)
304) Bartholomew House, 38 London Road, Newbury RG14 1JX
305) Bear Place Farm, Blakes Lane, Hare Hatch RG10 9TA
306) Beverley Gardens, Wargrave RG10 8ED (no number supplied)
307) Bints Farm, Dunsden Green RG4 9QG
308) Black Gables, 22 East View Road, Wargrave RG10 8BH
309) Bowsey Hill House, Bowsey Hill, Wargrave RG10 8QJ
310) Bramble Cottage, Star Lane, Knowl Hill RG10 9XY

- 311) Bridleways, Munday Dean Lane, Marlow SL7 3BU (x2)
312) Broad Oak, Old Honey Lane, Hurley SL6 5LW
313) Carisbrooke Avenue, Fareham PO14 3PR (no number supplied)
314) Cheriton Avenue, Twyford RG10 9DB (no number supplied)
315) Choke Lane, Cookham SL6 6PL (no number supplied)
316) Church Farm, Milley Road, Waltham St. Lawrence RG10 0JD
317) Clear View Cottage, Whistley Green RG10 0DU
318) Colleton Drive, Twyford RG10 0AX (no number supplied)
319) Courtlands Lodge, 35a Saint Peters Avenue, Caversham RG4 7DH
320) Crantock, 102 London Road, Ruscombe RG10 9HH
321) Cutbush Commercial, Cutbush Lane East, Shnfield RG2 9AF
322) Dunt Lane, Hurst RG10 0TA (no number supplied)
323) Farmend, Halls Lane, Waltham St. Lawrence RG10 0JB
324) Ferncroft, Burchett's Green, Maidenhead SL6 3QW
325) Glebe House, Blakes Lane, Hare Hatch RG10 9TD
326) Greyhome, Farriers Close, Woodley RG5 3DD (x2)
327) Haddon Drive, Woodley RG5 4LU (no number supplied)
328) Hill Farm Lodge, Shurlock Row, Twyford RG10 0PL
329) Holleys Fine Foods Ltd, Unit C, Poplar Park, Poplar Way East, Bristol BS11 0DD
330) Hurst Farms, Hurst House, Church Hill, Hurst RG10 0SH
331) Laurel House, Millers Lane, Littlewick Green SL6 3GQ
332) Linden Place, Bath Road, Knowl Hill RG10 9UT
333) Local Area Ltd, 20 Broad St, Wokingham RG40 1AH
334) Loddon, Loddon Drive, Wargrave RG10 8HL
335) Loggers Leap, Hare Hatch RG10 9HW (x2)
336) Longfield Road, Twyford RG10 9AT (no house number)
337) Lulworth, Longfield Road, Twyford RG10 9AT
338) Magnolia, 9 Springfield Place, Twyford RG10 9JG
339) Malvern, Ruscombe Lane, Ruscombe RG10 9JN
340) Manderhill, Andrew Hill Lane, Hedgerley SL2 3UL
341) McMinn, Weir House, Latimer Road, Chesham HP5 1QJ (x2)
342) Meadow Cottage, Davis Street, Hurst RG10 0TJ
343) Middle House, School Hill, Wargrave RG10 8DY
344) Mile Away, Pump Lane North, Marlow SL7 3RD
345) Millwards Cottage, 117 High Street, Wargrave RG10 8DG (x2)
346) Minster Grove, Wokingham RG41 2AP (no number supplied)
347) Mumbery Field House, School Hill, Wargrave RG10 8DY
348) Old Blades, 103 Remenham Lane, Henley On Thames RG9 3DB
349) Park View, 135 Honey End Lane, Reading RG30 4EG (x2)
350) Pax Cottage, Byron Road, Twyford RG10 0AE
351) Poppy Corner, 25 Concorde Way, Woodley RG5 4NF
352) Rambler Cottage, Bracken Road, Maidenhead SL6 3EF
353) Riverways Farm, New Bath Road, Twyford RG10 9RY
354) Rose Cottage, Stud Green, Holyport SL6 2JH
355) Scarletts Close, Scarletts Lane, Kiln Green RG10 9XD
356) Sobraon, 81 Wargrave Road, Twyford RG10 9PE
357) Spinney Cottage, Rosehill, Henley-On-Thames RG9 3ED
358) Station Road, Wargrave RG10 8EU (no number supplied)

- 359) Stoneycroft, Forest Dale Road, Marlborough SN8 2AS
- 360) Tape Lane, Hurst RG10 0DP (no number supplied)
- 361) The Bungalow, Shurlock Row RG10 0PL
- 362) The Dene, Milley Road, Waltham St. Lawrence RG10 0JT
- 363) The Dutch Barn, Highfield Lane, Crazies Hill RG10 8PU
- 364) The Hollies, Ruscombe Lane, Ruscombe RG10 9JN
- 365) The Lime Tree, 54a Hurst Road, Twyford RG10 0AN
- 366) The Mount, Lines Road, Hurst RG10 0RT
- 367) The Old Police House, Station Road, Twyford RG10 9NG
- 368) The Shrubbery, Milestone Avenue, Charvil RG10 9TN
- 369) The Willows, Knowl Hill Common RG10 9YE (x2)
- 370) The Wishing Well, Bracken Road, Maidenhead SL6 3EF
- 371) Tudor Close, Wokingham RG40 2LU (no number supplied)
- 372) Valentine Crescent, Caversham RG4 5JL (no number supplied)
- 373) Vine House, Coronation Road, Littlewick Green SL6 3RA (x2)
- 374) Wargrave House, School Lane, Wargrave RG10 8AA
- 375) West Hope, Wards Cross, Hurst RG10 0DS
- 376) Windlestone, Bath Road, Littlewick Green SL6 3RQ
- 377) Woodside, 48 Reading Road, Woodley RG5 3DB
- 378) Yewgate Cottage, Remenham Hill RG9 3ES
- 379) The Rt Hon Theresa May MP, Member for Maidenhead

The submissions raised the following comments:

- Development is within the scope of Green Belt legislation
- Will protect the openness of the countryside
- There is already a precedent to allow garden centres in the Green Belt

Officer comment: The proposal does not accord with Green Belt policies in the NPPF as advanced in 'Principle of Development' (paragraphs 29-51). The other garden centres in the area are not viewed as precedents that would justify approval of this application.

- Business adaptation should be encouraged
- Will stimulate local trade and employment
- Need to be on a level playing field with other businesses
- Will allow for a financially sustainable business
- Support for local business
- Business will support the required new homes in the area
- Would complement other businesses by drawing in business

Officer comment: Paragraph 84 of the NPPF supports the growth of rural based businesses but this does not sufficiently override Green Belt policy. A balanced consideration is required and this forms part of the wider planning balance consideration of the application at paragraphs 133-139.

- Other nurseries have changed into chain stores
- Dobbies is an example of big businesses getting rid of small businesses

Officer comment: Whether the operation is run as a chain or a local owner does not form a planning consideration.

- If refused, the site will become derelict

Officer comment: Whilst this scenario is possible, it does not weigh in favour of the proposal.

- Is not an over development of the site
- Plans represent an improvement to a wasted and poorly maintained space
- Café needs an upgrade
- Buildings need to be upgraded
- Plants can be housed in better conditions

Officer comment: There are visual benefits associated with the redevelopment of the site, as noted in comments in 'Principle of Development' (paragraphs 29-51) and 'Character of the Area' (paragraphs 71-84).

- Excellent business
- Well liked business
- Well-loved in the community
- Good range of products
- Provided a good service during lockdown
- They provide a positive community and charitable service
- They provide other extra uses (wildlife, charities, children activities)
- Staff are friendly
- Business is innovative
- Business has valuable expertise
- Café is well attended
- Acts as a community hub
- Business has been helpful during the pandemic
- Independent, family run business and should be supported
- Will re-establish the reputation
- Plants sold here aren't always available elsewhere

Officer comment: It is recognised that Hare Hatch Sheeplands has a positive reputation in the local community and this has been noted in the 2019 appeal decision whereby the community support for the scheme was viewed as one of the very special circumstances. The same approach is applied in assessing this application, as noted in 'Principle of Development' (paragraphs 46-51).

- Playground is positive for children
- Positive for children
- Will encourage growth in gardening
- Facilities are needed in the area
- Will increase diversity and choice
- Outdoor space will benefit mental health

- Valuable asset for the local community
- Allotments will be available for the community

Officer comment: It is noted that the proposal includes facilities that could be utilised by the local community, including fields and children's playground. These benefits weigh in favour of the scheme, as discussed at paragraphs 133-139.

- Hedgerow will provide an ecological corridor
- Woodland barrier is positive element alongside the A4
- Good green credentials with positive biodiversity measures

Officer comment: There are positive biodiversity outcomes associated with the proposal (see paragraphs 116-118) and this weighs in favour of the application in the consideration of the planning balance at paragraphs 133-139.

- Will help the Council meet the climate emergency targets
- Proposes unique ideas, including carbon capture
- Sustainable business with low food miles
- Supports local business and philosophy of being local
- Will support local food production

Officer comment: The sustainability benefits are noted in 'Sustainability' (paragraphs 52-55) and weigh in favour of the scheme, as noted from paragraphs 133-139.

- There is ample parking

Officer comment: The proposal provides sufficient car parking as noted in comments from the Highways Officer at paragraph 90.

- Would assist infrastructure in the area

Officer comment: Any associated improvement to the infrastructure of the surrounding area must account for the principle of such development in the Green Belt. In this case, it is viewed as limited.

- Fair should continue

Officer comment: The existing events fair is subject to a temporary permission and it is the choice of the owner whether any use would form part of any future approval of this application.

- Minimal effect on the borough

Officer comment: This observation has limited relevance to the subject application.

- WBC needs to respect the Crown Court judgement

Officer comment: The judgement is an unrelated consideration for the subject application.

Objection

1) 17 Bayliss Road, Wargrave RG10 8DR.

The submission raised the following comments:

- Development has already been refused in 2011 because of car parking and access issues
- Inadequate car parking

Officer comment: Matters of parking and access are to the satisfaction of the Council's Highways Officer, as noted in paragraphs 89-100.

- Over development of a green belt site for a nursery
- Inappropriate development in the Green Belt

Officer comment: The proposal represents inappropriate development in the Green Belt and poses a degree of harm to the openness, as outlined in paragraphs 29-51 and Reason for Refusal 1 and 2.

- Carbon capture is unjustified, including how much

Officer comment: This aspect of the development weighs partly in favour of the application, as discussed in the planning balance at paragraphs 133-139. However, Tree Officer comments at paragraph 106 are of relevance.

APPLICANTS POINTS (obtained from Conclusion of Planning Statement)

The proposed development is a carefully considered, high quality rationalisation and enhancement of the existing sites use. It is the culmination of many years of HHS trying find a sustainable and long-term future for the business and site. It is very clear, based on the unique planning history and set of circumstances that have led to this point, that the proposed development is absolutely necessary to ensure this valued community focused business is viable in the long term. The Agricultural Viability Assessment is clear on this point. The site cannot operate successfully on a production basis with only limited retail opportunities. If the uses are limited to those uses the site will fall into dereliction.

In this context, what is being proposed is only the minimum required to ensure the business can operate viably, respecting the Green Belt designation of the site. In this regard, it is demonstrated that the site is in mixed use, thus is considered to be Previously Developed Land (PDL) meaning, new buildings can be built on the site as part of a redevelopment scheme, on the proviso that there is no greater impact on the openness of the Green Belt than the existing development.

Assessing the impact on the openness of the Green Belt, the site is not considered to be currently contributing to openness of the Green Belt in a meaningful way. Whilst spatial openness will change within the site as will the composition of visual openness,

the proposed developments-built nature, size and scale will be reduced substantially compared to the existing development.

There will be a 58% decrease in building floorspace and 35.61% decrease in building volume. These are significant reductions in both footprint and volume which will vastly improve the sense of openness across the site, by increasing the proportion of the site that is free from development. Where these structures are removed, this will visually and spatially open up the entire southern and western areas of the site in areas that are currently enclosed upon entering the site. An essential characteristic of openness is the 'absence of buildings' and the proposals will therefore clearly enhance the visual openness of the site.

Therefore, there will be a beneficial change in spatial and visual openness and certainly no greater impact on the openness than the existing development, meeting the proviso set out in Green Belt policy.

In addition, the proposed development accords with the Council's policies that control development in countryside; given the established existing mixed land use and fact Garden Centres cannot, by their very nature, be located on land in Towns or Local Centres. They are rural businesses, operating elements of horticulture and related products. The accompanying Retail Impact Assessment demonstrates the development will not have an adverse impact on the vitality or viability of local centres.

It is also demonstrated there will be no adverse landscape, ecological, arboricultural, amenity or transport impacts resultant of the development. The development accords with the Council's relevant development plan policies' in this regard.

What is evident, is that there will be a significant number of social, economic and environmental planning benefits that will flow from the development. These include:

- Removal of poor-quality glasshouse structures and improvement of the built form through a new, high quality and sympathetic sustainable garden centre structure
- Significant landscape and Green Belt improvements
- Significant improvements to environment and a substantial net gain in biodiversity across the site. Through enhanced planting and removal of the glasshouses there will be a 26.44% in habitat biodiversity and monumental 564.22% for hedgerow biodiversity
- Economic benefits to the rural economy through continued local spending;
- Community benefits promoting social cohesion (mix of facilities, retention of café facilities, children's play area, garden to shop allotments for charitable use such as Thrive or other local organisations)
- Employment and training (particularly for young people) in both construction and future operation. There will be an increase in full time equivalent positions from 32 to 69
- Carbon storage woodland adjacent to the A4 to assist the Council with their Climate
- Emergency
- Improved and rationalised parking provision, including EV charge points and accessible spaces; and
- Improved pedestrian connections to the site, via the proposed footpath to the north

Therefore, overall, planning permission should be granted for the development. In this case, as demonstrated in this report, the proposal is considered to accord with the

Councils Development Plan when taken as a whole. It is a sustainable development, with a multitude of planning benefits which weigh heavily in favour of the scheme in the planning balance.

Notwithstanding, should the Council consider that in Green Belt terms the development is 'inappropriate', it is demonstrated in this report that there are indeed 'very special circumstances' (VSC) which outweigh the potential harm to the Green Belt by reason of inappropriateness, and any other harm resulting from the proposal.

These include:

- Significant removal and reduction of poor-quality glasshouse structures and improvement of the built form;
- Significant landscape and Green Belt spatial and visual quality improvements;
- Significant improvements and a substantial net gain in biodiversity across the site;
- Economic benefits to the rural economy and supporting the diversification of a long standing rural business;
- Meeting customer expectations;
- Community Benefits (including a mix of facilities, retention of facilities, children's play area, garden to shop allotments for charitable use such as Thrive or other local organisations);
- Employment and training (particularly for young people);
- Carbon storage woodland adjacent to the A4 to assist the Council with their Climate Emergency;
- Viability and sustainability of the business and land use. The site is not viable for horticultural uses. It cannot operate successfully on a production basis with only limited retail opportunities.
- The ability to compete with other local businesses as was referenced by the 2008 Inspector for the farm shop and café. This is particularly relevant given the competition is being given the competitive edge by the planning system and issuing of CLEUDs elsewhere;
- Enhancing a valued community facility;
- Overwhelming community support for this local business;
- The impact of the Abuse of Process and financial viability on the business; and
- The proposal being the best and most appropriate use for the site in Green Belt terms

Therefore, it is clear if the Council do not agree with the Green Belt assessment, there are VSC that would outweigh any potential harm to the Green Belt by reason of inappropriateness, and any other harm the Council considers results from the proposal. These circumstances, along with the significant social, economic and environmental planning benefits that would arise from the scheme as outlined above, all indicate that there are tangible and demonstrably other material considerations that would outweigh any conflict

PLANNING POLICY		
National Planning Policy Framework 2021	Section 6	Building a Strong, Competitive Economy
	Section 14	Protecting Green Belt Land
	Section 15	Conserving and Enhancing the Natural Environment
Core Strategy 2010	CP1	Sustainable Development
	CP2	Inclusive Communities

	CP3	General Principles for Development
	CP6	Managing Travel Demand
	CP9	Scale and Location of Development Proposals
	CP11	Proposals outside Development Limits
	CP12	Green Belt
Managing Development Delivery Local Plan 2014	CC01	Presumption in Favour of Sustainable Development
	CC02	Development Limits
	CC06	Noise
	CC07	Parking
	CC09	Development and Flood Risk
	TB01	Development within the Green Belt
	TB18	Garden Centres and Other Small Rural Units outside Development Limits
	TB20	Service Arrangements and Deliveries for Employment and Retail Use
Borough Design Guide SPD	Section 6	Parking
	Section 7	Non-Residential
	Section 8	Rural and Settlement Edge

PLANNING ISSUES

Description of Development

1. The proposal involves the following works:
 - a. Demolition of the existing greenhouses, covered canopies and delivery store and retention of an existing office building, farm shop building and water tank
 - b. Redevelopment of the site to accommodate a change in use from mixed use (including horticultural, café, farm shop and temporary retail) to garden centre
 - c. New main garden centre shop building to the east of the existing farm shop with metal and wood finish, rooflights and solar panels and housing garden items
 - d. Relocated café building to its south, with seating for 250 persons and customer toilets
 - e. Outdoor plant area and covered canopy area to the north of the garden centre shop
 - f. Allotment gardens for charitable purposes and serving the farm shop
 - g. Children's play area and informal recreational field to the southern end of the site comprising play equipment open to general public
 - h. Redeveloped, rearranged and resurfaced car park for 192 vehicles, with an overflow car park to the north for 91 vehicles amongst a proposed managed woodland and a main service yard
 - i. Reconfigured vehicular access to London Road and introduction of pedestrian access to Bath Road
 - j. Associated landscaping and site works, including hedgerow to the boundary

Description of Site

2. The wider 2.4-hectare site occupies a prominent corner location at the intersection of Bath and London Roads. It comprises a large collection of greenhouses and buildings that comprise a horticultural nursery, farm shop, café (with outdoor seating), retail floorspace, offices and a periodic event space but otherwise appears open and rural, with the buildings having a light and low key appearance. There is also a residential dwelling in the south western corner which has an agricultural/horticultural

occupancy condition, and a car park and service yard within the centre of the site. The surrounding area includes a collection of nurseries and garden centres, with some dwellings and commercial buildings interspersed throughout the surrounding area.

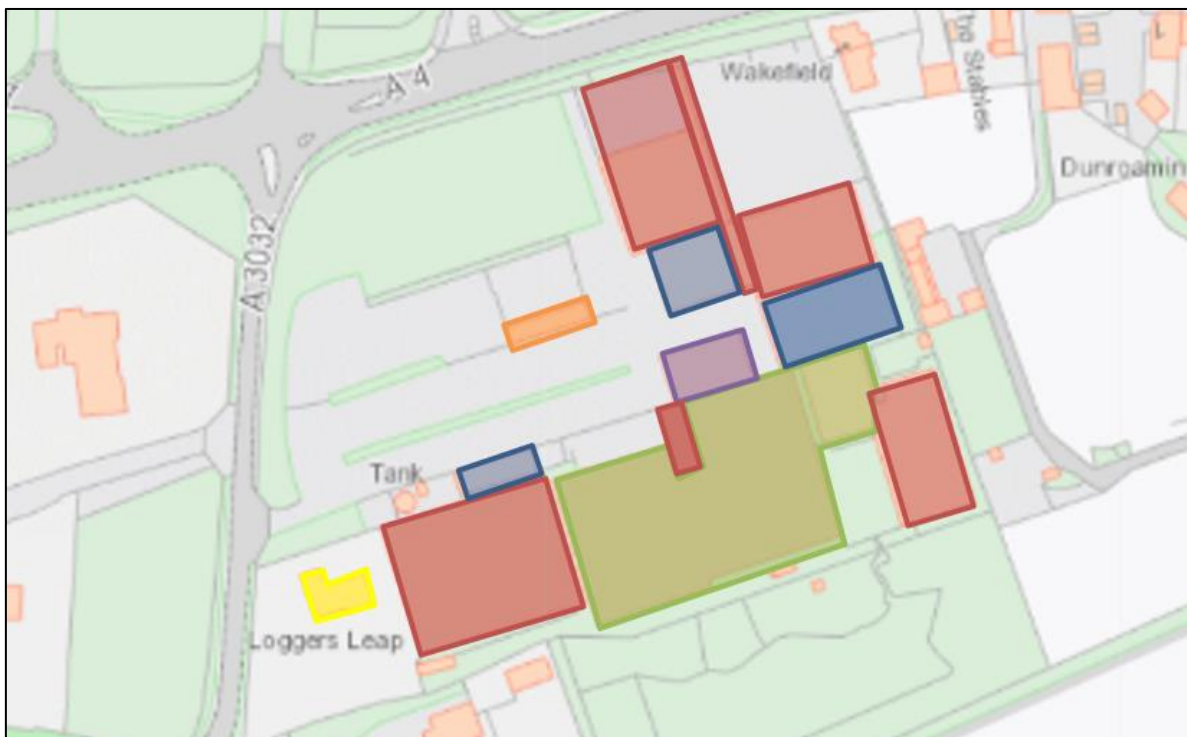
Site History

Initial development

3. The Hare Hatch Sheplands site was originally part of a much larger site on both sides of the Bath Road used for flower growing and known as 'Waterer's Floral Mile'. The original greenhouses were built in 1976 (application 03604). A salad nursery operated until 1987 and a legal agreement restricted sales to natural produce picked from the land with sales from a mobile kiosk as part of pick your own operations run by Twyford Fruit Farms holding. In 1989, a Section 64 determination was issued for use of the site as a containerised tree nursery for sale by wholesale and retail. The site was then disused through to 1992 when purchased by the current owners before reopening to the public in May 1993.

Expansion of development

4. An agricultural barn and polytunnels were approved in 1993 (application 39618) but not implemented. Condition 7 limited retail sales to trees, shrubs and plants. A new agricultural barn (now the farm shop) was approved in 1996 (F/1995/63187). Condition 6 prohibited retail sales from the barn and Condition 7 limited storage to horticultural goods. Four greenhouses and three covered walkways totalling 5,128m² were approved in 1997 (F/1997/66297), with a condition ensuring that the only trees and shrubs to be sold were those grown on site. Two additional glasshouses and an office/toilet block were approved in in 2002 (F/2001/5225). Condition 9 limited the buildings to ancillary to horticultural/agricultural use. At this point, the site remains wholly within horticultural use with restricted retail sales.



Permissions across the site (see table below)

	Permission	Approved	Description
	03604	15/12/1976	Greenhouses for horticultural use
	F/1995/63187	27/09/1996	Barn for horticultural use
	F/1997/66297	3/11/1997	Greenhouses for trees grown on site
	F/2001/5225	9/01/2002	Glasshouses, offices and toilets
	F/2002/7504	22/01/2003	Dwelling house with horticultural tie
	No permission identified		Service yard

5. Of the above permissions, F/2001/5225 requires removal of the building within three years if the horticultural use were to cease within ten years of its first use.

Retail uses

6. Since 2002 there has been an incremental change from a wholesale tree nursery to a site that comprises a mixed use far removed from the original tree nursery.
7. **Pet store:** Application F/2003/8706 sought approval for a 195m² retail building in the car park operating as a pet shop. It followed the unlawful operation of the pet store from a shipping container on the site for approximately two years. The application was refused on 28 April 2003 because (a) it was inappropriate development and represented loss of openness in the Green Belt and (b) a lack of supporting information relating to traffic and highway safety. Application F/2004/2238 involved a largely identical scheme in the same location. It was refused on 26 August 2004, again because it was inappropriate development and represented loss of openness in the Green Belt. Application 152748 sought approval for 250m² of floorspace within the existing greenhouses for use as a pet store. The Council declined to determine the application on 16 January 2016 because there was a current enforcement notice.
8. **Farm shop:** Application F/2007/0225 granted approval for a retail farm shop (with cold store) with a floor area of 445m² on 25 May 2007. Condition 5 limited sales to fresh farm produce from farms in a 10-mile radius of the site. Condition 5 was then deleted at appeal on 23 April 2008. Application F/2008/2295, approved 2 February 2009, involved the addition of a butcher with retail floorspace of 94m². A fishmonger was approved on 4 September 2017 as part of variation application 172161. Application 201693 granted approval for an additional 18m² of back-of-house space for the butcher within a replacement greenhouse on 10 September 2020 (not yet implemented). Application 201777 granted approval for 7m² of outdoor retail space in front of the farm shop on 16 September 2020.
9. **Café:** Application F/2007/0226 sought approval for a 156m² café/restaurant. It was refused on 23 April 2007 as (a) it was inappropriate development in the Green Belt in an unsustainable location. Application F/2007/1428 again sought approval for a 280m² café. It was refused on 27 July 2007 for the same reason but upheld at appeal on 23 April 2008. Application 201693 granted approval for an additional 33m² of kitchen space and toilets for the cafe within a replacement greenhouse on 10 September 2020 (not yet implemented). Application 201777 granted approval for 120m² of outdoor seating on 16 September 2020.
10. **Redevelopment as a garden centre:** Application F/2011/0648 involved a change of use of the site to a garden centre similar to what is proposed in the subject application. It involved replacement buildings, a total of 8800m² of retail floorspace,

revised parking and additional landscaping. It was refused on 19 December 2011 for the following reasons:

- a. Poor site sustainability
- b. Increase in retail floorspace was inappropriate development in the Green Belt
- c. Loss of openness to the rural setting by virtue of bulk and nature of buildings and hard paving
- d. Retail impact upon Twyford Town Centre
- e. Lack of Flood Risk Assessment
- f. Increased use of substandard access onto London Road

An appeal was withdrawn.



Masterplan for F/2011/0648

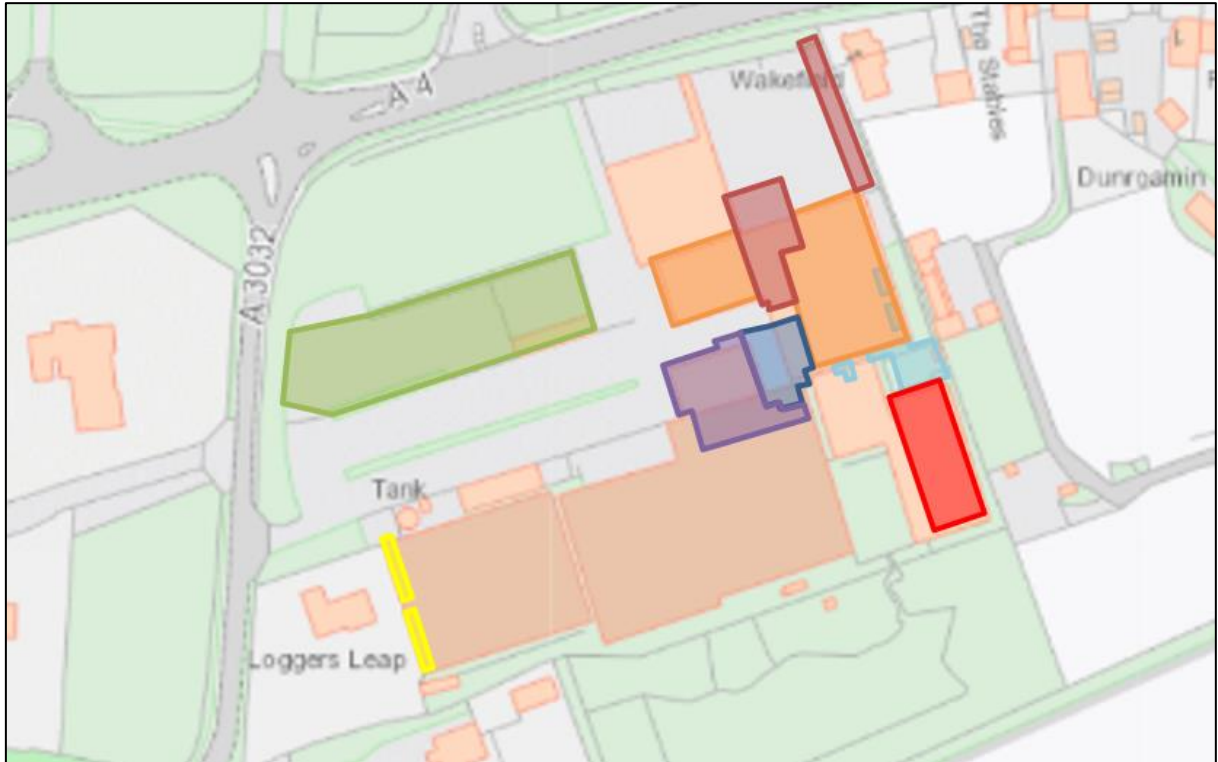
11. **Retail shop:** Application F/2008/0038 related to the change of use of 2200m² of the existing glasshouses to retail floorspace. It was refused on 2 February 2009 because it was inappropriate development in the Green Belt in an unsustainable location. Certificate application CLE/2014/1462 then sought permission for retail sales across 6225m² of the site (3,870m² internal, 2,355m² external) but it was refused on 31 March 2015 because there was a current enforcement notice. An appeal was dismissed for the same reason on 5 October 2016. Certificate application 160677 sought permission for retail sales across 7,075m² of the site (4,155m² internal, 2,920m² external) but this was refused on 13 May 2016, also because there was an existing enforcement notice.
12. Application 173316 involved a proposal for 500m² of retail floorspace (355m² internal, 145m² external) within the existing greenhouses. It was approved at appeal on 14 March 2019, temporary for three years. A concurrent application 172850 for the same proposal was not determined. Application 211086 approved an additional

180m² of external retail floorspace and an extension of the temporary permission for a further two years granted on 11 August 2021 for one year. A condition within that permission is currently subject to appeal and it is yet to be implemented.

13. **Children's play area:** Application 152747 sought permission for a children's play area to 1415m² of indoor greenhouses and a recreational farm across 7925m² of land to the south of the site. The Council declined to determine the application on 13 January 2016 because there was a current enforcement notice.
14. **Timber store:** Application 191519 granted approval for a 165m² of the greenhouse floorspace at the very western end to be used for the residential storage of logs associated with the adjacent dwelling house. It was approved on 17 September 2019.
15. **Service yard:** Application 191520 granted retrospective approval for an additional 40 parking spaces to the north of the existing car park and use of the service bay. It covered a combined external area of 2,780m² but with the service area comprising 485m² (155m² internal, 330m² external). It was approved on 10 September 2019.
16. **Events:** Application 192018 granted approval for the dual use of 1490m² of the existing greenhouses for events use, temporary until 14 March 2022. Condition 3 limited events to 24 days per year and retail sales were not permitted (though there is dispute on this aspect). An extension of the time period for two years was sought in application 211085 but approved on 11 August 2021 for one year. A condition within that permission is currently subject to appeal and it is yet to be implemented.
17. **Exotic animals:** Application 201047 granted approval for the housing of exotic animals within display cabinets in an area of 110m² of existing greenhouses with a separate outdoor exercise area measuring 120m². It was approved on 23 June 2020, temporary for three years.
18. **Exhibition space:** Application 211084 granted approval for 670m² of exhibition space for the public display of Hawthornia and other plants. Temporary permission for 3 years was sought but it was approved on 11 August 2021 until 14 March 2023.
19. The following table summarises the various existing uses across the site, temporary or permanent. There is a margin of error between the Council's and applicant's calculations

	Use	First Approved	Floorspace			Notes
			Internal	External	Total	
	Total floorspace		11,190m ²	4,260m ²	15,450m ²	
	Farm shop	2007	557m ² (5%)	7m ² (<1%)	564m ² (4%)	Includes butcher and fishmonger
	Cafe	2008	313m ² (3%)	120m ² (3%)	433m ² (3%)	Includes outdoor seating
	Retail shop	2019	355m ² (3%)	325m ² (7%)	680m ² (4%)	Temporary to March 2023
	Timber store	2019	165m ² (1%)	0m ² (0%)	165m ² (1%)	Dual residential and horticultural use
	Service yard	2019	155m ² (1%)	330m ² (7%)	485m ² (3%)	Shared amongst all uses
	Events	2019	1,490m ² (13%)	0m ² (0%)	1490m ² (10%)	Temporary to March 2023 and 24 days per year

Animals	2020	110m2 (1%)	120m2 (3%)	230m2 (1%)	Temporary to June 2023 and shared with events use
Exhibition	2021	670m2 (6%)	0m2 (0%)	670m2 (4%)	Temporary to March 2023
Non horticultural		3,815m2 (34%)	902m2 (21%)	4,717m2 (31%)	Total of all non-horticultural uses across the site
Horticultural	1976	7,375m2 (66%)	3,358m2 (79%)	10,733m2 (69%)	Includes areas not currently being used for horticulture

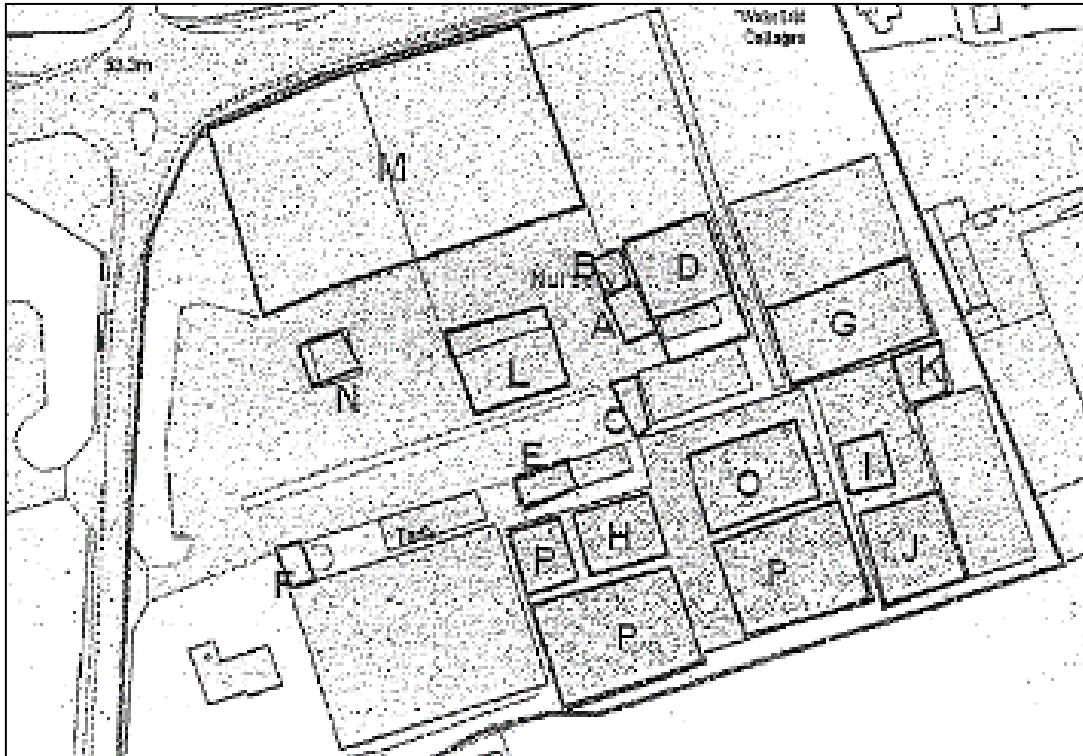


Plan showing non horticultural uses, inclusive of temporary and permanent uses (see table above)

Enforcement

20. Numerous breaches were identified in 2011 and 2012 including an unauthorised conservatory display building (RFS/2011/00614), laying of hardstanding to provide car parking (RFS/2012/00041), carpet shop (RFS/2012/00127) and unauthorised sale of pet food (RFS/2010/00417). A consolidated enforcement notice addressing these and other matters was served on 25 October 2012 in relation to the following:
- a. Two display conservatories
 - b. Container used for solar energy business
 - c. Retail sale of fish and chips and ice cream
 - d. Retail sale of items including floor coverings, sweets, giftware in a new building
 - e. Siting of a container
 - f. Residential mobile home
 - g. Retail sale of children's toys, cards, ornaments, luggage and giftware, baguette bar and children's play area within existing glasshouses
 - h. Retail sale of camping equipment and garden furniture in an existing glasshouse
 - i. Extension to approved café seating
 - j. Garden area associated with the coffee shop
 - k. Children's play area

- l. Storage of vehicles, plant, waste, storage and display or items and depolluting area
- m. Additional hardstanding for overflow car parking
- n. Building used for a pet store
- o. Events arena
- p. Storage associated with retail use



Site plan for 2012 enforcement notice

21. An appeal against the October 2012 notice was withdrawn and following further unauthorised works, the notice was withdrawn and a new enforcement notice was issued, outlining the following breach:

Without planning permission,

- 1) *The material change of use of the land from horticulture to a mixed-use comprising horticulture, A1 retail, A3 restaurant/café, D2 children's play facilities and the stationing of a residential mobile home*
- 2) *The construction of hard surfaced areas for car parking and outdoor retail use*
- 3) *The erection of structures for purposes ancillary to the mixed use*

22. An appeal against the 2013 Notice was also withdrawn. Following a failure to comply with the Order, a High Court Injunction was issued on 20 February 2017 requiring compliance with outstanding matters on the enforcement notice and forbidding:

- a. Use of the land for retail, café and/or play area, other than that already approved
- b. Erection of a marquee
- c. Use of the land as a fishmonger or sale of swimming pools, sheds or summer houses
- d. Use for advertising
- e. Stationing for mobile homes
- f. Any other development under Section 55 of the Town and Country Planning Act

23. A two-year suspended sentence was also issued and compliance with the Notice and Order was finally achieved in June 2017. An Abuse of Process finding was also found against the Council as part of the wider proceedings. The Council has sought legal advice and considers that both the injunction and notice remain extant and in force.
24. Since then, unauthorised activities have continued, for example, there were unauthorised car storage in the car park in 2018 (RFS/2018/084428). This has now ceased. More recently, (RFS/2021/086434) alleges that retail floorspace has extended beyond that permitted under approvals 173316 and that events are occurring outside the scope of the planning permission granted in 192018. The investigation is held in abeyance pending determination of this application. Any expansion of the retail floorspace has not influenced the overall consideration of the application.

Principle of Development

Location within the Green Belt and countryside

25. The site is located outside settlement limits within the Green Belt and the Countryside. Development is ordinarily resisted or restricted by Paragraphs 147-150 of the NPPF, which states that new development in the Green Belt is inappropriate, unless it meets specific exceptions. Inappropriate development is, by definition, harmful to the Green Belt and should not be approved except in very special circumstances.
26. Policy CP12 of the Core Strategy and Policy TB01 of the MDD Local Plan state that planning permission will not be granted for inappropriate development and where it is not inappropriate, it must maintain the openness of and not conflict with or harm the purposes of the Green Belt. There is consistency in these policies with the NPPF and so these policies have considerable weight.
27. Policy CP11 of the Core Strategy also does not permit development outside of development limits except if in the case of diverse and sustainable rural enterprises or in the case of other countryside based enterprises and activities, it contributes and/or promotes recreation in, and enjoyment of, the countryside and does not lead to excessive encroachment or expansion of development away from the original buildings; and is contained within suitably located buildings which are appropriate for conversion.
28. The relevant considerations for development in the Green Belt are outlined in Paragraphs 147-150 of the NPPF:

Whether the proposal would be inappropriate development for the purposes of Section 13 of the NPPF and Development Plan policy

29. The NPPF states that new buildings/development in the Green Belt are inappropriate by their nature unless they fit into certain exception criteria. Part (g) of paragraph 149 permits "*limited infilling or the partial or complete redevelopment of previously developed land, whether redundant or in continuing use (excluding temporary*

buildings), which would not have a greater impact on the openness of the Green Belt than the existing development...

30. For the purposes of this assessment, the proposal involves partial redevelopment as the office building and farm shop building are being retained. The definition of previously developed land is defined in Annex 2 of the NPPF:

“Land which is or was occupied by a permanent structure, including the curtilage of the developed land (although it should not be assumed that the whole of the curtilage should be developed) and any associated fixed surface infrastructure. This excludes: land that is or was last occupied by agricultural or forestry buildings; land that has been developed for minerals extraction or waste disposal by landfill, where provision for restoration has been made through development management procedures; land in built-up areas such as residential gardens, parks, recreation grounds and allotments; and land that was previously developed but where the remains of the permanent structure or fixed surface structure have blended into the landscape.”

31. The crux of the applicant’s argument is as follows:

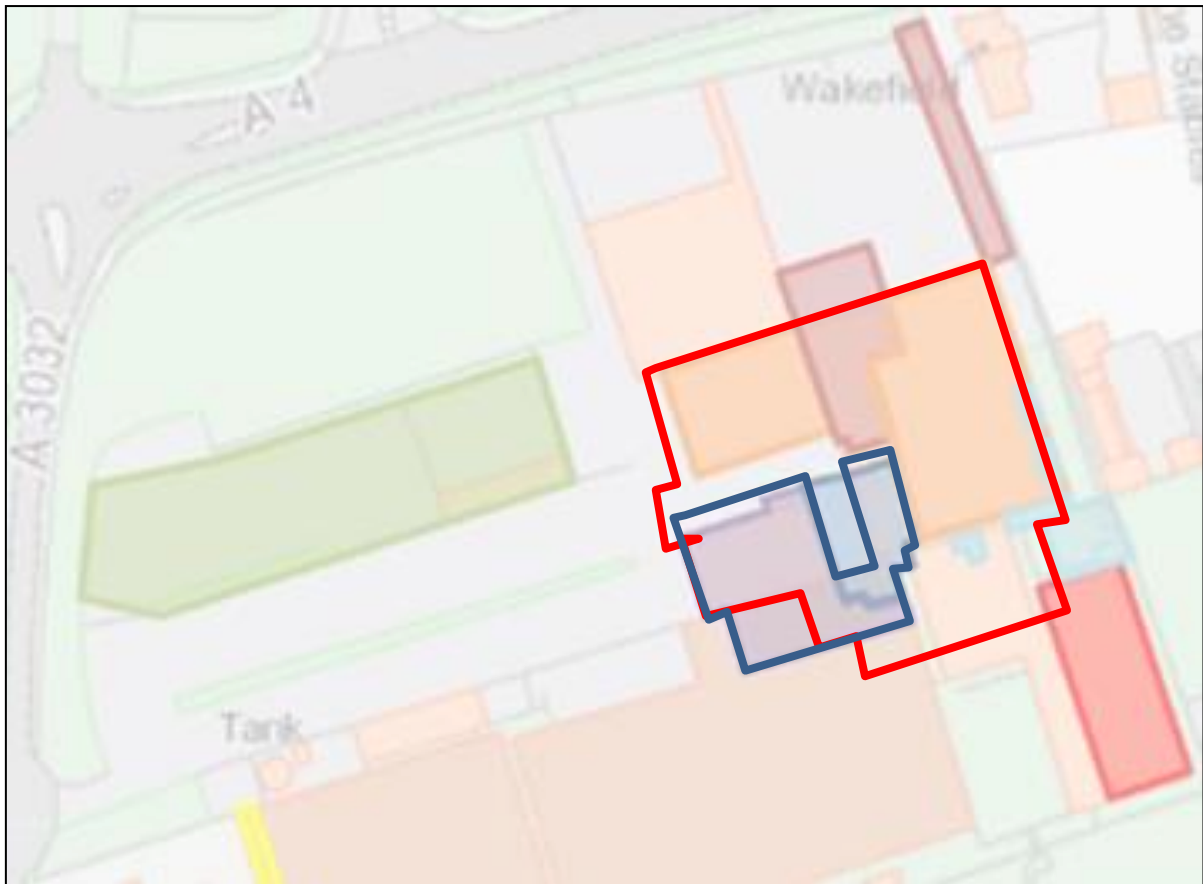
- a) The glasshouses and other structures are permanent structures subject to planning permissions over the years
- b) The site is one planning unit that was originally horticultural but since the granting of the farm shop and café uses and through the years of the 2013 enforcement notice, as a whole, it became mixed or sui generis use and not in sole horticultural (agricultural) use. It is also operated as one business and customers would interpret it as such. To apportion specific uses would be incorrect
- c) This would then render the entire site as previously developed land, with the exception being the fields to the south of the site which are undeveloped
- d) The 2013 enforcement notice does not accurately reflect the site. Even if there is dispute on this point, the replacement building will be sited on the clearly established areas of previously developed land

32. There is also reference to an appeal decision for the redevelopment of a horticultural nursery for residential dwellings at Wheeler Street Nurseries in Witley, Godalming (APP/R3650/W/16/3163050). There are two significant differences in the subject application. The appeal decision relates to a lawful and established retail use (as opposed to a temporary retail use in this application site) on a smaller condensed site of 0.7 hectares (as opposed to 2.7 hectares) and the sole planning permission for the site applied to the entire site, thereby allowing the Inspector to conclude the site was in mixed use.

33. Turning first to the planning unit, in the case of *Burdle v Secretary of State for the Environment* [1972] 1 WLR 1207 Bridge J took the opportunity to set out clear criteria for determining the correct planning unit. He identified three criteria for determining the correct planning unit, which are set out below:

- a) Whenever it is possible to recognise a single main purpose of the occupier’s use of his land to which secondary activities are incidental or ancillary, the whole unit of occupation is considered

- b) Even though the occupier carries on a variety of activities and it is not possible to say that one is incidental or ancillary to another, the entire unit of occupation should be considered
 - c) Where there are two or more physically separate and distinct uses, occupied as a single unit, but for substantially different and unrelated purposes, each area used for a different main purpose (together with its incidental and ancillary activities) ought to be considered a separate planning unit
34. A further consideration in identifying the correct planning unit arose in a case, which considered a piece of land which had been divided into small plots for occupation by caravan dwellers. In this case it was held in *Rawlings v Secretary of State for the Environment and Tandridge District Council* (1990) 60 PCR 413 that the selection of the appropriate planning unit was essentially a matter of fact and degree.
35. In the 2013 enforcement notice, the Council concluded that the site was one planning unit and the same approach has been concluded in recent planning approvals on the site. The Council considers the site is one planning unit.
36. Dealing then with the use of the site, the same recent planning approvals have recognised that the site is in a mixed use. This is consistent with the findings of the inspector in the 2019 appeal decision for the retail use, where it was concluded that the retail use was not ancillary to the horticultural use but rather, represented a material change of use.
37. Moving then to whether the site is previously developed land, the definition (quoted in full in paragraph 30) does not include any reference to mixed use sites or, particular to this case, where there is a mixture of horticulture and commercial use. That would allow, therefore, a consideration of land that is or was last occupied by agriculture as distinct from other uses on the site.
38. Returning to the table and plan of the existing uses at paragraph 19 of this report, about 70% of the site remains in horticultural use, or about 66% of the greenhouses and other buildings and 79% of the outdoor areas (not including the car park or fields). The areas in horticultural use are not previously developed land in line with the exclusion of agriculture (of which horticulture is a form of agriculture) in the NPPF.
39. Conversely, the areas in non-horticultural use (office building, farm shop and café) are considered as previously developed land. Complicating the situation further is that the existing retail floorspace, events use and exhibition space are all temporary use (until 23 March 2022, extended to 23 March 2023 and also subject to appeal). With respect to the retail floorspace, the intent of the applicant as advanced at the 2019 appeal was that some retail floorspace was needed for a temporary period to enable re-establishment of and reinvestment into the horticultural business. These areas should be excluded from previously developed land because paragraph 149 of the NPPF excludes temporary buildings and by extension, where the definition of previously developed land excludes agricultural buildings, a temporary use should also be excluded. Most but not all of the proposed garden centre building is outside the previously developed land. Almost all of the café building falls outside of previously developed land. This is illustrated in the extract of the plan below with the existing uses in background colour, the outline of the garden centre building in red and the area of previously developed land in blue.



Existing uses with outline of proposed retail and cafe overlaid in red and PDL in blue

40. Because much of the building falls outside of the areas that are considered as previously developed land, and because retail use is proposed then the proposal is inappropriate development. This is consistent with the findings of the inspector at paragraph 20 of the 2019 appeal decision.

The effect of the proposal on the openness of the Green Belt and Countryside

41. The NPPF highlights that the fundamental aim of Green Belt policy is to prevent urban sprawl by keeping land permanently open and that the essential characteristics of Green Belts are their openness and their permanence.
42. Policy TB01 of the MDD Local Plan state that development must maintain the openness of and not conflict with or harm the purposes of the Green Belt. Openness is capable of having both spatial and visual aspects. It can also require consideration of ancillary elements such as traffic generation. Volume wise, there are significant decreases in the footprint and volume of the buildings on the site in the figures provided by the applicant, which are .

Applicant's figures	Footprint	Volume	Height
Existing	9,973m ²	33,747m ³	6.5m
Proposed	4,154m ²	21,726m ³	6.7m
% increase	-58%	-35%	3%
Council's figures	Footprint	Volume	Height
Existing	11,190m ²	34,200m ³	6.5m
Proposed	5,050m ²	24,264m ³	6.7m
% increase	-54%	-29%	3%

43. In referencing spatial awareness, the applicant refers to the existing built form of the village of Hare Hatch and the opportunity to enhance openness of the site as part of the proposal. In terms of visual considerations, there is no dispute that there is a reduction of between 29 and 35%.
44. Paragraph 19 of the 2019 appeal decision for the retail floorspace noted the following with respect to openness:

The proposed sales area would mainly be contained within glasshouses. Whilst permanent these buildings, by virtue of the thin aluminium frames and extensive use of glass, are lightweight in nature and are not of substantial construction. With regard to the smaller outdoor element of the sales area, with the items proposed to be sold there would be a slight reduction in openness. If it is as popular as hoped for the sales area would also make the site busier with noticeably more visitors. This would manifest itself in increased car parking associated with visitors, staff employed in the sales area and more deliveries which would also adversely affect openness. However, the temporary three-year nature of the permission sought lessens the harm to openness that would be caused.

45. The Transport Statement submitted with the subject application suggests that there will be increased traffic movements on surrounding roads and into the site. There is also anticipated to be additional staff. With a significant increase in retail and café floorspace comes additional deliveries of goods, even when accounting for the benefits of the allotment gardens. Second, as advanced in paragraphs 76-79, whilst the southern and western areas of the site are opened up, there is additional bulk and building height closer to the A4. The permanence and greater impression of bulk that comes with this affects the openness both within the site and from outside. These two aspects render the proposal inappropriate in terms of a consideration of openness.

If it is inappropriate development, whether the harm by reason of inappropriateness, and any other harm, is clearly outweighed by other considerations, so as to amount to the very special circumstances necessary to justify the development

46. In the 2019 appeal decision, the Inspector found that a temporary planning permission would be harmful but allowed it on the basis of very special circumstances. The appeal decision notes the following circumstances:
- Customers' expectations, re-building of the nursery, allowing increased range and choice to customers to compete with other surrounding businesses
 - Popularity and value to the community: Numerous letters in support of the proposal
 - Employment: Providing job security for existing employees and three additional FTE positions and positions for younger people
47. The applicant has argued that the proposal is not inappropriate development but has included their own very special circumstances if there is disagreement on this point. These include the following:

- Wholesale removal of poor-quality glasshouse structures and improvement of the built form
- Landscape and Green Belt improvements
- Net gain in biodiversity
- Economic benefits to the rural economy and the diversification of a long-standing rural business
- Enhancing customer expectations
- Community benefits including a mix of facilities and charitable use
- Employment and training (particularly for young people);
- Carbon storage woodland
- Viability and sustainability of an unsustainable existing business and opportunity to compete with local businesses
- Popularity and value to the community
- There are other garden centres in the Green Belt in the borough
- Other less desirable uses may be established in the future, including residential housing, a care home or a food store
- There has been an abuse of process by the Council
- A refusal could result in other negative outcomes or a derelict site through abandonment

48. Some of the aforementioned justification could be considered very special circumstances, either because they are consistent with the findings of the inspector in the 2019 appeal decision or they are relevant to the circumstances of the application. For instance, there remains community support, in the form of representations in favour of the development. The expansion of the business would also bring employment benefits, both during construction and use.
49. Paragraph 84 of the NPPF also supports the sustainable growth and expansion of all types of business in rural areas, both through conversion of existing buildings and well-designed new buildings and the development and diversification of agricultural and other land-based rural businesses. In this respect, the sustainability and viability of the development is a relevant consideration and the Council has not disputed the conclusions reached in the Agricultural and Viability Assessment submitted with the application. Benefits of the scheme, include community use of the fields and playground, charitable intentions of the allotments and carbon capture from significant tree planting can be viewed as very special circumstances.
50. Considerations such as the fact that there are other garden centres in the borough or the potential for other uses on the site are not relevant planning considerations because each application is considered on its own merits (including whether it is within the Green Belt). Other garden centres along the A4 benefit from planning permission whereas the application site has never had a planning permission for such use. Biodiversity net gain is (or soon will be) a legislative requirement as part of the planning application process and so cannot be a very special circumstance. To argue that alternate undesirable uses or abandonment are potential outcomes is irrelevant to the application even if they are possible. Indeed, the site was put forward for inclusion in the Local Plan update for retail, café and care home use but not included because of the siting in the Green Belt. The relevance of referring to the abuse of process finding is also unclear but clearly irrelevant.
51. Overall, there are some very special circumstances that weigh in favour of the scheme, but not to the extent that they would outweigh the harm arising from the inappropriate development and the impact upon the spatial openness of the site and its surrounds.

This forms Reason for Refusal 1 and is explored further in the planning balance in paragraphs 133-139.

Sustainability

52. Policies CP1, CP6 and CP11 of the Core Strategy permit development where it is based on sustainable credentials in terms of access to local facilities and services and the promotion of sustainable transport. Being in the Green Belt, the site lacks any ready access to facilities and services and the site is located in an area with very poor sustainability credentials. Public transport is almost non-existent, with the services provided not meeting the Council's standard for a good bus service. Cycling on Bath Road is also unattractive because of high traffic levels, significant speeds and a lack of lighting and infrastructure. Most, if not all, customers would need to drive to the premises.
53. The proposal would reinforce the heavy reliance upon private car travel in an area that is poorly serviced by public transport and through Twyford town centre which is an Air Quality Management Area. The Transport Statement submitted in support of the application also demonstrates that there will be an increase in trip movements. However, in the appeal decision for 173316, the Inspector noted that:

'Nurseries are of necessity located in rural areas. Moreover, by the nature of what they sell the majority of customers have to travel by car in order to take home the plants and related items they have bought. Therefore, whilst policy CP6 of the Core Strategy supports granting planning permission where a choice of sustainable transport options is available, given the nature of a horticultural nursery, such provision is not appropriate in this instance.'

54. By extension, this applies to garden centres but only to a degree. There are other similar retail uses in the near vicinity and the consideration of the sequential test in paragraph 58 concludes that an out of town location is broadly acceptable in terms of the NPPF. On this basis, there is insufficient justification to refuse the application on these grounds.
55. Input from the Council's Highways Officer has noted that the application is supported by a framework travel plan statement. The document sets out interim mode share and targets with the final mode shares and targets set following surveys undertaken six months after opening. It includes a travel plan co-ordinator. Whilst no budget was mentioned in the framework, the full travel plan will include how it will be resourced and managed. There is broad support of the contents and intent of the Travel Plan although it should include an action plan, more on potential initiatives and linking into the Borough's MyJourney initiative. These aspects could be conditioned.

Retail development

56. Policy TB16 of the MDD Local Plan and paragraph 90 of the NPPF require a sequential test and retail impact test to ensure that the vitality and viability of the borough's town/district centres are not compromised by new town centre uses outside of these areas. Policy TB16 sets a threshold of 500m² and paragraph 90 sets the threshold at 2500m². The garden centre exceeds both requirements when ignoring the temporary retail permission. Policy TB18 of the MDD does allow retail development outside development limits where it is connected and economically

relates to the primary holding, is ancillary in nature and there would be no adverse impact upon other retail areas.

57. Paragraph 87 of the NPPF requires a sequential test, the direction being main town centre uses to town centres, then in edge of centre locations. Only if suitable sites are not available in those locations should out of centre sites be considered.
58. The application was submitted with a sequential test. It considers 32 alternative centre and edge of centre sites within the boroughs of Wokingham, Reading, Bracknell Forest and the Royal Borough of Windsor and Maidenhead. The overall conclusion of the sequential test is that it is not possible for the proposed development to be accommodated on any identified alternative sites in a town centre or local centre within the catchment area. Based on the information, it is considered that there are no other suitable and available alternative sites within the defined catchment area. This includes a recognition that garden centres are, by their nature, often situated in edge of centre, out of centre, or even countryside locations that require a certain type of floorspace compared to traditional in centre retail uses.
59. A retail impact assessment was included with the application. It states that the existing nursery and garden shop draw customers from a wide and dispersed area, including linked visits to other garden centres. The study does not make the case that there is a need for comparison good or garden centre floorspace. Within the catchment area, the assessment has identified the main centres that the proposals would likely draw trade from. It additionally follows the principle that like competes with like and so has identified the following out of centre garden centres: Dobbies, The Berkshire Gardner (formerly Ladds) and Wargrave Nursery Plant Centres and the Big Plant and Knowl Hill Nurseries close to Hare Hatch and other similar facilities in the wider area.
60. The applicant's Retail Assessment and Planning Statement state that the goods which are intended to be sold from the garden centre are predominantly specialist in nature, ancillary and related to the wider horticultural activities of a plant nursery. In this regard there would be little direct comparison with traditional retail offering within town centres. However, the Retail Assessment does acknowledge that the proposals would comprise between 670–955m² for sale of giftware and clothing. These goods are likely to have a retail impact, and any future diversification of products sold could also impact on in centre locations.
61. The retail impact assessment quotes typical garden centre trading densities as being around £1,250-£2,000/m². Non-food retail is quoted as having a £4,000/m² as a mid-point for town centre locations. Forecasting information projects £875m² trading density. This is markedly lower than typical garden centres and no detail has been provided to explain this, but on the face it seems unrealistic. Notwithstanding, the retail impact assessment uses a figure of £1,600 in the middle of this range for the purposes of a 'realistic' trader diversion assessment, and also includes a 'worst case' scenario which applies the £4,000m² estimate for ordinary retail to the maximum potential floorspace (955m²) for the clothing and giftware elements of the proposals. This is considered a reasonable approach. In both scenarios trading impact on centres within the catchment is less than 1%.
62. Based on the evidence shown, it is considered that the proposal would have no adverse impact on the viability and vitality of existing retail centres. The assessment

includes realistic assumptions about the type and diversity of goods sold for the purposes of the impact analysis which appropriately factors in non-garden related retail floorspace in a realistic manner. The assessment therefore appears to offer a worst-case view which is considered reasonable and proportionate.

Character of the Area

Policy context

63. Paragraph 170(b) of the NPPF requires that planning applications enhance the natural and local environment by '*recognising the intrinsic character and beauty of the countryside, and the wider benefits from natural capital and ecosystem services – including the economic and other benefits of the best and most versatile agricultural land, and of trees and woodland.*'
64. Policy CP3 of the Core Strategy states that development must be appropriate in terms of its scale, mass, layout, built form, height and character of the area and must be of high-quality design. RD1 of the Borough Design Guide SPD requires that development contribute positively towards and be compatible with the historic or underlying character and quality of the local area. RD2 relates to protecting existing features and characteristics, RD3 seeks to retain the setting and RD5 requires consistency in building form in the village.
65. There are also numerous non-residential guidelines in the Borough Design Guide, including:
 - NR1 states development should respond to key characteristics of the site
 - NR2 states that proposals should improve the area
 - NR3 requires a positive arrival impression for all modes of transport
 - NR5 notes that height, bulk and massing should respond to the local context
 - NR9 states that large floorplates must be designed to minimise potential impact upon the character of the area
 - NR12 requires consideration of boundary treatments
66. RD11 notes that replacement buildings may be appropriate in the countryside where there is no adverse increase in scale, form or footprint and where there are environmental improvements. As a reference, a 50% increase in volume for a one storey development in a permanent lawful use in the countryside, is acceptable when compared with the original building.

Landscape character area

67. The site is located within Area H1 (Wargrave-Twyford Arable Chalk Lowland) of the Landscape Character Assessment Area. It is a moderate quality landscape of moderate sensitivity, with a modest capacity for change, consisting of the following characteristics:
 - A distinctly flat to gently shelving landform, consisting of arable farmland and horticultural polytunnels, and plant nursery enclave at Hare Hatch. Linear transport corridors including A4 creating a sense of disturbance in an otherwise peaceful landscape and often remote landscape of open views.

- The key issues include hedgerow loss and soil erosion, expansion of poly tunnels leading to visual intrusion, along with commercial development pressures along Bath Road which could result in the loss of rural and settlement intactness.
68. The landscape strategy is to enhance the existing landscape character and condition through management, particularly along Bath Road and to preserve the gaps and identity of settlements.
69. Area G1 is to the east of the site and comprises:
- A sloping and gently undulating landform of mixed small to medium arable and pastoral fields Numerous small woodland blocks and roadside belts of BAP priority habitat lowland mixed deciduous woodland, characteristically a combination of oak, ash and hazel, many of which are ancient. A scattered rural settlement pattern and intervening landscaped parkland, An intimate landscape with a strong sense of enclosure.
 - The key issues spread of hobby farms leading to loss of traditional features such as hedgerows and hedgerow trees. Temporary fencing and structures resulting in a decline in rural intactness. Changes in deciduous woodland structure through loss of species diversity of the traditionally coppiced woodlands. The expansion of the coniferous element of woodlands in the past has resulted in a change in woodland character.
70. The landscape strategy to conserve and enhance the existing character; the agricultural landscape, scattered semi-natural woodlands, and parkland landscapes, the robustness of which has declined and to retain open views to the prominent wooded horizon of the adjacent character area. In terms of development, the aim is to conserve the sparse settlement pattern and historic built form of the villages, hamlets, farmsteads and manors.

Landscape visual assessment

71. The application was supported by a Landscape and Visual Impact Assessment (LVIA). The LVIA judges the quality and condition, and the sensitivity of the landscape of the site and surrounding area as medium and the Council's Trees Officer concurs with this conclusion.
72. The LVIA judges the magnitude of effect on the landscape as permanent and low, which is also agreed as the proposals rationalise built form within the site and that built form is of a similar height and use and an improved character and spatial layout. The landscape will contain many enhancements that will improve the character and quality of the site within its rural setting.
73. The assessment of the significance of effect is judged as slight/adverse initially due to the temporary impact of construction on the character area. In the long term, it will not result in any significant adverse level of effect on the character and quality of the landscape overall and support is given on these grounds.
74. Of the 15 viewpoints none, will receive a significant level of visual effect. Receptors at VP7 Wakefield Cottage receive a moderate/adverse effect from the current development. A further four receptors will continue to receive a slight adverse visual effect from the garden centre, and six slight and neutral. Once the development is

built and the landscape enhancements implemented this will help the scheme to integrate with its surroundings.

75. The Council's Trees and Landscape Officer concludes that there will be a beneficial change in landscape and visual openness in respect of the green belt, due to a spatial change within the site, with an overall beneficial change in the visual and spatial openness in respect of the green belt. However, a consideration of built form, bulk and scale remains a separate consideration.

Built form

76. Notwithstanding the visual landscape assessment submitted by the applicant, it is necessary to consider the overall bulk and appearance of the development within the site. NR5 of the Borough Design Guide SPD notes that height, bulk and massing should respond to the local context and the prevailing heights in the area.
77. NR2 of the Borough Design Guide aims to ensure that proposals improve the area. In this sense, the removal of many aging or dilapidated glasshouses and their consolidation into one garden centre building is viewed as an improvement to the site. However, NR9 states that large floorplates must be designed to minimise potential impact upon the character of the area
78. Against the backdrop of the smaller glass greenhouses (height generally to 3.1m) along the eastern and southern edges of the site, the farm shop is the dominate building on the site, with a footprint of about 380m², eaves height of 5m and ridge height of 6.6m. It is true that there is a net reduction in volume, but much of it comes from glasshouses which have an open glass appearance which suits the lawful and permitted horticultural use and many of which should be demolished when no longer required for horticulture. These glasshouses are mostly located to the southern and eastern end of the site away from the northern and western roadside boundaries.
79. The proposed garden centre building would extend east from the farm shop building with a slightly higher ridge and eaves height. Inclusive of the existing farm shop building but excluding the existing glasshouses, the footprint would increase by approximately 725% to 3300m². It would be 68m along its north west elevation and 48m along its north east elevation and the height would be about 2.6m higher than the existing glasshouses. It would not extend any closer to the A4 than the existing glasshouses but because of its increased height and footprint as well as its solid nature (as distinct from the glasshouses), it would be a much more prominent feature in the site and when viewed from the A4. This forms the primary basis for the imposition of Reason for Refusal 2. As a comparison, Dobbies on the opposite side of the A4 has an eaves height of 2.7m and ridge height of 4.9m and the footprint of the main building is 3350m².



View from outside the site at the corner of London Road and New Bath Road



View from inside the site near the service yard within the site

Parking

80. P2 of the Borough Design Guide SPD ensures that parking is provided in a manner that is compatible with the local character. NR10 states that car parking is to be unobtrusive and landscaped and NR11 requires that servicing be screened.
81. The car park and service yard occupies largely the same footprint as the existing car park but with some additional tree planting and the removal of the existing shed in the service yard. The existing row of trees to the existing car park will be retained and protected. There is a net improvement to the appearance of the area. The woodland area to the north acts as an overflow car park and once established would provide some additional screening to parked vehicles beyond that already provided by the boundary hedgerow. On the expectation that the overflow car park will be used on limited occasions, there is no objection on character grounds.

Design and materials

82. NR8 and RD6 of the Borough Design Guide SPD states the elevations are to be well composed, proportioned and detailed and RD7 requires that materials, colours and details respond to the distinctive elements of the locality. The garden centre building will comprise mid/dark green metal cladding roof and walls, with glazing to the elevations and particularly to the main entrance. When accounting for the existing horticultural glasshouses and the barn like farm shop, the continuation of these

materials is not opposed. The café will include timber cladding which is also acceptable.

83. NR3 requires a positive arrival impression for all modes of transport. The main entrance has been remodelled, a pedestrian entrance is included to the north of the site, internal pedestrianization is included, additional tree planting is provided in the car park and the main entrance to the building is largely inviting, No objection is raised on these grounds.

External lighting

84. Paragraph 180 of the NPPF aims to limit the impact of light pollution from artificial light on local amenity, intrinsically dark landscapes and nature conservation. There are no specific details of external lighting but given its Green Belt location, it would be important to minimise any lighting schemes to protect ecological aspects and dark skies. These details could form a condition of consent.

Accessibility (incorporating The Public Sector Equality Duty (Equality Act 2010))

85. The Council is required to have due regard to its obligations under the Equality Act 2010, including age, disability, gender, gender reassignment, marriage and civil partnership, pregnancy and maternity, race, religion or belief. Policy CP2 of the Core Strategy also seeks to ensure that new development contributes to the provision of sustainable and inclusive communities, including for aged persons and the disabled.
86. There is no indication or evidence that persons with protected characteristics as identified by the Act have or will have different needs, experiences, issues and priorities. The submitted plans indicated that level access is afforded through the site and given the open plan arrangement, no impediment to access through the building. There is provision for accessible toilets in the café and within the existing office building. The provision of 13 disabled car spaces accords with the minimum standards. Accordingly, there would be no significant adverse impacts and no objection raised.

Neighbour Amenities

87. Policy CP3 of the Core Strategy aims to protect neighbouring amenity and Policy CC06 and Appendix 1 of the MDD Local Plan requires that development protect noise sensitive receptors from noise impact.
88. At the intersection of London and Bath Roads, the only adjoining residential properties are at 1 and 2 Wakefield Cottages in the north eastern corner and Loggers Leap in the south western corner although the latter is within the same ownership of Hare Hatch Sheeplands. Most of the property boundaries adjoin non-residential areas, with a nursery to the south west and open land to the south and east. At its minimum, the built form of the garden shop building will be 4.2m from the eastern boundary with a maximum building height of 6.6m, which is sufficient to ensure that there are no adverse levels of dominance to Wakefield Cottages. Whilst there will be increased activity associated with the retail and café uses (including vehicle movements), it is well contained within the site and sited amongst the backdrop of other garden centres and the background noise of the A4. On this basis, no in-principle objection is raised.

Access and Movement

Parking

89. Policy CC07 and Appendix 2 of the MDD Local Plan stipulates minimum off street car and cycle parking standards, including provision for charging facilities. Retail floorspace has a parking requirement of 1 space per 20m² and café floorspace is applied at a rate of 1 space per 5m². The proposed garden centre and café would require in the vicinity of 321 spaces.
90. The car park will be broadly in the same location as existing, with 192 spaces but with an additional overflow parking area to its north within the woodland area comprising of 91 spaces. The Council's Highways Officer is comfortable with the total provision of 283 spaces. As the proposed development involves a reduction in floor space, the parking ratio is improved from 1 space per 47m² to 1 space per 20m². All the spaces are 5m x 2.5m in accordance with the standards.
91. 13 disabled spaces are provided which meets the standard. The spaces are provided at the entrance which is supported. 20 motorcycle spaces are provided which is well in excess of the standard.
92. There would be 28 electric vehicle charging spaces (14 passive and 14 active) located within the main car park which accords with the standards. Some spaces would need to be provided for the disabled parking and location of passive spaces would need to be shown. The infrastructure for the passive places would be installed as part of construction and the process of monitoring and bring forward the passive spaces into operation would need to be included in a parking management plan. Subject to these details by condition, no objection is raised.
93. There are six cycle spaces within the service yard and 18 visitor spaces at the southern side of the car park. The total provision accords with the minimum standards. The location of the visitor cycle parking could be better suited in terms of visibility and convenience for customers but it is not unreasonable. Final details would be conditioned.

Access

94. Policy CP6(f) of the Core Strategy seeks an enhancement of road safety.
95. The existing access would be used with amendments to improve manoeuvrability at the entrance. Visibility splay distances will suit in excess of the 85th percentile speed of 28-33mph on London Road. Swept paths for HGVs are provided and are acceptable. The number of daily deliveries would be low and usually in a van or 10m lorry (not an articulated lorry) and outside of peak periods. Conflict with customer vehicles is not anticipated. Swept access to the car park for a large vehicle is also acceptable.
96. There is no pavement on the eastern side of London Road limiting pedestrian access through the vehicle access. However, a new entrance is introduced from New Bath Road to the north with access through the car park, which is a positive outcome. In

the absence of any detail, pedestrian and cycle directional signage would need to form a condition of consent.

Traffic

97. Paragraph 111 of the NPPF states that development should only be prevented or refused on highways grounds if there would be an unacceptable impact on highway safety, or the residual cumulative impacts on the road network would be severe. Policy CP6(e) seeks to mitigate any adverse effects upon the local and strategic transport network that arise.
98. A Transport Statement was submitted with the application. Observed flows for the existing site were collected in September 2021 and the TRICS database was used to calculate the forecast trips from the proposed development with other representative sites and figures from Saturday movements (busiest of the days) included. The only trips during the AM and PM peak periods will be staff.
99. The net increase in traffic activity is estimated at 63 movements in the network peak hour (approximately 1 movement per minute). Against the daily fluctuations of traffic on the local roads, the anticipated impact is not severe and the Council's Highways Officer has not raised objection.
100. In the absence of a Construction Method Statement or Management Plan, the document would be conditioned. It is required because of the scale of the development and its location on a busy connector road.

Trees and Landscaping

101. Policy CC03 of the MDD Local Plan aims to protect green infrastructure networks, promote linkages between public open space and the countryside, retain existing trees and establish appropriate landscaping and Policy TB21 requires consideration of the landscape character.
102. A tree survey schedule and Arboricultural Assessment and Method Statement including a Tree Protection Plan were submitted with the application. A Landscape Strategy and Preliminary Ecological Appraisal and Biodiversity Impact Assessment were also provided. There are 53 trees (including 27 Category B and 22 Category C trees) within the site with seven proposed for removal and two proposed for relocation. Accounting for replacement planting in the woodland and elsewhere within the site, there is a significant increase in tree planting and the Council's Trees Officer is supportive.
103. The roadside boundaries are marked with a hedgerow. There are some trees within/close to the hedge that are mostly Horse Chestnut, which are to be maintained and enhanced with native hedge and copse planting. The maintenance will enhance the visual quality of the boundary along with new tree planting along this boundary that maintains visibility whilst enhancing the view, as would be conditioned.
104. All of the proposed structures are outside of the root protection areas of the retained trees, including the row of Indian Bean trees in the car park, which will be retained. When recognising the replacement planting, the Council's Trees Officer raises no objection to the tree removal or the level of arboricultural information

105. The wildflower meadow and the provision of play and recreation areas in the gardens and play area being supportive of policy. Managing the existing trees to achieve optimum contribution to biodiversity and carbon capture is acceptable. Additional mature trees along the northern boundary but at suitable distances to retain permeability and a view into the site from passers by would need to form part of a revised landscape scheme. Additional mixed scrub and hedgerow is included to the farm field and allotments.
106. The woodland at the northern part of the site with *Paulownia tomentosa* is unacceptable because it is contrary to the recommendations within the Landscape Character Area. *Paulownia* would look out of place and be potentially invasive in the landscape. The alternate species, as would be conditioned, is native Hazel (*Corylus avellana*), or a native mix of shrubs and trees, especially fruiting varieties that will encourage biodiversity. These still provide good rates of carbon capture and in any event, a final species mix would be negotiated as part of a conditioned landscape scheme.
107. The site layout incorporates landscape features and elements that would need to be subject to further details as conditioned in a landscape layout/strategy plan. This includes details of how the northern woodland area would operate as an overspill car park, equipment in the play area, hard landscape elements, native trees and bushes on the boundary and treatments in the allotments.

Ecology

108. Section 15 of the NPPF aims to protect and enhance biodiversity. Policy CP7 of the Core Strategy states that development which may harm habitats or, species of principle importance will be resisted. Policy TB23 of the MDD Local Plan requires the incorporation of new biodiversity features, buffers between habitats and species of importance and integration with the wider green infrastructure network.
109. The application is supported by a Preliminary Ecological Appraisal Survey report (Pleydell Smithyman, October 2021) and an accompanying Biodiversity Impact Assessment spreadsheet.

Bats

110. Only one of the buildings has been identified as having bat roost potential and it is to be retained. The Council's Ecology Officer agrees that no further survey information is required or that there are likely to be any adverse effects on the bat population.

Other Mammals

111. Hedgehog and Badger have been recorded locally and may be active on site at the time of any construction works. Mitigation measures to reduce the risk of harm are included within a conditioned CEMP.

Amphibians

112. A risk assessment of the potential to adversely affect Great Crested Newt using the Natural England rapid risk assessment tool has been undertaken. It is agreed that

even if a population of Great Crested Newt is discovered in the identified ponds, the subject site is sufficiently remote to result in no adverse effects on the local population. Further survey work or mitigation is unnecessary.

Reptiles

113. The ecological report recognises that there are habitats on site that present suitable foraging and resting places for the widespread reptile species. Nearly all of these habitats and resting places would be subject to some disturbance, albeit where biodiversity enhancements are proposed.
114. The proposed habitat changes would likely result in an improvement in terms of suitability for reptiles and it is concluded that there will be a long-term minor benefit for reptiles. There remains a risk to individual animals during construction but this could be adequately mitigated by the implementation of precautionary measures as covered in a CEMP.

Species specific enhancements

115. No species-specific enhancements have been proposed and the Council's Ecology Officer seeks species enhancements relevant to hedgehogs, bats, birds, and reptiles to be incorporated in the areas of soft landscaping, as conditioned.

Biodiversity Net Gain

116. The Biodiversity Impact Assessment has been undertaken in the most recent Defra metric 3.0 calculator. Taking into account habitat enhancement measures the ecological report posits the headline change of a 26.44% in habitat units and a 564.22% change in hedgerow units.
117. The Council's Ecology Officer has reviewed the findings and departs from the findings in the following areas:
- a) A moderate strategic significance of a local strategy for biodiversity (rather than the high as specified) because of a lack of justification
 - b) A poor condition in the summer flowering butterfly and bee meadow mix (rather than moderate as specified) because it is shaded or in very thin marginal strips
 - c) A poor condition for the broadleaved woodland (rather than moderate) because it is to be dominated by a non-native species that will be frequently cut
 - d) Rounding to 1m² (rather than 100m²) as expected in the calculator
 - e) The length of native species rich hedgerow is reduced to reflect where it forms part of the mixed scrub and will not be maintainable as a distinct hedgerow
118. Accounting for these errors, the proposal still demonstrates biodiversity net gain figures of +10.50% for habitats and +483.45% for hedgerow such that it is likely to be able to provide a habitat net gain on-site. This is reliant upon the soft-landscaping detail and an ongoing Landscape Environmental Management Plan (LEMP) being secured to bring the enhancements into effect.

Security and Crime Prevention

119. Paragraphs 92 and 130 of the NPPF are supportive of designs that are safe, inclusive and accessible and which promote health and well-being, with a high standard of amenity for existing and future users and where crime and disorder, and the fear of crime, do not undermine the quality of life or community cohesion and resilience.
120. The Crime Prevention Officer at Thames Valley Police was consulted. No in-principle objection is raised, subject to clarification of specific security measures are proposed, (including lighting and CCTV around unused night time areas) and access through the gates (such as electronic fob access). This would form a condition in the event of an approval.

Sustainable Design

121. Policy CC04 of the MDD Local Plan and the Sustainable Design and Construction SPD require sustainable design and conservation. The proposal involves the demolition of lightweight (and partly dilapidated in many areas) and reconstruction of a new garden centre building. It is of relatively simple form typical of warehouse type developments and would be appropriate on sustainability grounds. The ridgelines of the new garden centre building run east-west allowing south facing rooflights and implementation of solar panels. Fabric insulation (including glazing), airtightness and energy conservation standards in the building regulations will be met.
122. The building design is complemented by landscape features including the allotments, wildflower meadow and woodland planting. The suggestion in the Design and Access Statement is that an acre of *Paulownia tomentosa* can absorb 103 metric tons of CO₂ per year, compared to most species which can absorb 1.1 to 9.5 metric tons. Whilst the comments of the Trees Officer in paragraph 106 seek alternative Hazel planting, the outcome remains a positive aspect and will accord with the Council's climate emergency intentions.
123. Policy CC05 of the MDD Local Plan encourages renewable energy and decentralised energy networks, with encouragement of decentralised energy systems and a minimum 10% reduction in carbon emissions for developments in excess of 1000m². With the use of solar panels, this would likely be exceeded and this would be applied by condition.

Waste Storage

124. Policy CC04 of the MDD Local Plan requires adequate storage and collection arrangements for general waste, recycling, green waste and food waste. The Planning Statement refers to the continued management of waste in the service yard, which is acceptable, including in terms of access and turning space. Final details of the storage structure, if any, would form a condition of consent.

Flooding and Drainage

125. Policy CC09 of the MDD Local Plan requires consideration of flood risk from historic flooding and Policy CC10 requires sustainable drainage methods and the minimisation of surface water flow.

126. A Flood Risk Assessment was submitted with the application. The site and access is within Flood Zone 1 and the proposal represents no additional flood risk or vulnerability. It is therefore acceptable in terms of Policy CC09.
127. With a reduction in overall building footprint and the use of gravel in the car park, there is a net reduction in the impermeable areas of the site and this would likely result in an improvement in on site infiltration and peak runoff rates. The Council's Drainage Officer raises no objection on these grounds.

Employment Skills

128. Policy TB12 of the MDD Local Plan requires an employment skills plan (ESP) with a supporting method statement for major development such as the subject application. It is intended to provide opportunities for training, apprenticeship, or other vocational initiatives to develop local employability skills required by developers, contractors, or end users of the proposal.
129. Based on the total floorspace, the employment skills plan would generate a need for five community skills support positions (eg work experience or CSCS training courses), two apprenticeships and two jobs. If for any reason, the obligation is not delivered, a contribution in lieu totalling £15,000 is required. This is based on the cost of the Council supporting the employment outcomes of the plan. Both scenarios would be incorporated into a s106 legal agreement.

Site Contamination

130. With a historic horticultural use, there is no known contamination of the site.

Nuclear Zone

131. The site is within the special case zone of the Burghfield Atomic Weapons Establishment but there are no specific implications for this proposal and no objection is raised.

Community Infrastructure Levy

132. It is anticipated that the development would be CIL liable as there is a net increase in retail floorspace, both in terms of permanence (against the previous temporary permissions) and overall floorspace. It is payable at £50/m² index linked.

CONCLUSION

133. Paragraph 8 of the NPPF sets out that achieving sustainable development means that development should satisfy three overarching objectives in relation to economic, social, and environmental benefits.
134. There are economic benefits in the scheme and this is clearly evident with an expansion of the existing business. It includes short term construction jobs and job creation associated with the future use (including for younger persons). However, it needs to be borne in mind that this is an area which enjoys very high levels of employment. There is also support for local businesses through the use of local

contractors and producers. More broadly, there is stimulation to the economy of the local area and increased competition resulting in benefits for customers. It attracts moderate weight.

135. Social benefits are evident in terms of the community support that the proposal has garnered. In excess of 380 submissions were received (although they are from a wide area and some do not live within the local community), many citing the community feel of the business, the friendly staff and the social and mental health benefits that the proposed development would bring. A children's play area and allotment gardens for charitable purposes would have clearly apparent benefits. This attracts minor weight.
136. In terms of environmental benefits, there are several additional aspects to the development that contribute to a wider environmental benefit. The woodland planting aids in carbon capture and screening of the site whilst the wildflower meadow contribute to a wider net biodiversity net gain for the site. The redevelopment of the site also brings about a renewal of the site by removing dilapidated structures although a rundown visual appearance is not sufficient justification to redevelop a site on its own. Moreover, some of the glasshouses would have to be removed if the use is changed or abandoned.
137. In addition, it is worthwhile to consider the 2011 application for a garden centre even though it involved a larger scale development. It was refused for six reasons, with the following summary outlining any change in stance with respect to the assessment of the subject application:
 - a. Poor site sustainability – The inspector in the 2019 appeal recognised that horticultural nurseries are rural enterprises by their nature and that cars are needed to transport goods. By extension, this applies to garden centres but only to a degree. There are other similar retail uses in the near vicinity and on this basis, there is insufficient justification to refuse the application on these grounds.
 - b. Increase in retail floorspace was inappropriate development in the Green Belt – The 2011 application predated the NPPF and was assessed against PPS2 when paragraph 149(g) did not exist. However, the Council maintains that the site does not constitute previously developed land and the principle of the development remains unacceptable as noted in Reason for Refusal 1
 - c. Loss of openness to the rural setting by virtue of bulk and nature of buildings and hard paving – There is a net reduction in volume in the subject scheme but a taller and more visible built form which weighs against the proposal. This forms Reason for Refusal 2.
 - d. Retail impact upon Twyford Town Centre – The Council has reviewed the retail impact assessment and raises no objection.
 - e. Lack of Flood Risk Assessment – An FRA has been provided with the subject application
 - f. Increased use of substandard access onto London Road – There are improvements to the main vehicular entrance in terms of manoeuvrability and visibility and the Council's Highways Officer has no objection to the scheme
138. The adverse impacts of granting planning permission relate to the retail intensification of and within the site, including traffic generation, deliveries, movement of goods, staff and customers. These impacts are heightened because of the location

within the Green Belt. There is harm to the openness brought about by the more permanent impression of the buildings and their increased height and siting closer to the road and the increase in activity.

139. In summary, the scheme does not accord with the Development Plan and the NPPF insofar as it involves inappropriate development and a degree of harm to the openness of the Green Belt and the character of the countryside. The economic, social and environmental benefits are not sufficient to outweigh the impacts and this is outlined in Reason for refusal 1 and 2. Reason for refusal 3 relates to a lack of an Employment Skills Plan, solely based on the absence of a legal agreement.

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HARE HATCH - SHEEPLANDS



LEGEND

- Application Site Boundary
- Adjoining land under ownership of Applicant.
- Red dashed lines indicate approximate areas for approved non-horticultural uses.

223

53.3m

A 3032

A 4

GARDEN SHOP

Existing Site Layout Plan

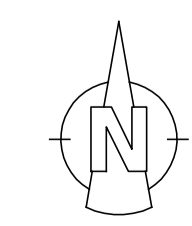
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CLIENT: HARE HATCH - SHEEPLANDS	
TITLE: Existing Site Layout Plan	
DATE: AUG 2021	SCALE: 1:500@A1
DRAWN: STM	CHECKED: PP
DRAWING N°: M07.188.D.016	REVISION: J

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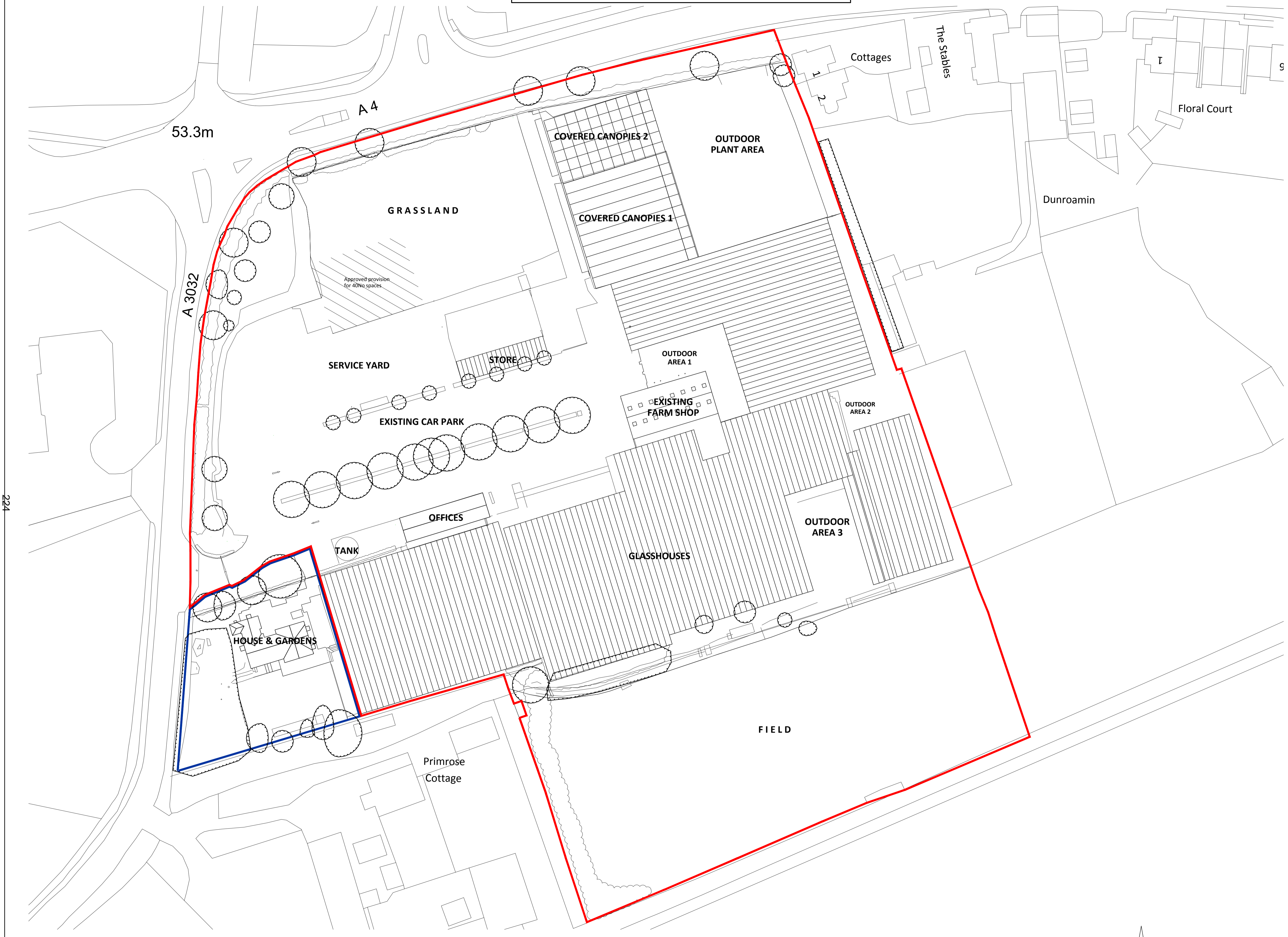


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HARE HATCH - SHEEPLANDS

LEGEND

- Application Site Boundary
- Adjoining land under ownership of Applicant.



224

Existing Site Plan

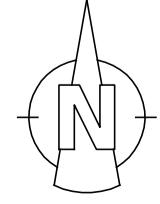
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PROJECT: HARE HATCH - SHEEPLANDS	
CLIENT: HARE HATCH - SHEEPLANDS	
TITLE: Existing Site Plan	
DATE: AUG 2021	SCALE: 1:500@A1
DRAWN: STM	CHECKED: PP
DRAWING N°: M07.188.D.019	REVISION: D

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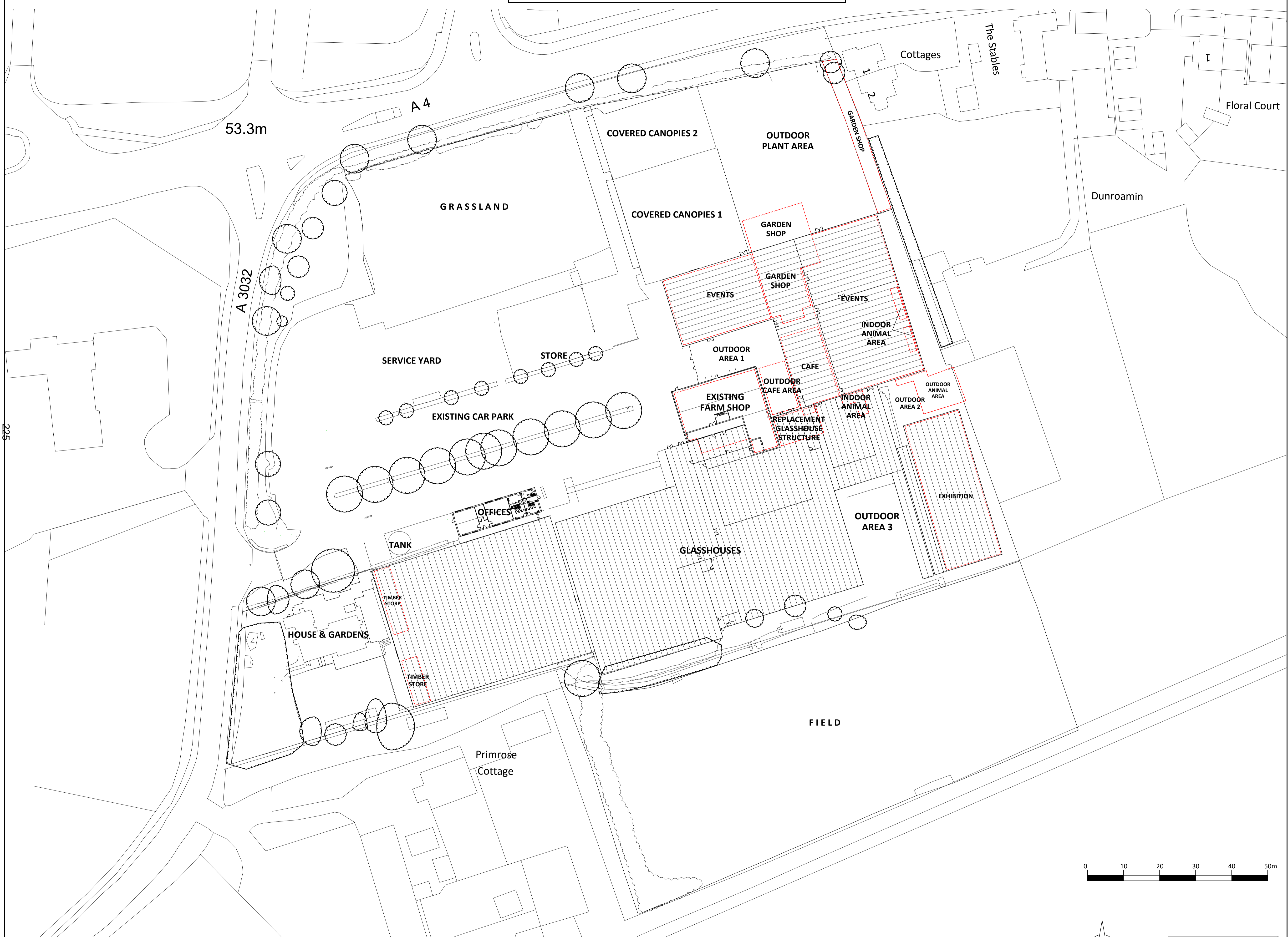
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SHROPSHIRE, TF8 7NH.

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HARE HATCH - SHEEPLANDS

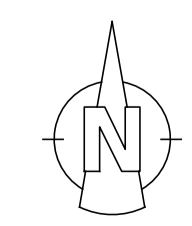


LEGEND

Red dashed lines indicate existing zonal areas.

225

Existing Floor Plan



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DRAWING STATUS: PLANNING	
PROJECT: HARE HATCH - SHEEPLANDS	
CLIENT: HARE HATCH - SHEEPLANDS	
TITLE: Existing Floor Plan	
DATE: AUG 2021	SCALE: 1:500@A1
DRAWN: STM	CHECKED: PP
DRAWING N°: M07.188.D.020	REVISION: G

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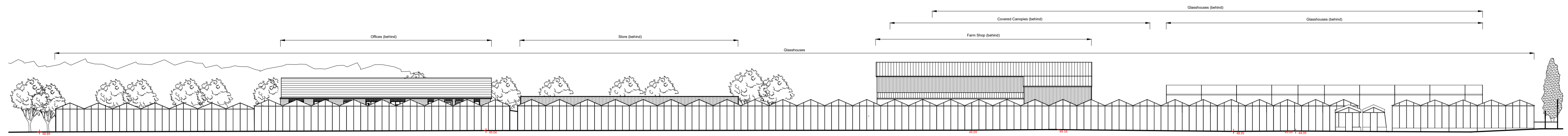
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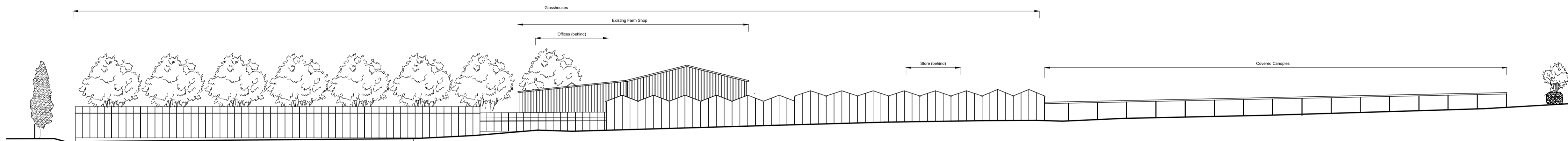
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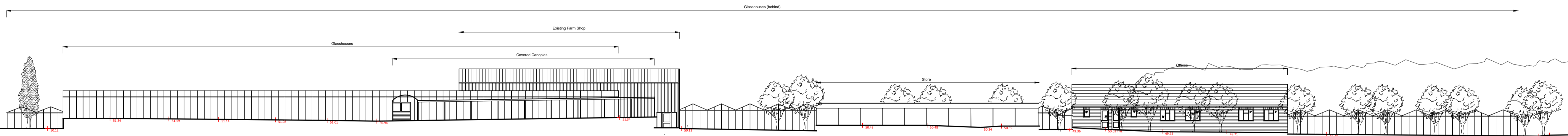
HARE HATCH - SHEEPLANDS



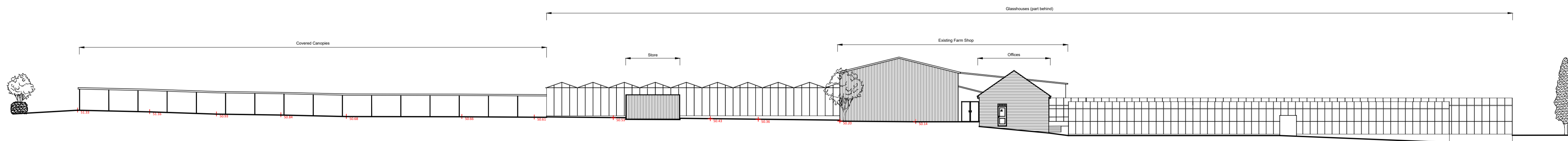
EXISTING SOUTHEAST ELEVATION 1:250



EXISTING NORTHEAST ELEVATION 1:250

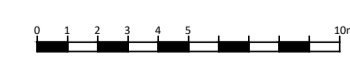


EXISTING NORTHWEST ELEVATION 1:250



EXISTING SOUTHWEST ELEVATION 1:250

2/26



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Existing Elevations

DRAWING STATUS: PLANNING	
PROJECT: HARE HATCH - SHEEPLANDS	
CLIENT: HARE HATCH - SHEEPLANDS	
TITLE: Existing Elevations	
DATE: SEP 2021	SCALE: 1:250@A1
DRAWN: LS	CHECKED: PP
DRAWING N°: M07.188.D.022	REVISION: B

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HARE HATCH - SHEEPLANDS



Proposed Site Layout Plan

LEGEND

- Application Site Boundary
- Adjoining land under ownership of Applicant.
- Number of proposed car parking spaces.**
- Core Area**
12No main car park spaces which includes:
28No EV spaces made up of:
14No active
14No passive
13No Accessible spaces
- Plus**
18No customer bicycle parking
6No customer bicycle parking (located in Service Yard)
9No motor bike spaces
- Overflow/Carbon Capture Woodland**
with permeable grasscrete surfacing
11No spaces.

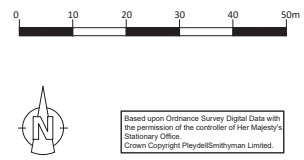
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PROJECT: HARE HATCH - SHEEPLANDS	
CLIENT: HARE HATCH - SHEEPLANDS	
TITLE: Proposed Site Layout Plan	
DATE: AUG 2021	SCALE: 1:500@A1
DRAWN: STM	CHECKED: PP
DRAWING N°: M07.188.D.017	REVISION: K

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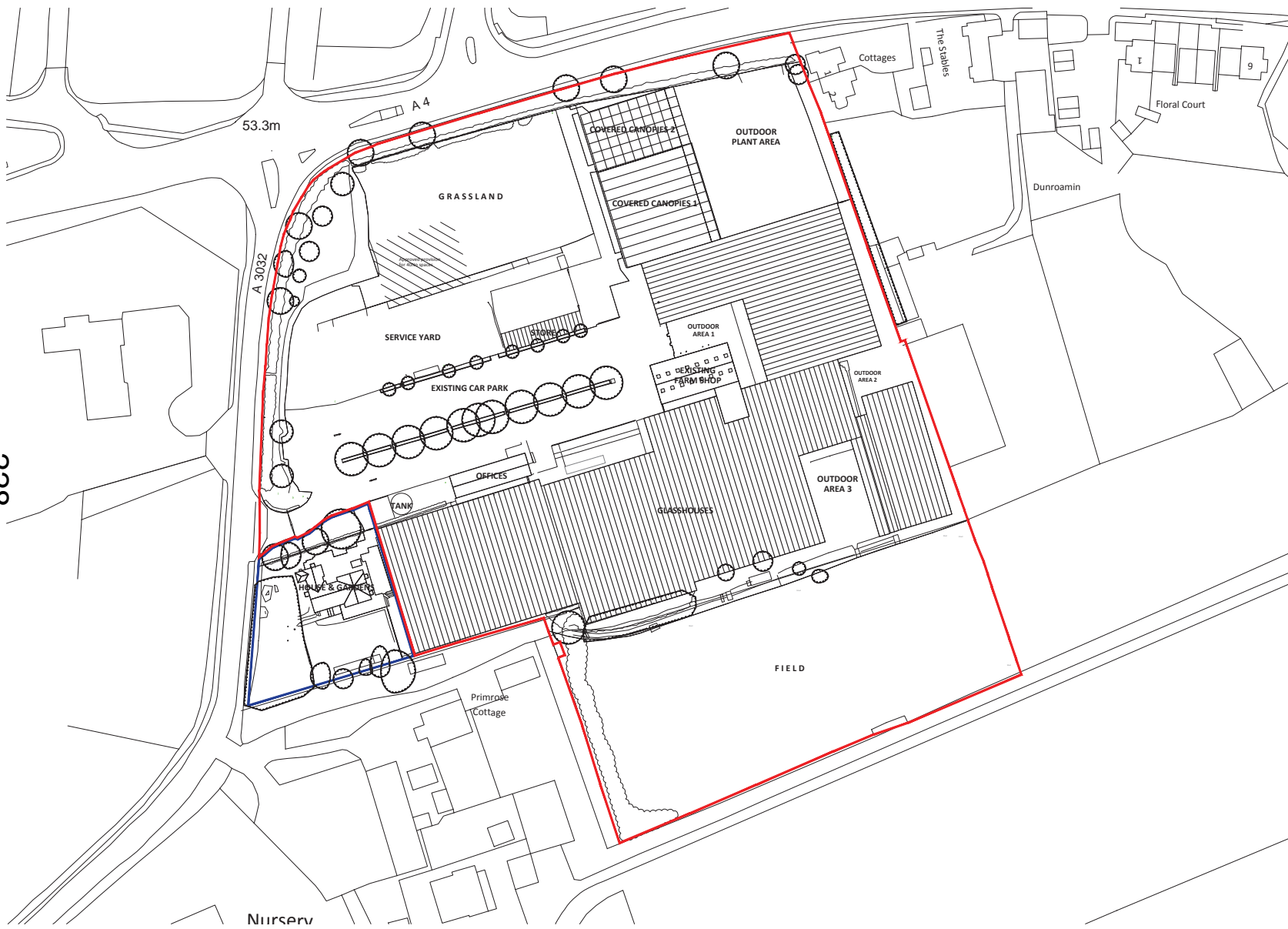
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HARE HATCH - SHEEPLANDS



LEGEND

- Application Site Boundary
- Adjoining land under ownership of Applicant.

DRAWING STATUS: PLANNING	
PROJECT: HARE HATCH - SHEEPLANDS	
CLIENT: HARE HATCH - SHEEPLANDS	
TITLE: Site Location Plan	
DATE: SEP 2021	SCALE: 1:1250@ A1
DRAWN: STM	CHECKED: PP
DRAWING N°: M07.188.D.018	REVISION: D

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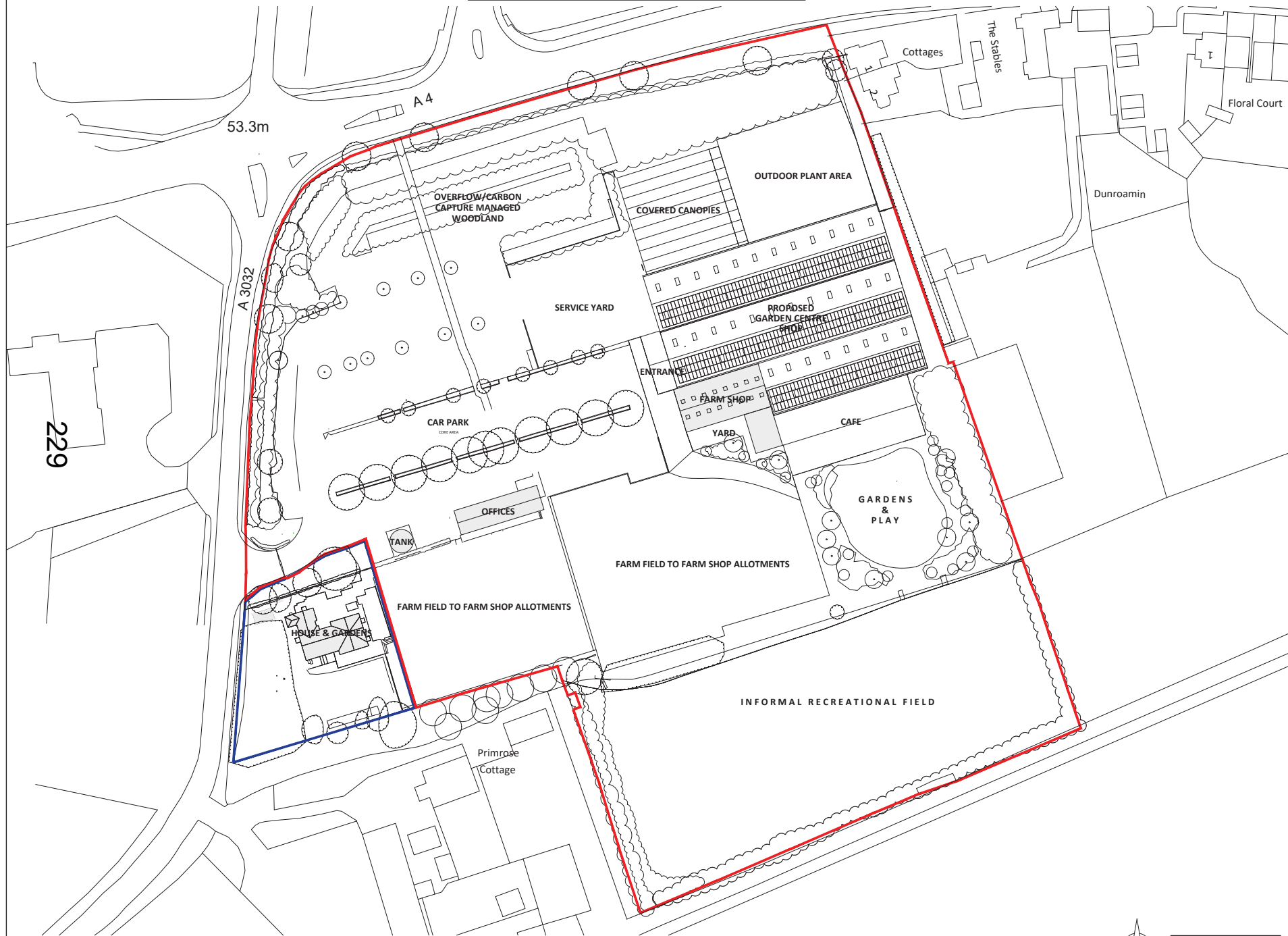
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Site Location Plan



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HARE HATCH - SHEEPLANDS



Proposed Site Plan

LEGEND

- Application Site Boundary
- Adjoining land under ownership of Applicant.

Number of proposed car parking spaces.
Core Area
 152No main car park spaces which includes:
 28No EV spaces made up of:
 *14No active
 *14No passive
 13No Accessible spaces
 Plus
 18No customer bicycle parking
 6No customer bicycle parking (located in Service Yard)
 9No motor bike spaces
Overflow/carbon Capture Woodland
 91No spaces.

DRAWING STATUS: PLANNING	
PROJECT: HARE HATCH - SHEEPLANDS	
CLIENT: HARE HATCH - SHEEPLANDS	
TITLE: Proposed Site Plan	
DATE: SEP 2021	SCALE: 1:500@A1
DRAWN: LS	CHECKED: PP
DRAWING N°: M07.188.D.023	REVISION: K

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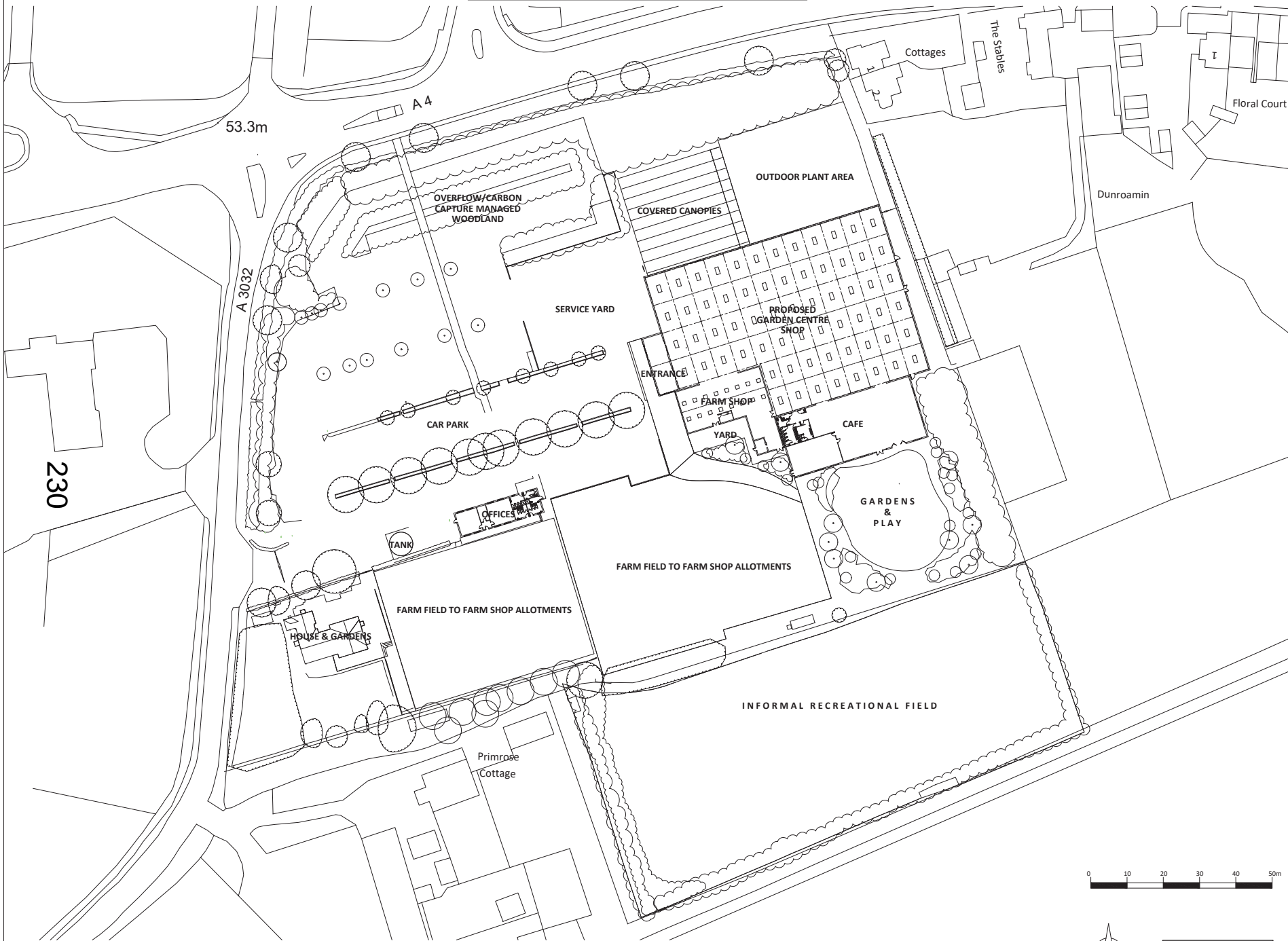
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HARE HATCH - SHEEPLANDS

LEGEND



Proposed Floor Plan

DRAWING STATUS: PLANNING	
PROJECT: HARE HATCH - SHEEPLANDS	
CLIENT: HARE HATCH - SHEEPLANDS	
TITLE: Proposed Floor Plan	
DATE: SEP 2021	SCALE: 1:500@A1
DRAWN: STM	CHECKED: PP
DRAWING N°: M07.188.D.024	REVISION: H



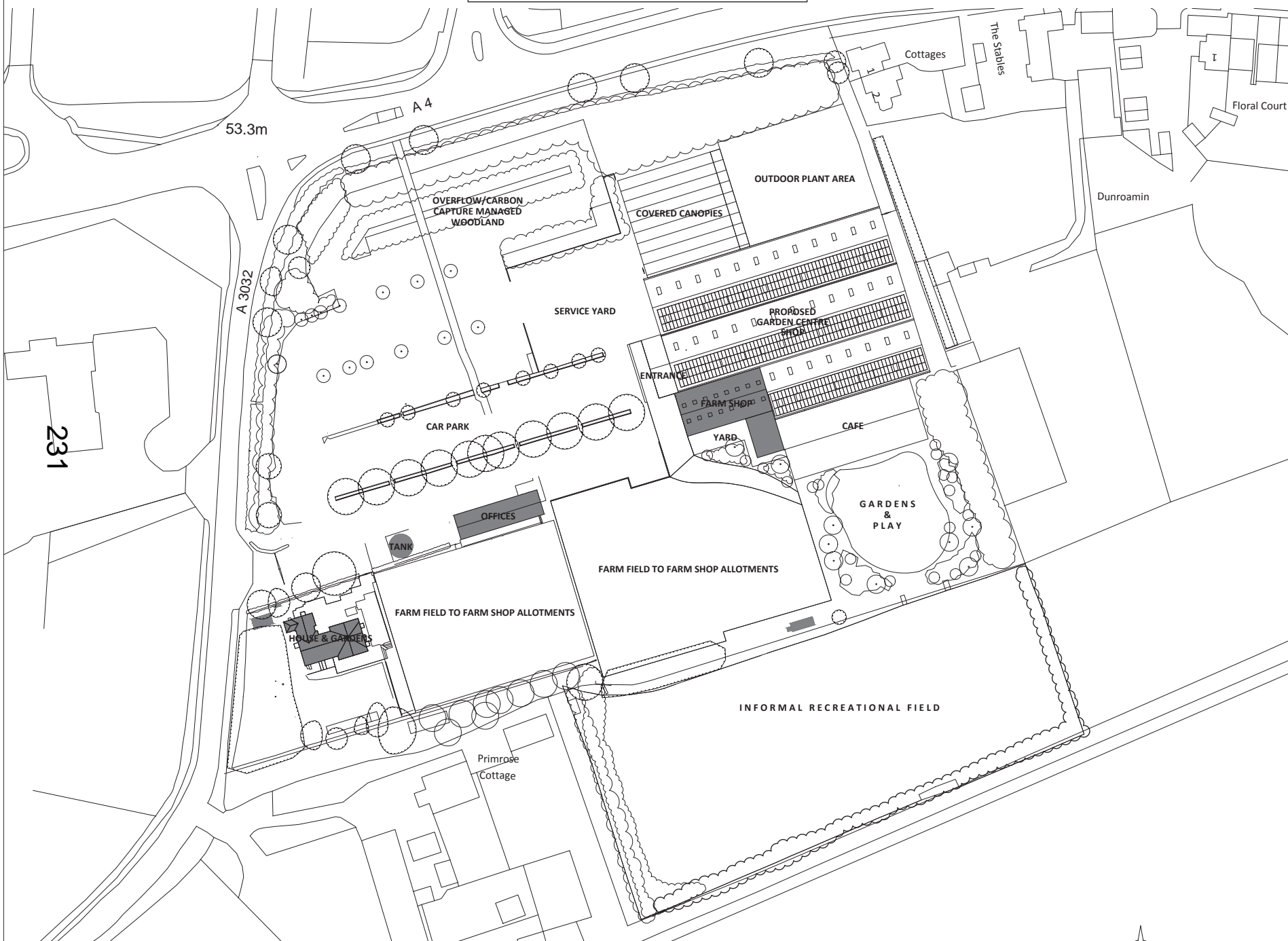
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HARE HATCH - SHEEPLANDS

LEGEND



Proposed Roof Plan

DRAWING STATUS: PLANNING	
PROJECT: HARE HATCH - SHEEPLANDS	
CLIENT: HARE HATCH - SHEEPLANDS	
TITLE: Proposed Roof Plan	
DATE: SEP 2021	SCALE: 1:500@A1
DRAWN: STM	CHECKED: PP
DRAWING N°: M07.188.D.025	REVISION: G

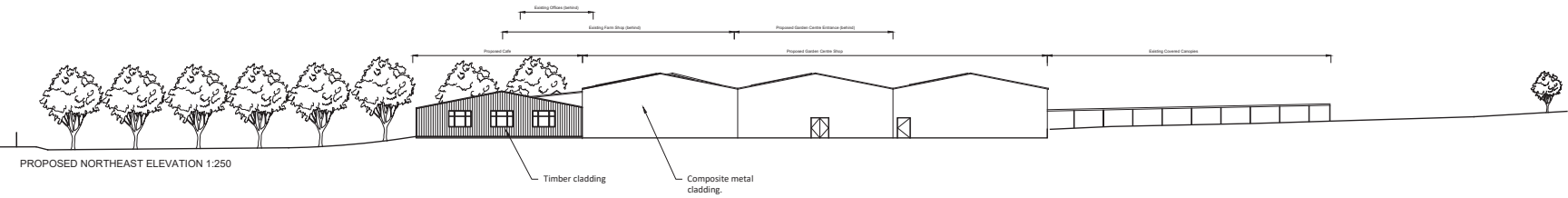
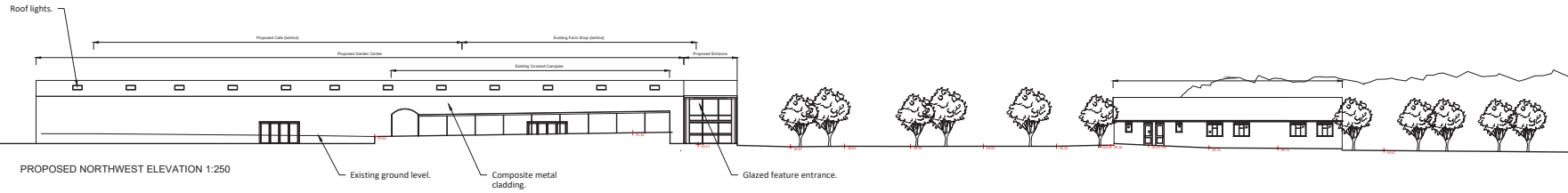


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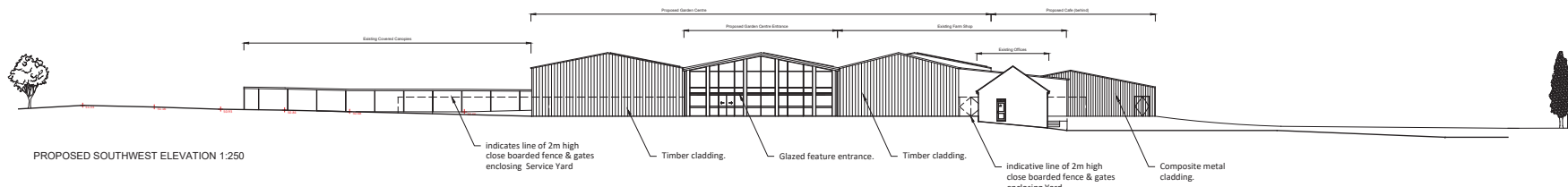
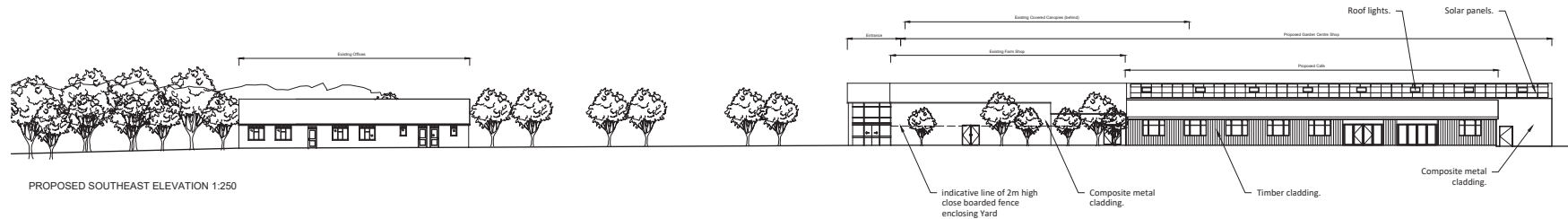


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HARE HATCH - SHEEPLANDS



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Proposed Elevations

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DRAWING STATUS: PLANNING	
PROJECT: HARE HATCH - SHEEPLANDS	
CLIENT: HARE HATCH - SHEEPLANDS	
TITLE: Proposed Elevations	
DATE: SEP 2021	SCALE: 1:250@A1
DRAWN: LS	CHECKED: PP
DRAWING N°: M07.188.D.026	REVISION: B

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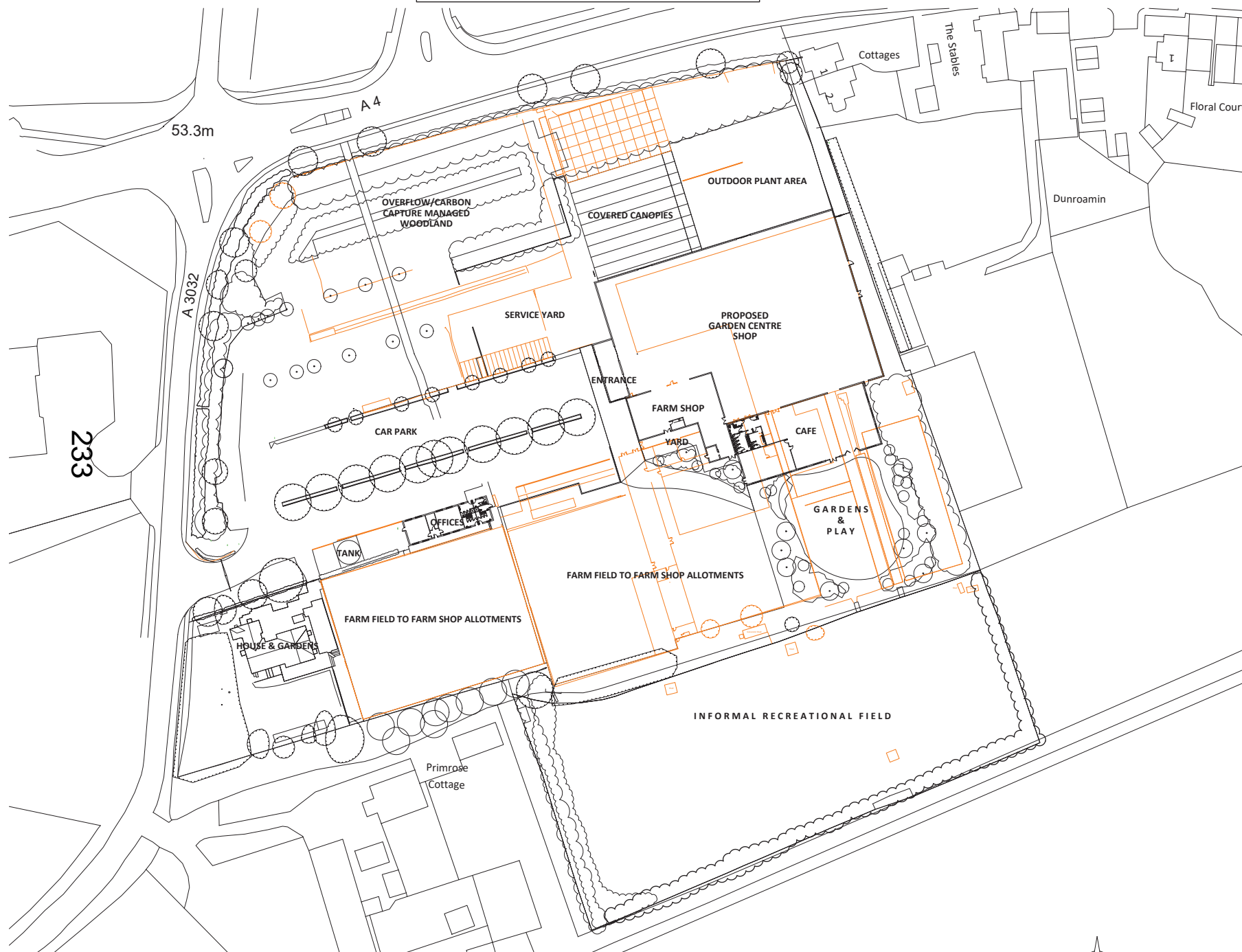
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HARE HATCH - SHEEPLANDS

LEGEND

Orange indicates demolitions



Demolitions Plan

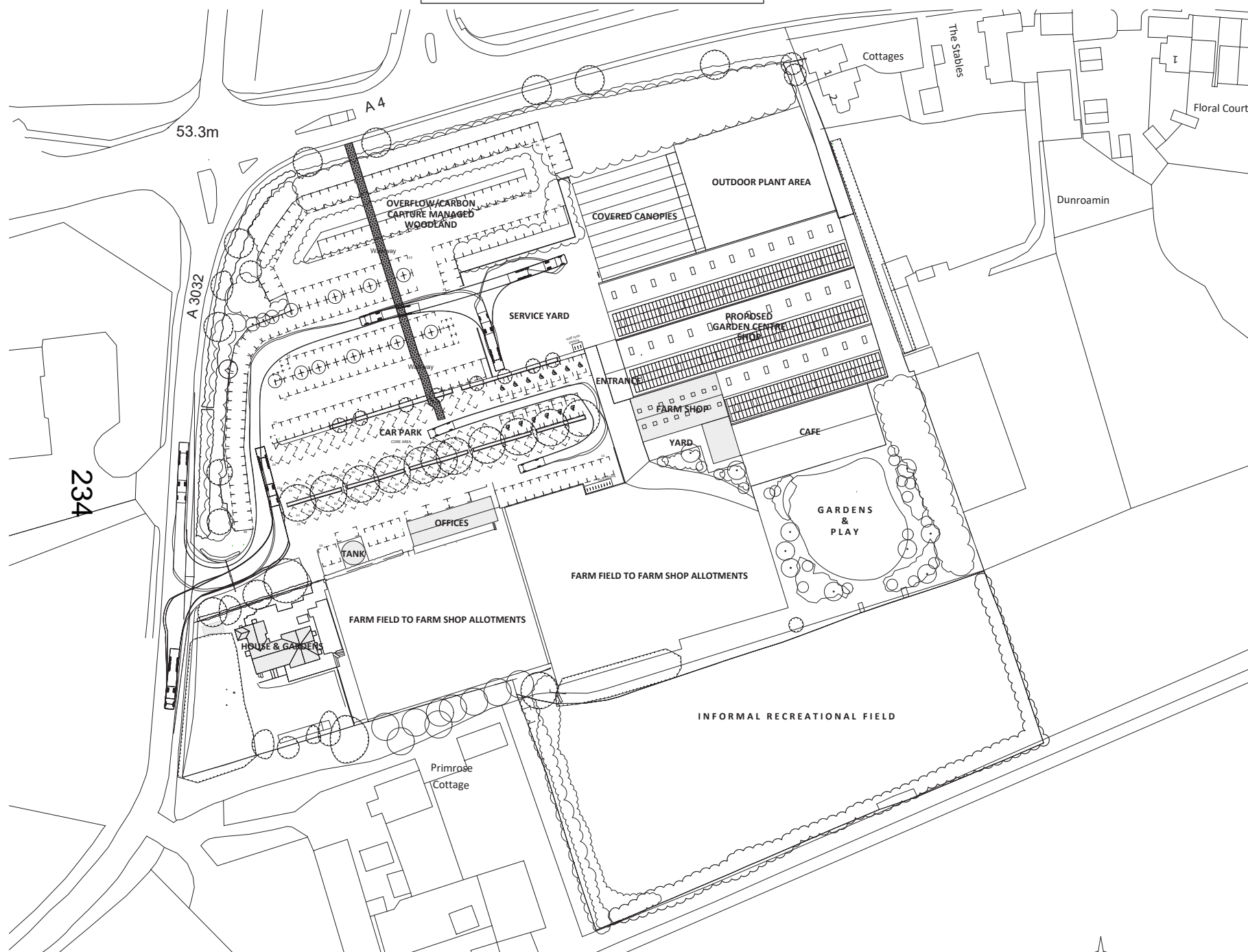
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PROJECT: HARE HATCH - SHEEPLANDS	
CLIENT: HARE HATCH - SHEEPLANDS	
TITLE: Demolitions Plan	
DATE: SEP 2021	SCALE: 1:500@A1
DRAWN: LS	CHECKED: PP
DRAWING N°: M07.188.D.027	REVISION: 1



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HARE HATCH - SHEEPLANDS



Proposed Parking Plan

LEGEND

Number of proposed car parking spaces.

Core Area
 192 No main car park spaces which includes:
 28 No EV spaces made up of:
 14 No active
 14 No passive
 13 No Accessible spaces

Plus
 18 No customer bicycle parking
 6 No customer bicycle parking (located in Service Yard)
 9 No motor bike spaces

Overflow/Carbon Capture Woodland
 91 No spaces.

DRAWING STATUS: PLANNING	
PROJECT: HARE HATCH - SHEEPLANDS	
CLIENT: HARE HATCH - SHEEPLANDS	
TITLE: Proposed Parking Plan	
DATE: OCT 2021	SCALE: 1:500@A1
DRAWN: LS	CHECKED: PP
DRAWING N°: M07.188.D.029	REVISION: E



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HARE HATCH - SHEEPLANDS

LEGEND



Landscape Strategy Plan

DRAWING STATUS: PLANNING	
PROJECT: HARE HATCH - SHEEPLANDS	
CLIENT: HARE HATCH - SHEEPLANDS	
TITLE: Landscape Strategy Plan	
DATE: NOV 2021	SCALE: 1:500@A1
DRAWN: STM	CHECKED: PP
DRAWING N°: M07.188.D.030	REVISION: B

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HARE HATCH - SHEEPLANDS

LEGEND



DRAWING STATUS: PLANNING	
PROJECT: HARE HATCH - SHEEPLANDS	
CLIENT: HARE HATCH - SHEEPLANDS	
TITLE: Illustrative Allotment and Garden Layout	
DATE: Nov 2021	SCALE: 1:250@A1
DRAWN: LS	CHECKED: PP
DRAWING N°: M07.188.D.031	REVISION:



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Illustrative Allotment & Garden Layout



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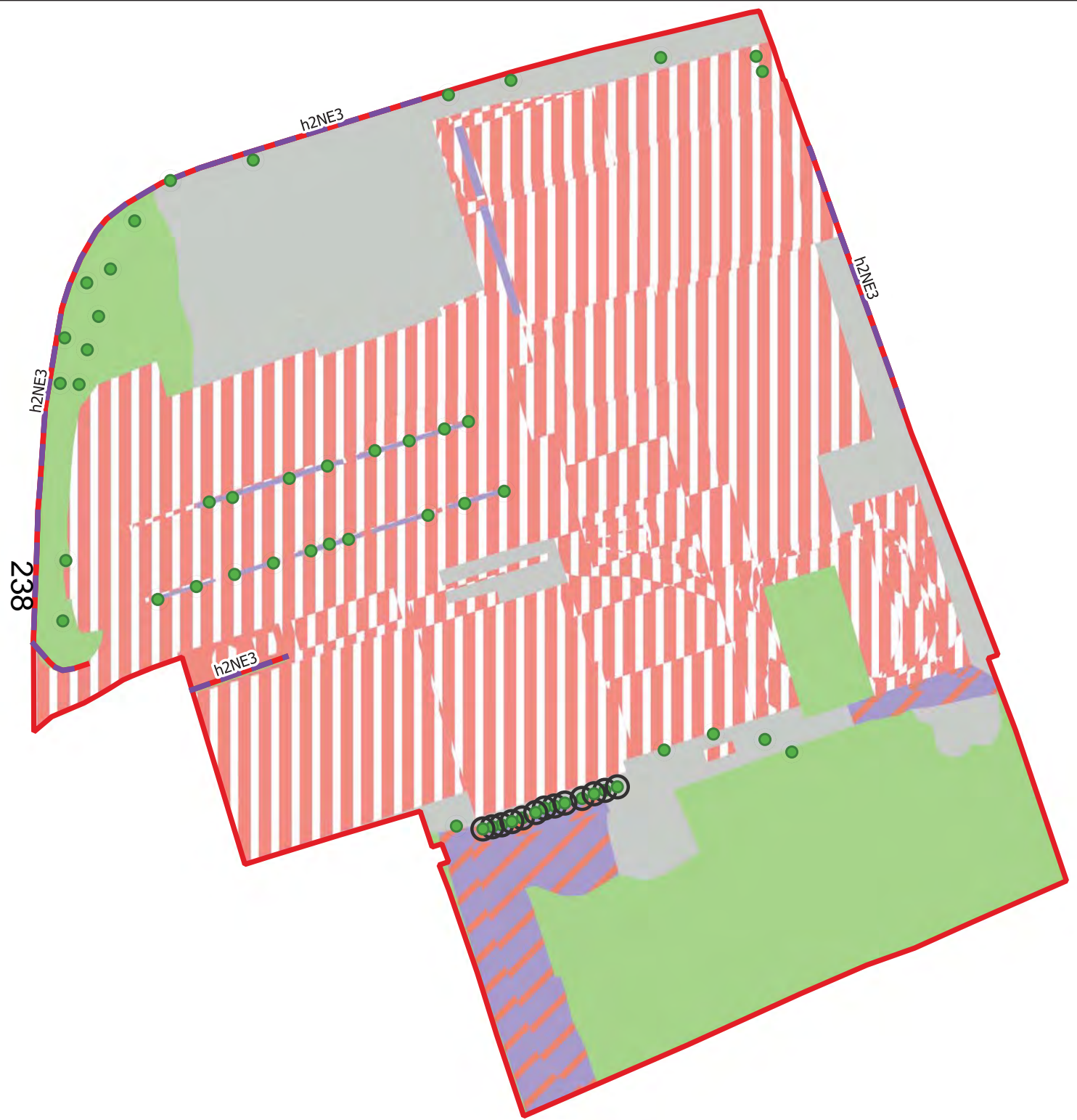


- ### Key
- Red Line Boundary
 - Allotments
 - Developed land; sealed surface
 - Introduced shrub
 - Mixed scrub
 - Modified grassland
 - Other neutral grassland
 - Other woodland; broadleaved
 - Hedge Ornamental Non Native (h2NE3)
 - Native Species Rich Hedgerow (h2NE2)
 - Proposed Large
 - Proposed Medium
 - Proposed Small



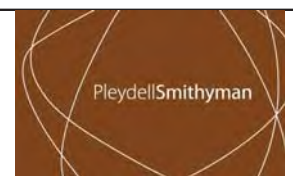
Client: Hare Hatch Sheplands		
Project: Hare Hatch Sheplands Garden Centre		
Drawing Title: BIA Proposed Habitats (with enhancements)		
Drawing No. RSE_5316_BIA Proposed	Scale @ A4: 1:1,229	Rev: V1
Drawn By: LAJM	Date: 22/11/2021	

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 TC01952 433211
 gCpsl@pleydellsmithyman.co.uk
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Key

-  Red Line Boundary
-  BraA l te nsrul
-  Deveped tand; neated nurfase
-  nIrodused nmrul
-  b odified Manntand
-  RuderatxgpheA erat
-  OasanIxderetisI tandx I areMround
-  wedMe HrnaA enIat Non NaIive (h2NgE3)
-  g) ininMLarMe
-  g) ininMb ediuA
-  g) ininMSA att



PtenIC ware waIsh Sheeptandm		
i roiesIC ware waIsh SheeptandmGarden PenIre		
Dra: inMTiIteC BcJ Banetine wal iiaIm		
Dra: inMNo.		RevC
RSg65E5G6BcJ Banetine		05
Dra: n ByC	Ssate V J@@	DaIeC
LJ_b	1:1,229	22/11/2021

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 TC01952 433211
 gCpsl@pleydellsmithyman.co.uk
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PLANNING REF : 214108
PROPERTY ADDRESS : Council Office
: Pavilion, Recreation Road, Wargrave, Wokingham
: RG10 8BG
SUBMITTED BY : Wargrave Parish Council
DATE SUBMITTED : 01/02/2022

COMMENTS:

Wargrave Parish Council had no objection to this application.

Note: Improved visibility splay looking to the right of the existing access to the site would be welcomed.

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Agenda Item 86.

Application Number	Expiry Date	Parish	Ward
214046	15 March 2022	Earley	Hawkedon

Applicant	The Owner and/or Occupier
Site Address	Auto Trader House and Hartman House, Danehill, Lower Earley RG6 4UT
Proposal	Full application for the proposed demolition of the existing office buildings (Class E(g)(i)) and the erection of 1 no. building to form a single employment unit (Flexible Class E(g)(iii), B2 and B8 use(s)) with ancillary offices, including vehicular and pedestrian access, parking, landscaping, infrastructure and associated works
Type	Full
Officer	Simon Taylor
Reason for determination by committee	Major application (>1000sqm floorspace)

FOR CONSIDERATION BY	Planning Committee on Wednesday 9 March 2022
REPORT PREPARED BY	Assistant Director – Place

SUMMARY
<p>The application site is part of Cutbush Industrial Estate at Danehill in Lower Earley. It comprises two x two-storey buildings originally in mixed office and light industrial uses (most recently occupied by Auto Trader and an AV business) but which have been vacant and boarded up since 2015. Planning permission was granted at appeal in 2019 for the redevelopment of the site for 76 residential units across three buildings. Prior approval was later granted for conversion of the existing buildings for 26 residential units.</p> <p>Neither application has been implemented and there is no longer any intention to bring forward residential units on the site with the planning permission expiring in June 2022. Instead, the application involves the construction of a single industrial and logistics building in Class B and E(g) use with a footprint of about 3200sqm and car parking for 48 cars.</p> <p>There are no in-principle objections to the proposal with no objections received from the town council, ward members, surrounding residents or from internal or external consultees.</p> <p>The principle of the development is acceptable and it represents a positive redevelopment of an otherwise derelict site. The sustainability credentials for the building are BREEAM excellent. The parking and access arrangements are well designed and whilst there is a shortfall with the overall parking requirement, there is alternative transport measures and on-site infrastructure and overall support from the Council's Highways Officer. There is a net gain of full-time employment. 24-hour use is proposed to support its logistics purpose and this is acceptable on neighbour amenity grounds. There is also an acceptable landscape scheme to complement the woodland surrounds.</p> <p>No objection is raised subject to pre commencement details including construction management (Condition 3), highway construction (Conditions 4-5), EV charging (Condition 6), drainage (Condition 8), contamination (Condition 9) and landscaping</p>

details (Conditions 10 and 11). Pre-occupation details relating to ecology (Condition 13), building sustainability (Condition 14), parking management (Condition 15), lighting (Condition 16) and deliveries (Condition 17) are also included. Conditions 22-24 also limit the Class B2 floorspace, external storage and internal floorspace on parking availability grounds.

PLANNING STATUS

- Major Development Location
- Adjoins WBC land on western and southern boundaries
- Major Accident Hazard Pipeline Buffer (SGN intermediate and high-pressure pipe to south western corner)
- M4 Air Quality Management Area (Nitrogen Dioxide NO₂)
- Flood zone 1
- Localised 1 in 1000-year surface water flooding
- Public Open Space (opposite site in Danehill)
- Bat roost suitability
- Thames Basin Heath Special Protection Area (7km zone)
- Sand and gravel extraction consultation zone
- Non-classified road
- Adopted highway

RECOMMENDATION

That the committee authorise the GRANT OF PLANNING PERMISSION subject to:

- 1) Prior completion of a legal undertaking to secure (a) an Employment Skills Plan and (b) contributions for a Traffic Regulation Order. If the undertaking is not submitted and agreed within 3 months of the date of this resolution, planning permission will be refused unless the Operational Manager for Development Management in consultation with the Chairman of the Planning Committee agree to a later date**
- 2) The following conditions and informatives:**

Conditions

1) Timescale

The development hereby permitted shall be begun before the expiration of three years from the date of this permission.

Reason: In pursuance of s.91 of the Town and Country Planning Act 1990 (as amended by s.51 of the Planning and Compulsory Purchase Act 2004).

2) Approved details

This permission is in respect of the plans numbered DR-Z-8200-P01 (dated 8 December 2021) and plans 2732-RDJWL-XX-XX-DR-A-0016, 2732-RDJWL-01-ZZ-DR-A-0025, 2732-RDJWL-01-ZZ-DR-A-0030 and 2732-RDJWL-ZZ-ZZ-DR-A-0035, all Rev C1, dated 8 December 2021 and received by the local planning authority on 10 December 2021 and the plan numbered 2732-RDJWL-ZZ-XX-DR-A-0020, Rev C4, dated and received on 28 February 2022. The development shall be carried out

in accordance with the approved details unless other minor variations are agreed in writing after the date of this permission and before implementation with the Local Planning Authority.

Reason: For the avoidance of doubt and to ensure that the development is carried out in accordance with the application form and associated details hereby approved.

3) Construction method statement and management plan

Prior to the commencement of the development hereby permitted, a revised and final Construction Method Statement and Management Plan shall be submitted to and approved in writing by the local planning authority. The approved Statement shall be adhered to throughout the construction period and shall provide for:

- a) The parking of vehicles of site operatives and visitors
- b) Loading and unloading of plant and materials
- c) Storage of plant and materials used in constructing the development
- d) The erection and maintenance of security hoarding including decorative displays and facilities for public viewing, where appropriate
- e) Wheel washing facilities
- f) Measures to control the emission of dust and dirt during construction
- g) A scheme for recycling/disposing of waste resulting from demolition and construction works

Reason: In the interests of highway safety and convenience and neighbour amenities. Relevant policy: Core Strategy policies CP3 and CP6.

4) Access

Prior to the commencement of the development hereby permitted, details of the proposed vehicular access onto Danehill (including visibility splays of 2.4m by 43m) shall be submitted to and approved in writing by the local planning authority. The access shall be formed as so approved, and the visibility splays shall be cleared of any obstruction exceeding 0.6m in height prior to the occupation of the development. The accesses shall be retained in accordance with the approved details and used for no other purpose and the land within the visibility splays shall be maintained clear of any visual obstruction exceeding 0.6 metres in height at all times.

Reason: In the interests of highway safety and convenience in accordance with Core Strategy policies CP3 and CP6.

5) Highway construction details

Prior to the commencement of the development hereby permitted, full details of the construction of roads and footways, including levels, widths, construction materials, depths of construction, surface water drainage and lighting shall be submitted to and approved in writing by the local planning authority. The roads and footways shall be constructed in accordance with the approved details to road base level before the development is occupied and the final wearing course will be provided within 3 months of occupation unless other minor variations are agreed in writing after the date of this permission and before implementation with the local planning authority.

Reason: To ensure that roads and footpaths are constructed to a standard that would be suitable for adoption as publicly maintainable highway, in the interests of providing a functional, accessible and safe development. Relevant policy: Core Strategy policies CP3 and CP6.

6) Electric vehicle charging

Prior to the commencement of the development hereby permitted, details for electric vehicle charging points serving the development shall be submitted to and approved in writing by the local planning authority. The charging points shall be implemented in accordance with such details as may be approved before occupation of the development and be permanently retained in the approved form for the charging of electric vehicles and used for no other purpose.

*Reason: In order to ensure that secure electric vehicle charging facilities are provided so as to encourage the use of sustainable modes of travel.
Relevant policy: Core Strategy policies CP1, CP3 and CP6 and Managing Development Delivery Local Plan policy CC07.*

7) Travel Plan

Prior to the commencement of the development hereby permitted, a Travel Plan shall be submitted to and approved in writing by the local planning authority. The Travel Plan shall include a programme of implementation and proposals to promote alternative forms of transport to and from the site, other than by the private car and provide for periodic review. The Travel Plan shall be fully implemented, maintained and reviewed as so approved.

Reason: To encourage the use of all travel modes. Relevant policy: NPPF Section 9 and Core Strategy policy CP6.

8) Drainage

Prior to the commencement of the development hereby permitted, details of the drainage system shall be submitted to and approved in writing by the local planning authority. The details shall include:

- a) BRE 365 test results demonstrating whether infiltration is achievable
- b) Use of SuDS following the SuDS hierarchy, preferably infiltration
- c) Why other methods of the SuDS hierarchy cannot be implemented
- d) Groundwater monitoring confirming seasonal high groundwater levels in the area.
- e) Drainage strategy plan indicating the location and sizing of SuDS features, with the base of any SuDS features located at least 1m above the seasonal high water table level
- f) Details of management and maintenance of SuDS throughout the lifespan of the development

The hard surfacing hereby permitted shall be constructed from porous materials or provision shall be made to direct water run-off from the hard surface to a permeable or porous area within the curtilage of the development.

The approved scheme shall be implemented prior to the first occupation of the development and shall be maintained in the approved form for the life of the development.

Reason: To prevent increased flood risk from surface water run-off. Relevant policy: NPPF Section 15, Core Strategy policy CP1 and Managing Development Delivery Local Plan policies CC09 and CC10.

9) Phase II Intrusive Investigation

Prior to the commencement of the development hereby permitted, a Phase II Intrusive Investigation shall be submitted to and approved in writing by the local planning authority. The scheme shall include an investigation and assessment to identify the extent of contamination and the measures to be taken to avoid risk when the site is developed. Development shall not commence until the measures approved in the scheme have been implemented.

Reason: To ensure that risks from land contamination to the future users of the land and neighbouring land are minimised, together with those to controlled waters, property and ecological systems, and to ensure that the development can be carried out safely without unacceptable risks to workers, neighbours and other offsite receptors.

10) Landscaping

Prior to the commencement of the development hereby permitted, revised and final details of hard and soft landscape proposals shall be submitted to and approved in writing by the local planning authority. These details shall include revisions to planting plans and schedules, as appropriate, proposed finished floor levels or contours, means of enclosure, car parking layouts, other vehicle and pedestrian access and circulation areas, hard surfacing materials and minor artefacts and structure (e.g. furniture, play equipment, refuse or other storage units, signs, lighting, external services, etc). Soft landscaping details shall include planting plan, specification (including cultivation and other operations associated with plant and grass establishment), schedules of plants, noting species, planting sizes and proposed numbers/densities where appropriate, and implementation timetable.

All hard and soft landscape works shall be carried out in accordance with the approved details prior to the occupation of any part of the development or in accordance with a timetable approved in writing by the local planning authority. Any trees or plants which, within a period of five years after planting, are removed, die or become seriously damaged or defective, shall be replaced in the next planting season with others of species, size and number as originally approved and permanently retained.

Reason: In the interests of visual amenity. Relevant policy: Core Strategy policy CP3 and Managing Development Delivery Local Plan policies CC03 and TB21.

11) Landscape management plan

Prior to the commencement of the development hereby permitted, a landscape management plan, including long term design objectives, management responsibilities, timescales and maintenance schedules for all landscape areas, other than privately owned, domestic gardens, shall be submitted to and approved in writing by the local planning authority. The landscape management plan shall be carried out as approved.

Reason: In order to ensure that provision is made to allow satisfactory maintenance of the landscaping hereby approved. Relevant policy: Core Strategy policy CP3 and Managing Development Delivery Local Plan Policies CC03 and TB21

12) Compliance with Method Statement and Tree Protection Plan

The development hereby permitted and other operations shall be undertaken in complete accordance with Sections 9 and 11 and Appendices 4-6 of the Arboricultural Impact Assessment, Method Statement & Tree Protection Plan (ref: Ref: TH 2954/C, dated 12 November 2021) prepared by Trevor Heaps Arboricultural Consultancy Ltd (hereinafter referred to as the Approved Scheme).

No operations shall commence on site in connection with development hereby approved (including any tree felling, tree pruning, demolition works, soil moving, temporary access construction and or widening or any other operation involving use of motorised vehicles or construction machinery) until the tree protection works required by the Approved Scheme are in place on site.

No excavations for services, storage of materials or machinery, parking of vehicles, deposit or excavation of soil or rubble, lighting of fires or disposal of liquids shall take place within an area designated as being fenced off or otherwise protected in the Approved Scheme.

The fencing or other works which are part of the Approved Scheme shall not be moved or removed, temporarily or otherwise, until all works including external works have been completed and all equipment, machinery and surplus materials removed from the site, unless the prior approval in writing of the local planning authority has first been sought and obtained.

Reason: To secure the protection throughout the time that the development is being carried out of trees shrubs or hedges growing within or adjacent to the site which are of amenity value to the area, and to allow for verification by the local planning authority that the necessary measures are in place before development and other works commence Relevant policy: Core Strategy policy CP3 and Managing Development Delivery Local Plan policies CC03 and TB21

13) Compliance with Ecology Impact Assessment

The development hereby permitted shall be undertaken in complete accordance with Section 5 of the Preliminary Ecological Impact Assessment (ref: 21.0148.0002.F0, dated 30 November 2021, as prepared by Ecosa (hereinafter referred to as the Approved Scheme) and maintained thereafter.

Prior to the first occupation of the development hereby permitted, confirmation of the following shall be submitted to and approved in writing by the local planning authority:

- a) That the boxes, bricks or tiles have been installed, including a plan showing their location and photographs of the boxes, bricks or tiles in situ; and
- b) That permeable kick boards are installed beneath the 1.2m high fence on the western site boundary to allow free movement of mammals.

Reason: To comply with Policy CP7 of the core strategy and Paragraphs 109 and 118 of the NPPF.

14) Carbon minimisation and BREEAM

Prior to the first occupation of the development hereby permitted, certification that the approved development accords with the findings of the BREEAM report and Energy Statement (both prepared by JAW Sustainability and dated 9 December 2021) shall be submitted to and approved by the local planning authority. The approved measures shall remain operational for the lifetime of the development.

*Reason: To ensure developments contribute to sustainable development.
Relevant policy: NPPF Section 14, Core Strategy policy CP1, Managing Development Delivery Local Plan policy CC05 and the Sustainable Design and Construction Supplementary Planning Document.*

15) Parking management strategy

Prior to the first occupation of the development hereby permitted, a Parking Management Strategy shall be submitted to and approved in writing by the local planning authority. The Parking Management Strategy shall include details of the management of all parking spaces, site access through the security gates and the monitoring and the delivery of additional electric vehicle charging spaces when required. The car park is to be operated in accordance with the approved details for the life of the development.

Reason: To ensure satisfactory development in the interests of amenity and highway safety in accordance with Wokingham Borough Core Strategy Policies CP1, CP6, CP13 and CP21 and MDDLp policies CC07 and TB20.

16) External lighting

Prior to the first occupation of the development hereby permitted, details of external lighting shall be submitted to and approved in writing by the local planning authority. The details shall include location, height, type, hours of operation, direction of light sources and intensity of illumination, including lux level predictions beyond the site boundary, for all external lighting strategies including details of lighting for all highways, cycleways, footpaths, public areas, non-residential buildings and residential areas beyond the site boundary. The details shall also identify those areas that support light sensitive biodiversity and the mitigation measures to be included to prevent disturbance. No further external lighting shall be installed without the written approval of the local planning authority.

Reason: In the interests of residential amenity and highway safety and to seek to protect habitat and species of principal importance. Relevant policies: NPPF Section 15, Core Strategy Policies CP1, CP3, CP6, CP7, CP11 and MDD Policies TB21 and TB23.

17) Delivery and Servicing Plan

Prior to the first occupation of the development hereby permitted, a revised and final Delivery and Servicing Plan shall be submitted to and approved in writing by the local planning authority.

Reason: In the interests of highway safety and convenience. Relevant policy: Core Strategy policies CP3 and CP6.

18) Parking and turning space

No part of the building hereby permitted shall be occupied or used until the vehicle parking and turning space has been provided in accordance with the approved plans. The vehicle parking and turning space shall be retained and maintained in accordance with the approved details and the parking space shall remain available for the parking of vehicles at all times and the turning space shall not be used for any other purpose other than vehicle turning.

Reason: To provide adequate off-street vehicle parking and turning space and to allow vehicles to enter and leave the site in a forward gear in the interests of road safety and convenience and providing a functional, accessible and safe development and in the interests of amenity. Relevant policy: Core Strategy policies CP3 & CP6 and Managing Development Delivery Local Plan policy CC07.

19) Cycle parking

No part of the building hereby permitted shall be occupied or used until secure and covered parking for cycles has been provided in accordance with the approved drawings. The cycle storage shall be permanently so retained for the parking of bicycles and used for no other purpose.

Reason: In order to ensure that secure weather-proof bicycle parking facilities are provided so as to encourage the use of sustainable modes of travel. Relevant policy: NPPF Section 9 and Core Strategy policies CP1, CP3 and CP6 and Managing Development Delivery Local Plan policy CC07.

20) External materials

The materials to be used in the construction of the external surfaces of the development hereby permitted shall be in accordance with the approved details unless other minor variations are agreed in writing after the date of this permission and before implementation with the Local Planning Authority.

Reason: To ensure that the external appearance of the building is satisfactory. Relevant policy: Core Strategy policies CP1 and CP3.

21) Retention of trees and shrubs

No trees, shrubs or hedges within the site which are shown as being retained on the approved plans shall be felled, uprooted wilfully damaged or destroyed, cut back in any way or removed without previous written consent of the local planning authority; any trees, shrubs or hedges removed without consent or dying or being severely damaged or becoming seriously diseased within 5 years from the completion of the development hereby permitted shall be replaced with trees, shrubs or hedge plants of similar size and species unless the local planning authority gives written consent to any variation.

Reason: To secure the protection throughout the time that development is being carried out, of trees, shrubs and hedges growing within the site which are of amenity value to the area. Relevant policy: Core Strategy policy CP3 and Managing Development Delivery Local Plan policies CC03 and TB21.

22) Use limitations

Notwithstanding the provisions of the Town and Country Planning (General Permitted Development) (England) Order 2015 (or any Order revoking and re-enacting that Order with or without modification), the Class B2 use within the development hereby approved is limited to a maximum of 55% of the floorspace, unless otherwise agreed with the local planning authority.

Reason: To encourage a mix of uses and to ensure adequate parking to serve the site. Relevant policy: NPPF Section 9 and Core Strategy Policy CP6.

23) External storage

No materials, plant, machinery, containers or equipment shall be stored on the site outside of the approved building except for the approved waste storage area shown on the approved drawings and areas for plant equipment shown on the approved drawings, including DR-Z-8200-P01 dated 8 December 2021.

Reason: To safeguard the amenities of the area. Relevant policy: Core Strategy policies CP1 and CP3.

24) Additional floorspace

No additional floorspace beyond that hereby approved, including mezzanine floors, shall be constructed within the buildings hereby approved without prior written permission of the local planning authority.

Reason: To prevent an over-development of the site and to ensure adequate parking. Relevant policy: Core Strategy policies CP1, CP3 and CP6 and Managing Development Delivery Local Plan policy CC07.

25) Amplification

No sound amplifying equipment shall be installed within the exterior of the development hereby permitted.

Reason: To safeguard the residential amenities of nearby residents from noise and disturbance. Relevant policy: NPPF Section 15, Core Strategy policies CP1 and CP3 and Managing Development Delivery Local Plan policy CC06.

Informatives

1) Section 106 agreement

This permission should be read in conjunction with the legal agreement under section 106 of the Town and Country Planning Act dated [TBC], the obligations in which relate to this development.

2) Pre commencement requirements

The applicant is reminded that this approval is granted subject to conditions which must be complied with prior to the development starting on site. Commencement of the development without complying with the pre-commencement requirements may be outside the terms of this permission and liable to enforcement action. The information required should be formally submitted to the Council for consideration with the relevant fee. Once the details have been approved in writing the development should be carried out only in accordance with those details. If this is not clear please contact the case officer to discuss.

3) Demolition notice

The applicant is reminded that a Demolition Notice may be required to be served on the Council in accordance with current Building Regulations and it is recommended that the Building Control Section be contacted for further advice.

4) Changes to the approved plans

The applicant is reminded that should there be any change from the approved drawings during the build of the development this may require a fresh planning application if the changes differ materially from the approved details. Non-material changes may be formalised by way of an application under s.96A Town and Country Planning Act 1990.

5) Advertisement consent

This permission does not convey or imply any approval or consent that may be required for the display of advertisements on the site for which a separate Advertisement Consent application may be required. You should be aware that the display of advertisements without the necessary consent is a criminal offence liable to criminal prosecution proceedings through the courts.

6) Thames Water easement

Running west to east through the rear of the site are easement and wayleaves. Thames Water properties will seek assurances that it will not be affected by the

proposed development. The applicant should contact Thames Water to discuss their proposed development in more detail. All enquiries from developers in relation to proposed developments should be made to Thames Waters Developer Services team at Thames Water Developer Services, Reading Mail Room, Rose Kiln Court, Rose Kiln Lane, Reading RG2 0BY (0800 009 3921 or developer.services@thameswater.co.uk)

7) Groundwater Risk Management Permit

A Groundwater Risk Management Permit from Thames Water will be required for discharging groundwater into a public sewer. Any discharge made without a permit is deemed illegal and may result in prosecution under the provisions of the Water Industry Act 1991. Thames Water expect the developer to demonstrate what measures he will undertake to minimise groundwater discharges into the public sewer. Permit enquiries should be directed to Thames Water's Risk Management Team at 020 3577 9483 or trade.effluent@thameswater.co.uk . Application forms should be completed on line via www.thameswater.co.uk. Please refer to the Wholesale; Business customers; Groundwater discharges section.

8) Travel plan

The requisite Travel plan would need to comply with the latest national and local guidance:

- a) NPPF Section 9 (Sustainable Transport)
- b) The Essential Guide to Travel Planning (DfT, March 2008)
- c) Delivering Travel Plans Through the Planning Process (DfT, April 2009)
- d) A Guide on Travel Plans for Developers (DfT)
- e) Making Residential Travel Plans Work (DfT, June 2007) All accessible at: <http://www.dft.gov.uk/pgr/sustainable/travelplans/>

Also:

WBC Transport Plan 3 and Active Travel Plan 2011 – 2026

WBC Workplace Travel Plan Guidance and Residential Travel Plan Guidance

Documents, covering workplace travel plans and residential travel plans provide local guidance and are available on the Borough's website.

9) Mud on the road

Adequate precautions shall be taken during the construction period to prevent the deposit of mud and similar debris on adjacent highways. For further information contact the Highway Authority on tel.: 0118 9746000.

10) Discussion

The Local Planning Authority has acted positively and proactively in determining this application by assessing the proposal against all material considerations, including planning policies and any representations that may have been received. This planning application has been the subject of positive and proactive discussions with the applicant in terms of a full pre-application process and addressing concerns relating to highway safety. The decision to grant planning permission in accordance

with the presumption in favour of sustainable development as set out in the NPPF is considered to be a positive outcome of these discussions.

11) Works affecting the public highway

Any works/events carried out by or on behalf of the developer affecting either a public highway or a prospectively maintainable highway (as defined under s.87 New Roads and Street Works Act 1991 (NRSWA)), shall be co-ordinated and licensed as required under NRSWA and the Traffic Management Act 2004 in order to minimise disruption to both pedestrian and vehicular users of the highway.

Any such works or events, and particularly those involving the connection of any utility to the site must be co-ordinated by the developer in liaison with the Borough's Street Works team (0118 974 6302). This must take place AT LEAST three months in advance of the intended works to ensure effective co-ordination with other works so as to minimise disruption.

12) Service strip

The applicant's attention is drawn to the presence of a c.2m wide service strip running adjacent to the surfaced roadway. The service strip forms part of the adopted public highway. It is an offence for any works to be undertaken within this service strip without express permission in writing from the highway authority. The Head of Technical Services at the Council Offices, Shute End, Wokingham (0118 9746000) must be contacted for the approval of any such works.

PLANNING HISTORY		
App No.	Description	Decision/Date
16170	Industrial estate (outline)	Approved 2 March 1982
18652	Retail, offices, parking and open space (outline)	Refused 13 January 1983
22395	Industrial building for design testing and assembly of electronic equipment with car parking (not implemented)	Approved 18 October 1984
26694	Industrial building for mixed use research, development, light industrial, offices and storage, with car parking	Approved 17 December 1986
30051	Change of use from mixed use to Class B1 industrial	Approved 13 April 1988
34823	Bike shelter, store and car parking	Approved 8 January 1990
F/2001/4181	Change of use from light industrial to offices	Approved 12 September 2001
CLE/2011/1047	Use of site for Class B1 office use	Approved 16 June 2011
OFF/2015/0882	Conversion of offices to 25 residential flats (prior approval)	Withdrawn 14 May 2015
162247	Flat building comprising 126 units, car parking and landscaping	Refused 23 December 2016
W/17/3174392	Appeal against refusal of 162247	Dismissed 1 December 2017

173675	Flat building comprising 76 units, car parking and landscaping	Refused 15 May 2018
W/18/3205448	Appeal against refusal of 173675	Upheld 7 June 2019
190326	Conversion of offices to 26 residential flats (prior approval)	Approved 14 March 2019

SUMMARY INFORMATION		
	Existing	Proposed
Site Area	0.81 hectares	
Land use	Offices	Mixed industrial, storage and offices
Units	2 x two-storey office buildings	One storey warehouse with mezzanine
Floorspace	3,048m ²	3,692m ²
Height	6.2m and 9.0m	13.0m
Employment	0 (vacant site)	52 full time
Parking spaces	116 spaces	48 spaces + 5 motorcycle spaces + 5 HGV loading bays
Cycle spaces	0 spaces	26 spaces

CONSULTATION RESPONSES	
Internal	
Drainage	No objection, subject to Condition 8 requiring final drainage details and confirmation of capacity (see Thames Water comment below).
Highways	No objection, subject to finalised design details relating to access onto Danehill, electric vehicle charging, construction, parking management and deliveries, as specified in Conditions 4, 5, 15 and 17 respectively.
Landscape and Trees	No objection, subject to a landscape management plan and revised landscaping details in Conditions 10 and 11. Tree protection is acceptable, as conditioned in Condition 12.
Growth and Delivery	Whilst the loss of residential housing is unfortunate, there is no in-principle objection to the employment scheme.
Economic Growth	No objection, subject to delivery of an Employment Skills Plan, as secured by s106 agreement.
Property Services	No comments received.
Environmental Health	No comments received.
Ecology	No comments received.
Waste	No comments received.
External	
Thames Water	No objection in relation to wastewater and foul water capacity. Sequential tests should be used in dealing with on-site drainage. Easements run through the site. Informatives 6 and 7 are applicable.
Southern Gas	No objection.
SEE Power	No objection.
Health and Safety Executive	No objection.
Fire and Rescue	No objection.
Crime Prevention	No comments received.

REPRESENTATIONS	
Earley Town Council	No objection.
Ward Members	No comments received.
Neighbours	<p>One submission was received from Unit 1, Cutbush Court, Lower Earley RG6 4UW. It noted support of the application as an improvement from the current eyesore. Double yellow lines are suggested to assist in visibility and access on Danehill.</p> <p>The access will be remodelled and the double yellow lines extended to improve turning into and from the site. Beyond this, it is not considered that the double yellow lines be extended given that it would compromise the availability of on-street car parking, which is already very limited during working hours. The levels of parking and traffic from this development does not warrant such a restriction along the whole length of Danehill.</p>

APPLICANTS POINTS
<p>Nodia Properties RR Ltd and Aydevan Developers Ltd are proposing a high-quality employment development on existing brownfield land, which will assist with the regeneration of a site that has a long history of health and safety issues. The scheme will provide a number of jobs and will meet an unmet need for industrial and logistics in Wokingham. The development will also assist Wokingham in attracting a number of industrial and logistics businesses in the future.</p> <p>Having regard to the three dimensions of delivering sustainable development, the application proposal will perform:</p> <ul style="list-style-type: none"> • an economic role – developing and supporting the applicant and their investment in industrial and logistics both in Wokingham and the UK; • a social role – providing a high quality built and natural environment, including the enhancement of the landscape grounds and creating a number of new jobs; and • an environmental role – contributing to and protecting the natural environment by providing a net gain in biodiversity, minimising waste and pollution and mitigating and adapting to climate change including moving to a low carbon economy. <p>Overall, the development proposal complies with national and local planning policies and achieves sustainable development in line with NPPF Paragraph 11. For the reasons set out in this Statement and in the documents supporting this planning application, we ask the Council to support the applicants project programme and to grant planning permission without delay.</p>

PLANNING POLICY		
National Planning Policy Framework	Section 6	Building a strong, competitive economy
	Section 9	Promoting sustainable transport
	Section 12	Achieving well-designed places
	Section 15	Conserving and enhancing the natural environment
	CP1	Sustainable Development
	CP2	Inclusive Communities

Core Strategy 2010	CP3	General Principles for Development
	CP4	Infrastructure Requirements
	CP6	Managing Travel Demand
	CP7	Biodiversity
	CP8	Thames Basin Heaths Special Protection Area
	CP9	Scale and Location of Development Proposals
	CP15	Employment Development
	CP17	Housing Delivery
Adopted Managing Development Delivery Local Plan 2014	CC01	Presumption in Favour of Sustainable Development
	CC03	Green Infrastructure, Trees and Landscaping
	CC04	Sustainable Design and Construction
	CC05	Renewable Energy and Decentralised Energy Networks
	CC06	Noise
	CC07	Parking
	CC09	Development and Flood Risk (from all sources)
	CC10	Sustainable Drainage
	TB11	Core Employment Areas
	TB12	Employment Skills Plan
	TB20	Service Arrangements and Deliveries for Employment and Retail Use
	TB21	Landscape Character
	TB23	Biodiversity and Development
	TB24	Designated Heritage Assets
SPDs	BDG	Borough Design Guide
	SDC SPD	Sustainable Design and Construction Supplementary Planning Document

PLANNING ISSUES

Description of Development

1. The proposal involves the following:

- Demolition of the two existing buildings
- Construction of a single industrial and logistics building with Class B use and a floor area of 3232sqm with an additional 528sqm of mezzanine office floorspace
- Car parking for 48 cars (including eight EV parking spaces and four disabled spaces)
- Five loading bays at the rear of the site
- Modified entrance from Danehill and new access along the south eastern boundary
- Cycle storage and bin storage
- Associated landscaping (including removal and replacement of trees) and site works
- 24-hour operation

Description of Site

2. Auto Trader House and Hartman House are two separate two storey office buildings located to the southern side of Danehill in the Cutbush Core Employment Area. The

buildings have remained vacant since 2015 and the site is well boarded and shows clear signs of disuse. The collective footprint is approximately 1610sqm. Other warehouses and office buildings (some of which have been converted to residential units) predominate to the east with residential housing in the north and east and Lower Earley Way and the M4 motorway corridor with countryside beyond to the south. A dense coverage of trees adjoin the Council owned land to the west and along the southern boundary.

Principle of Development

3. The site is located within the built-up area of Earley in an area identified as a major development location. Development in this location is acceptable in principle providing, it is of an appropriate scale and character and accords with the relevant policies set out in the NPPF and Development Plan, including with respect to employment floorspace, loss of residential dwellings and use of brownfield land.

Employment use

4. Policy CP15 of the Core Strategy and Policy TB11 of the MDD Local Plan permit business, industry or warehousing (employment development) in Core Employment Areas (CEAs) and refurbishment or redevelopment of existing buildings in employment use outside of CEAs. Employment development inside development limits but outside the identified CEAs are assessed against policy CP15 of the Core Strategy. There should not be a net loss of Class B floorspace.
5. The Danehill area immediately to the east accommodates a number of business uses (with some office to residential conversions under Class O of Part 3 of the GPDO) but the site is not identified as a CEA. However, given the previous employment (office) use within the site and the wider setting of the Cutbush Industrial Estate as well as its proximity to the M4 motorway, there is no objection is raised to the redevelopment of the site as an industrial and/or logistical warehouse building broadly within the scope of use class B2, B8 and E(g). There is also a net increase of 644sqm floorspace which is supportive of policy.

Loss of residential

6. Policy CP3(i) of the Core Strategy requires that proposals do not lead to a net loss of dwellings and other residential accommodation or land. Additionally, whilst it carries little weight, Policy H4 of the Draft Local Plan states that *“Given the pressures to build more homes under national planning policy, it is important that existing housing and land that has permission for new housing is protected from being lost to other use, unless there is clear justification for such a loss.”*
7. The site benefits from planning permission (173675, approved at appeal APP/X0360/W/18/3205448) for the redevelopment of the site for 76 residential dwellings across three separate buildings and with a unit mix of 53 x 1-bed, 21 x 2-bed and two x 3-bed dwellings. The application will expire on 7 June 2022 and the statements in the Planning Statement are clear that there is no intention to deliver on this permission. A prior approval for 26 dwellings was issued 14 March 2019 but the provisions of the GPDO require that the development be completed by 14 March 2022. This is clearly not occurring.

8. The applicant's reasoning for not implementing the residential permission relates to viability, including that the employment scheme is more appropriate in this location with links to the M4.
9. Whilst the proposal would remove 76 dwellings from the deliverable Housing Land Supply, it is recognised that the existing use is for offices and that the site is not a proposed housing allocation within the emerging Local Plan. It is also accepted that despite the lack of concrete justification, there is currently no appetite for a residential scheme to be delivered.
10. There is no policy objection for the employment use given the previous employment use of the site and within the wider Cutbush Industrial Estate. The proposal would provide additional industrial/warehousing floorspace, for which there is an identified need within the Borough.

Brownfield land

11. Paragraph 120(c) of the NPPF requires the Council to give substantial weight to the value of using suitable brownfield land within settlements for homes and other identified needs, and support appropriate opportunities to remediate despoiled, degraded, derelict, contaminated or unstable land.
12. The site currently lies derelict and the two buildings are in significant disrepair with debris and evidence of squatting and vandalism in recent years. The reuse of the land has already been established with an extant permission, granted at appeal, for the erection of 76 residential flats. The subject application involves an alternate but acceptable proposal to regenerate the site, with soft landscaping and tree planting.

Site sustainability

13. Policies CP6 and CP9 of the Core Strategy permit development where it is based on sustainable credentials in terms of access to local facilities and services and the promotion of sustainable transport. The site offers good links with the M4 and is supportive of a distribution or logistical hub. It is within walking distance of public transport and services and facilities and is in a highly sustainable location. Because of the 24-hour use, there will remain a relatively high dependence and there is some evidence of poor parking management on Danehill.
14. A draft Travel Plan (Velocity Transport Planning Ltd, 7 December 2021) was submitted in support of sustainability measures. It includes details of the coordinator, MyJourney, efforts to take advantage of the good local bus services, measures to assist in reducing road traffic and congestion and the health benefits of walking and cycling and an initial action plan. A final Travel Plan will require details of the surveys after occupation, details of showers and lockers for cyclists and a forward budget. See Condition 7.

Character of the Area

Landscape and visual impact assessment

15. The site lies on the settlement edge with the woodland to the south and west designated as countryside. Ordinarily a transition of built form would be required but it is also recognised that Lower Earley Way and the M4 are dominant features in the landscape.
16. A Landscape and Visual Impact Assessment (Harper Landscape Architecture, December 2021) was submitted in support of the application. This has been reviewed by the Council's Trees and Landscape Officer.
17. The landscape character is assessed as low-medium slightly higher than the report, as a result of the existing landscape buffer to the west and surroundings areas of woodland and open space. Within the site, it is poor. The magnitude of impact from construction is agreed as low adverse at construction and low or beneficial post construction. The visual impact is noted in Section 8 as:

“The significant strengthened screening planting located along the Site’s existing boundaries and the City Limits Office building, would visually contain the development so that it continues to be located in a visually discreet position. All the public views of the development would be near distance and predominantly from within the Cutbush Industrial Estate, although there would be glimpsed views from the pavement following the busy Lower Earley Way and towards the entrance area from Cutbush Lane. All the public views would be partial, obscured, contained or framed.”



18. The Council's Trees Officer agrees that the site is 'visually contained' by the topography and adjacent planting and built form within the commercial setting. The proposed landscaping screens and uses a good palette of native and ornamental plants. However, there is heavy reliance on the assumed perpetuity of the woodland screening directly to the west of the site.

Built form and siting

19. Policy CP3 of the Core Strategy states that development must be appropriate in terms of its scale, mass, layout, built form, height and character of the area and must be of high-quality design. This Borough Design Guide SPD outlines similar guidelines:
- NR1 states development should respond to key characteristics and features
 - NR2 states that proposals should improve the area
 - NR3 requires a positive arrival impression for all modes of transport
 - NR4 requires that buildings address the street
 - NR9 states that large floorplates must be designed to minimise potential impact upon the character of the area
 - NR10 states that car parking is to be unobtrusive and landscaped
 - NR11 requires that servicing be screened
 - P2 aims to ensure that parking is provided in a manner that is compatible with the local character
20. The existing footprint of Hartman House and Auto Trader House is 1610sqm which fits comfortably on the site. The subject proposal has a footprint of 3200sqm which is 100% larger than existing and 40% of the site. The floorplate of the proposed building is substantial on its own. Another 3390sqm or 43% is made up of hard surfacing for the car park, turning, access road and pedestrian connectivity. About 17% would be made up of existing and proposed soft landscaping and trees.
21. Nonetheless, there is no in-principle objection to the large floorplate or the overall bulk and scale of the building. The servicing area is also well screened to the rear and the front elevation provides an inviting and clear presentation to Danehill, including the use of glazing.



Height

22. NR5 of the Borough Design Guide SPD notes that height, bulk, and massing should respond to the local context and the prevailing heights in the area. NR7 requires that roofs be designed to the context of the area, including visibility.



23. The height measures 13m with a parapet concealing the 11.2m eaves height and 12.8m ridge height. This is of significant height and is reinforced by the large footprint. However, it is sited 40m from the street frontage and partially concealed by City Limits building on the site to the north and this reduces its dominance. In the context of existing landscaping, which extends to 16m in height, the building will remain relatively well screened from wider views in the vicinity of Lower Earley Way to the south and the dense woodland to the west largely removes any impression of the building.

Parking

24. P2 of the Borough Design Guide SPD requires that parking is compatible with the local character and NR10 requires that it is to be unobtrusive and landscaped. There is a modest public car park to the front of the site where it is sufficiently complemented by existing and proposed landscaping. A larger car park and loading area dominates the rear of the site but there is adequate boundary planting in areas and it is well screened by existing woodland across the boundary.

Materials

25. NR8 of the Borough Design Guide requires high quality and simple materials and components. The palette includes silver and dark grey cladding to the walls and roof (with rooflights) and extensive glazing to the front elevation. These are acceptable materials in the locality and in contrast to the dense woodland to the rear. Materials form Condition 20.

External lighting

26. Paragraph 185(c) of the NPPF aims to limit the impact of light pollution from artificial light on local amenity, intrinsically dark landscapes, and nature conservation. To protect against undue glare from night time use and for wildlife in surrounding woodland, a lighting plan forms Condition 16.

Boundary Treatments

27. NR12 of the Borough Design Guide requires consideration of boundary treatments. There is little change to the boundary fencing. Some security fencing is shown around the service yard and to the side access road. It consists of steel materials

with views through and to a height of 2m. This is appropriate in the context of the site.

Accessibility (incorporating The Public Sector Equality Duty (Equality Act 2010))

28. The Council is required to have due regard to its obligations under the Equality Act 2010, include age, disability, gender, gender reassignment, marriage and civil partnership, pregnancy and maternity, race, religion, or belief. Policy CP2 of the Core Strategy also seeks to ensure that new development contributes to the provision of sustainable and inclusive communities.
29. The development includes lift access, accessible toilet and shower facilities to the ground floor, level access to the building, an open plan with good circulation and four disabled car spaces (8% of total provision) at the front of the building. On this basis, no concerns are raised.

Neighbour Amenity

30. Policy CP3 of the Core Strategy aims to protect neighbouring amenity and Policy CC06 and Appendix 1 of the MDD Local Plan requires that development protect noise sensitive receptors from noise impact.
31. The siting and scale of the building and its site context within Cutbush Industrial Area as well as its relationship with its nearest residential neighbours is such that there is no adverse outcome in terms of dominance, loss of visual privacy or loss of light. There is 7.5m separation to City Limits to the north and 11.8m to the property to the south west, both of which are considered sufficient for building separation. There are no foreseeable overlooking concerns given the separation distances and intended use.
32. The proposal seeks 24-hour operation to support its intended logistics role on the edge of the M4 and an Acoustic Assessment (Auricl Acoustic Consulting, 30 November 2021) was submitted in support of the application.
33. The assessment considered noise sources associated with delivery vehicles, internal activities and plant and included the findings of background noise level on the north western boundary (the boundary closest to the established residential area and furthest from the M4). The nearest residential receiver is about 60m to the north. It is based on the assessment of ten day and two-night HGV movements to and from the site.
34. With high levels of traffic noise from Lower Earley Way and the M4, the background noise level was 52dB in the daytime and 43dB in the night-time. The noise levels for the operation for plant can be mitigated and conditioned without any constraints. The internal operations of the facility are well below the background noise level (28dB during the day and 19dB during the night). The vehicle movements to and from the site are slightly higher but remain below the background noise level (10dB in the day and 2db in the evening) though this does not account for movements within the surrounding road network. Cumulatively, levels remain below the background noise level and the impact is low.

35. A review of other buildings at Cutbush has not found any permissions with restricted hours of use but this could be because of the historic nature of the original development of the wider site and the relatively low intensity of the uses taking place on the site that have not necessitated any restrictive conditions. With favourable conclusions in the acoustic assessment, there is no reason to oppose 24-hour operation of the facility.
36. Plant areas have not been specified but it appears likely that any such plant would be located at the rear. There are no in-principle objections given the levels of separation to neighbouring areas and when accounting for background noise levels.
37. There is sufficient separation to neighbouring buildings and tree coverage to ensure that there is no unreasonable spillage of light to neighbouring properties.

Access and Movement

Parking

38. Policy CC07 and Appendix 2 of the MDD Local Plan stipulates minimum off street parking standards, including provision for charging facilities. For an employment use, the car parking requirement varies between one space per 25sqm up to one space per 200sqm so the overall parking requirement is largely dependent upon the end use.
39. The proposed development involves a flexible employment use of 3,692sqm within a single unit comprising a mix of class E(g)), B2 and/or B8 uses with mezzanine office space. 48 car spaces are proposed, which is the equivalent of 1 space/79sqm. Whilst there is a large amount of storage and distribution floorspace, there would on average, need to be at least 53 spaces.
40. A Transport Statement (Velocity Transport Planning Ltd, 7 December 2021) was submitted in support of the planning application. TRICS data from acceptable sites has been used to assess the parking utilisation of this land use. These results illustrated that between 11am and 1pm, there would only be one space left (98% capacity) which may lead to overspill parking. The Employment Density Guide 3rd edition was then used to determine the number of workers, resulting in a range of employees of between 103 and 50 employees. The 2011 Census Journey to Work data for this area was used which indicated that 67% of workers travelled to work by car. The proposed trips have been assessed and would not have an adverse impact on the highway network.
41. The end use distribution is not yet confirmed and the applicant has advised that if the user of the site proposed more B2 use then there is potential for converting the loading bays to parking areas. Due to the location of the site with sustainable transport options as well as the travel plan, the level of parking is acceptable subject to a planning condition to limit the use as B2 to a percentage of the total site (55%). The Council's Highways Officer accepts that B2 use would require less loading space and this area can be converted to further parking.
42. There will be four disabled bays which is in line with standards. They are located appropriately at the front entrance in a publicly accessible area. Five motorcycle

(PTW) spaces are proposed at the rear, which exceeds the standards. Each space will be 5m x 2.5m in dimension in accordance with the Council's requirements.

43. Cycle parking comprises 26 parking spaces (18 long term at the rear and 8 short term at the front of the site) in two separate covered and secured sheds at the front and rear of the site. This exceeds the minimum standard by eight spaces and is acceptable. Details are conditioned in Condition 19.
44. Eight active electric vehicle charging spaces are proposed – four to the front and four to the rear. This exceeds the minimum requirements of 5% active and 5% passive spaces. Details will be secured by Condition 6.

Access

45. A Road Safety Audit Stage 1 was submitted which was reviewed and found to be acceptable. Remodelling of the main entrance onto Danehill is required to ensure improved access onto the site. Double yellow lines are required on Danehill to aid with turning. A contribution in a s106 to fund the TRO process and deliver the yellow lines is required.
46. Swept paths show that there is suitable access for cars (into the car spaces), a 17.5m articulated vehicle and 10m rigid truck (to the loading bays) and a refuse vehicle.
47. Separate pedestrian access via a dedicated footpath provides access from Danehill to the main entrance and to the rear of the site, which is a positive aspect. Lighting is required in Condition 16.

Deliveries

48. A framework Delivery and Servicing Plan (Velocity Transport Planning Ltd, 7 December 2021) was submitted in support of the planning application. Access will ultimately be via Lower Earley Way. Five loading bays at the rear of the site are future proofed for longer semi-trailers. TRICS calculations anticipate 31 two-way car and LGV movements between 8-9am and 22 two-way movements between 5-6pm. HGV movements are estimated at 7 two-way movements between 8-9pm.
49. The details are to the satisfaction of the Council's Highways Officer although the final document would need to be amended to ensure deliveries are outside the peak hours wherever possible. This is noted in Condition 17.

Construction

50. A framework Construction Logistics Plan (Velocity Transport Planning Ltd, 7 December 2021) was submitted in support of the planning application. It is likely that there will be up to 12 HGV movements are likely with routing via Lower Earley Way from the west but is subject to final agreement, The final document needs to include a compound plan, cycle and car parking, wheel washing and street sweeping, use of banksmen to manage deliveries and construction vehicle movement. This is noted in Condition 3.

Landscaping and Trees

51. Policy CC03 of the MDD Local Plan aims to protect green infrastructure networks, retain existing trees, and establish appropriate landscaping and Policy TB21 requires consideration of the landscape character.
52. There are no protected trees on the site, there is good woodland coverage to the west and south of the site, providing a visual and audible buffer between the site and Lower Earley Way and the M4.
53. An Arboricultural Impact Assessment and Method Statement (Trevor Heaps Consulting, 12 November 2021) was submitted with the application. Four Category B trees and three Category C trees and two Category C shrubs are proposed for removal, either to accommodate the remodelled entrance to the site or within the footprint of the proposed building. None of the trees are higher than 8m in height and six of the trees lie behind the City Limits building and have limited contribution to the streetscene. The setback to the north-west side allows for the retention of Trees T18 and T19 although their canopies are against the building. The layout of the car park also allows for the retention of Trees T21, 22 and 23 in the south western corner. The retention of the additional trees is welcomed by the Council's Trees Officer but being close to hard landscape features such as within the car park, an amelioration strategy to improve their growing environment is required in a Landscape Management Plan in Condition 11.
54. A Landscape Plan was included in the documentation with a good scheme mixing native and some ornamental species that will encourage biodiversity. Several additional opportunities are identified, including widening the planting bed south eastern boundary to Cutbush Court, thereby allowing further tree and hedge/shrub screening to this otherwise open boundary and to the north of the site around the City Limits building (as noted in the LVIA). To the eastern side of the access road, amendments to the scheme to provide a 1.0m wide planting buffer along the boundary provides some improvement and strengthening of the landscape buffer between the site and Cutbush Court to the east. Revised and finalised details form Condition 10.
55. A staff amenity space is provided to the north side of the building. It will receive some morning sun but will be in shade for most of the day. This is likely to lead to underutilisation as an outdoor space for staff. Nonetheless, it is not a strict departure from any policies and guidelines and whilst unfortunate, it does not warrant refusal of the application.

Ecology

56. Policy TB23 of the MDD Local Plan requires the incorporation of new biodiversity features, buffers between habitats and species of importance and integration with the wider green infrastructure network.
57. A Preliminary Ecological Impact Assessment (Ecosa, 30 November 2021) was submitted in support of the application. It included a desktop and field survey. The site comprises two vacant office buildings partially located within habitat where bat roosts have previously been found. The site is bordered by a small plantation woodland to the north and west.

58. Because of the dilapidated state of the buildings (no roof, broken windows) and the extensive hardstanding and poor quality of the site conditions, including vegetation, there is low suitability for roosting bats and suitability for foraging and commuting bats, hazel dormouse, breeding birds, reptiles and European hedgehog. The woodland to the north and west of the site provides better quality habitat and requires consideration in the design of the scheme.
59. Mitigation and compensation measures include the provision of native planting, preparation of sensitive timing of works, hooded luminaires to external lighting (where unavoidable) and working methods and provision of nesting boxes. Vegetation clearance works should be undertaken outside the main breeding bird season of March to August and caution is required when removing grassland to protect hedgehogs and reptiles. Enhancement recommendations include additional roosting and nesting features for bats in the form of two bat boxes and five bird boxes. Additionally, the Council has sought new or retained permeable kick boards beneath the 1.2m high fence on the western site boundary to allow free movement of mammals. Mitigation and enhancement details form Condition 13.

Thames Basin Heaths Special Protection Area

60. The subject property is located within 7km of the Thames Basin Heaths Special Protection Area but the scope of the works is minor whereby there will be no foreseeable impact upon the SPA. The proposal is therefore acceptable in terms of Policy CP8 of the Core Strategy.

Building Sustainability

61. Policy CC04 of the MDD Local Plan and the Sustainable Design and Construction SPD require sustainable design and conservation. In addition, Policy CC05 of the MDD Local Plan encourages renewable energy and decentralised energy networks, with encouragement of decentralised energy systems and a minimum 10% reduction in carbon emissions.
62. A BREEAM report (JAW Sustainability, 9 December 2021) was submitted in support of the application. The BREEAM considerations include management, health and wellbeing, energy, transport, water, materials, waste, land Use and ecology, pollution and innovation. The report suggests that an excellent rating (71.09%) can be achieved, which is a positive aspect to the development. BREEAM compliance is outlined in Condition 14.
63. An Energy and Sustainability Statement (JAW Sustainability, 9 December 2021) was also submitted with the application. The development incorporates passive design measures (efficient building fabric, insulated walls and high efficiency glazing), energy efficient equipment (efficient VRF heating system in the office and reception areas) and renewable energy (PV panels, air source heat pumps). Overall, the development demonstrates a 44% reduction in CO2 emissions over the building regulations notional building and a 55.5% CO2 savings, well in excess of the 10% standard in Policy CC05. Finalised outcomes are required as part of Condition 14.

Employment Skills

64. Policy TB12 of the MDD Local Plan requires an employment skills plan (ESP) with a supporting method statement for major development such as the subject application. It is intended to provide opportunities for training, apprenticeship, or other vocational initiatives to develop local employability skills required by developers, contractors, or end users of the proposal.
65. Based on the total floorspace, the employment skills plan would generate a need for five community skills support positions (eg work experience or CSCS training courses), two apprenticeships and two jobs. If for any reason, the obligation is not delivered, a contribution in lieu totalling £15,000 is required. This is based on the cost of the Council supporting the employment outcomes of the plan.
66. Measures to secure this arrangement form part of a section 106 legal agreement. The applicants/owners are bound by the planning obligation and are required to provide an estimated start on site date and a build duration.

Crime Prevention

67. Paragraphs 92 and 130 of the NPPF are supportive of designs that are safe, inclusive and accessible and which promote health and well-being, with a high standard of amenity for existing and future users and where crime and disorder, and the fear of crime, do not undermine the quality of life or community cohesion and resilience.
68. The Crime Prevention Officer at Thames Valley Police have not responded to the consultation. Broadly, there are no in-principle objections. The main entrance is clear, legible and inviting, the service area is securely fenced and cycle storage is at the rear where it is securely located and sited where there is good visibility and surveillance. Details of access through the secure gates form part of the Delivery Logistics Plan in Condition 15.

Waste Storage

69. A refuse and recycling area is shown at the rear of the building. It measures 32sqm (10m x 3.2m), is open air and consists of timber boarding. It is acceptable in terms of compatibility within the site and ease of collection given the circulation space at the rear of the site. A swept path shows that there is suitable access and turning within the site for a refuse vehicle.

Flooding and Drainage

70. Section 15 of the NPPF and Policy CC09 of the MDD Local Plan requires consideration of flood risk from historic flooding and Policy CC10 of the MDD Local Plan requires sustainable drainage methods and the minimisation of surface water flow.
71. A Flood Risk Assessment, SuDS and Drainage Strategy (Idom, 2 December 2021) was submitted in support of the application. The site is within flood zone 1 but there is a low risk of surficial flooding which, the FRA suggests, would imply existing problems with the highway drainage network.

72. There is no significant change to the extent of impermeable surfacing across the site. The Council's Drainage Officer has reviewed the scheme and raises no objection. Q100 for the existing site is 173.4L/s but discharge from the development will be limited at Q1 (59l/s) via a hydro-brake. Attenuation is proposed to cater for the 1 in 100-year flood event with a 40% allowance for climate change, with a storage volume of 122.5cubic metres. However, the calculations indicate that 124.5cubic metres is required. To address this shortfall, Condition 8 requires further details.
73. Both foul and surface water discharge are proposed into Thames Water systems to the north of the site.

Air Quality

74. Paragraphs 174, 185 and 186 of the NPPF seek to ensure that new development is appropriate for its location taking into account the likely effects of pollution on health, living conditions and the natural environment. This is reinforced in Policy CP1 of the Core Strategy.
75. The southern part of the site is within the M4 Air Quality Management Area, which would also be affected by traffic on Lower Earley Way and the M4. An Air Quality Assessment (Air Quality Consultants Ltd, December 2021) was submitted with the application. It concluded that development-generated traffic flows on the local road network will be below relevant published screening criteria and that the overall operational air quality effects of the proposed development are judged to be 'not significant'. No specific mitigation is necessary.

Contamination

76. Paragraph 183 of the NPPF requires that a site is suitable for its proposed use taking account of ground conditions and any risks arising from land instability and contamination.
77. The last planning permission for 76 units included a condition for a scheme of remediation for the residential use. this condition was not discharged but a desktop Phase 1 Environmental Assessment (Idom, 1 December 2021) was submitted with the application. It concluded a low to moderate risk of significant contamination arising from potential asbestos in building materials, demolition rubble and piles of debris and fly tipping. A recommendation for a Phase II Intrusive Investigation to determine the appropriate foundations for future development. This forms Condition 9.

CONCLUSION

78. The site is well suited for its proposed logistics use and the derelict nature of the site is in need of redevelopment. The proposal represents a functional and well-planned outcome for the site with excellent sustainability credentials and which brings job creation. A lack of onsite car parking has been justified, the loss of residential dwellings as part of the extant planning permission is unfortunate but accepted. 24-hour use is appropriate on neighbour amenity grounds. Conditional approval is recommended.

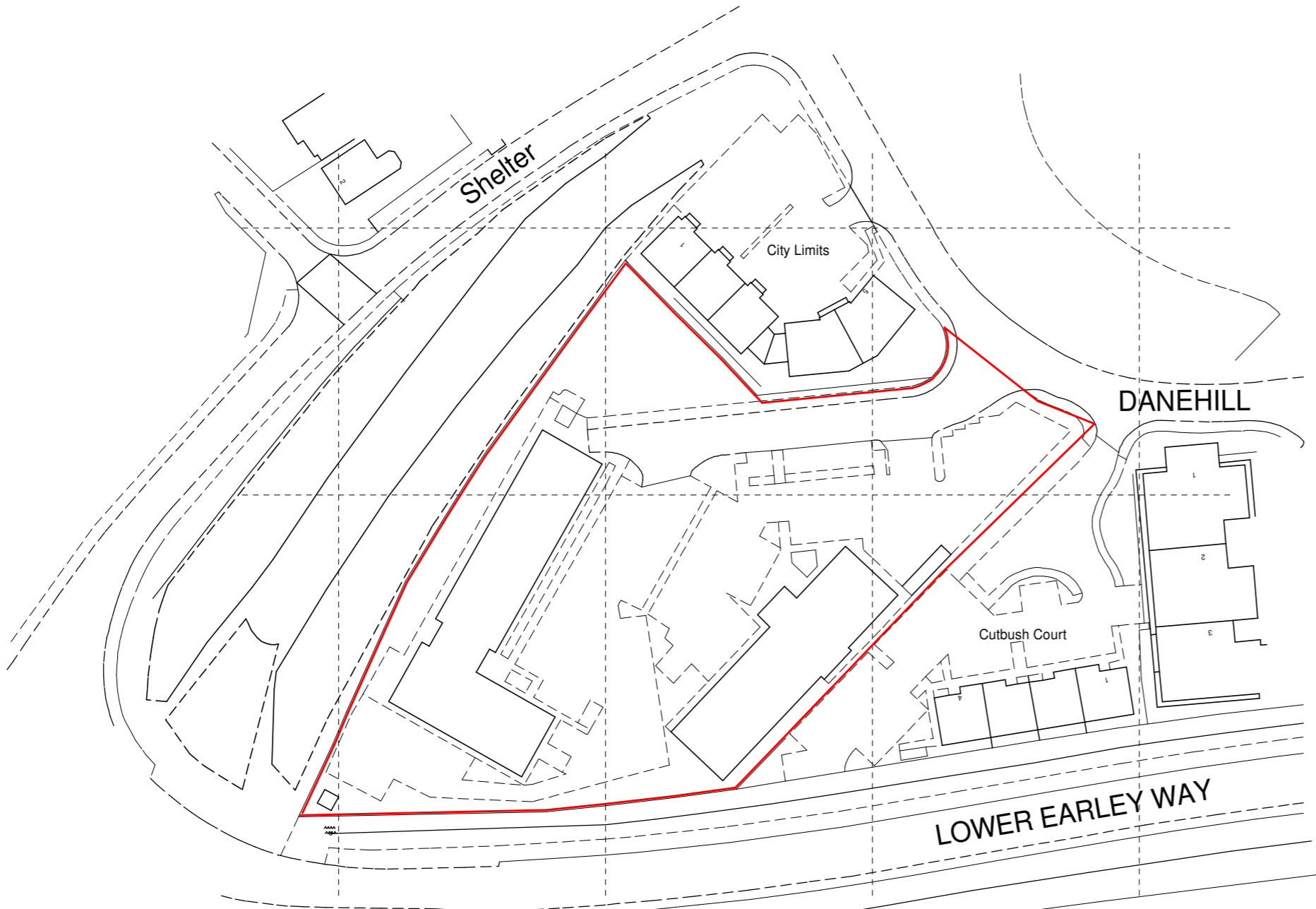
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PLANNING REF : 214046
PROPERTY ADDRESS : Radstock House
: Radstock Lane, Earley, Wokingham
: RG6 5UL
SUBMITTED BY : Earley Town Council
DATE SUBMITTED : 12/01/2022

COMMENTS:

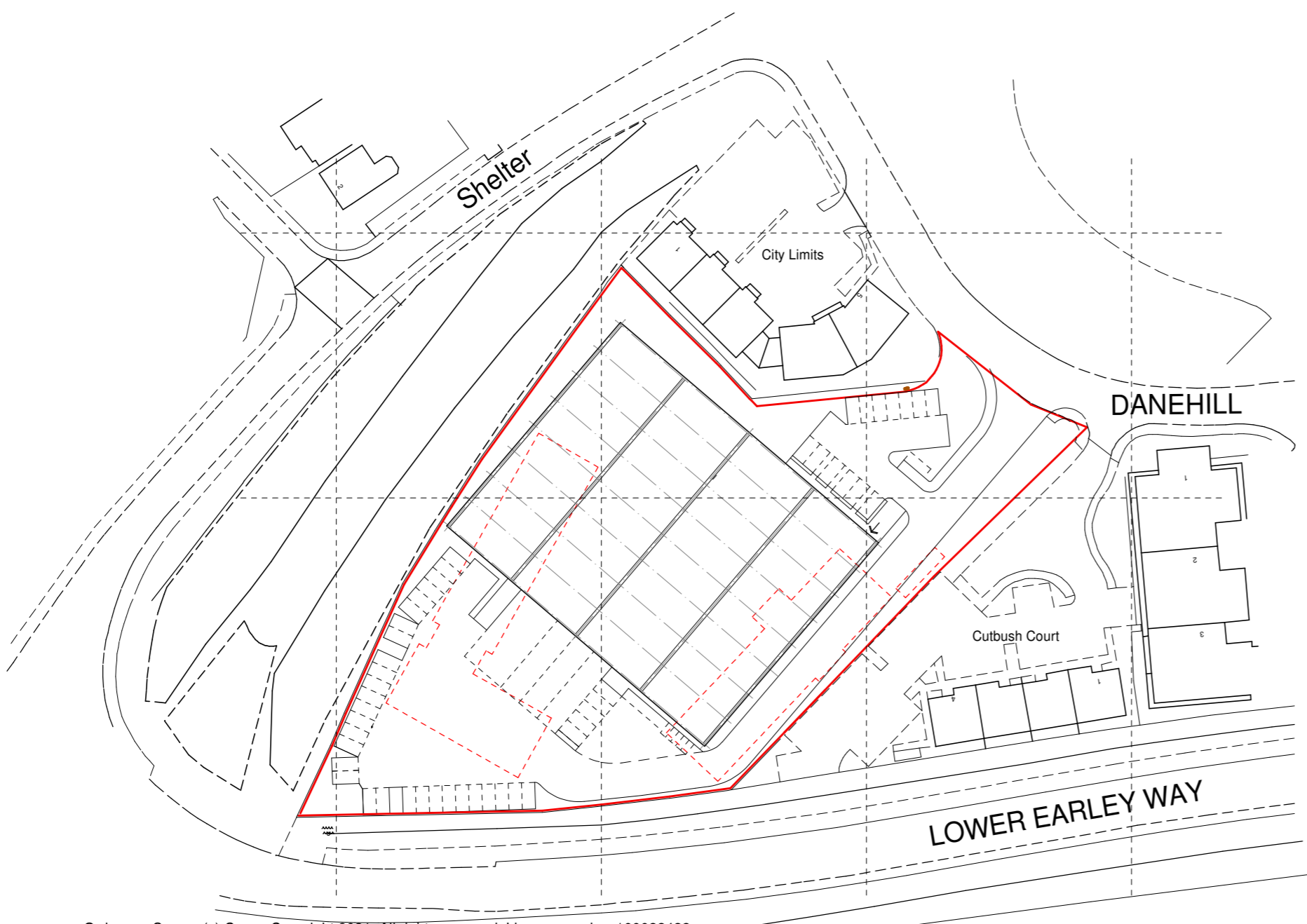
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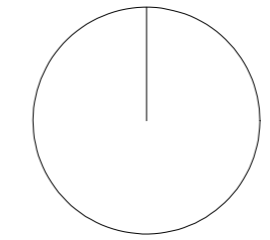
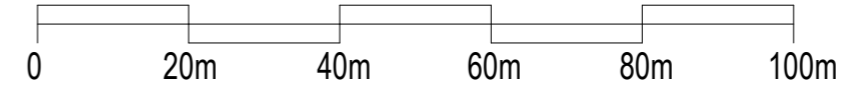
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EXISTING BLOCK PLAN



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PROPOSED BLOCK PLAN



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271

C1	08.12.2021	Issued for Planning	AM - KMD
P1	30.11.2021	First Issue	AM - KMD
REV	DATE	COMMENT	CHK - APD

Key - CHK = Checked by APD = Approved by



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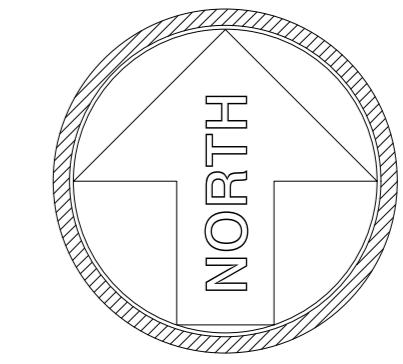
PROJECT
Hartman & Autotrader House
Land at Lower Earley Way
Danehill, Reading

TITLE
Existing and Proposed Block Plan

PURPOSE OF ISSUE	DRAWN BY	CHECKED BY
Planning	AM	KMD

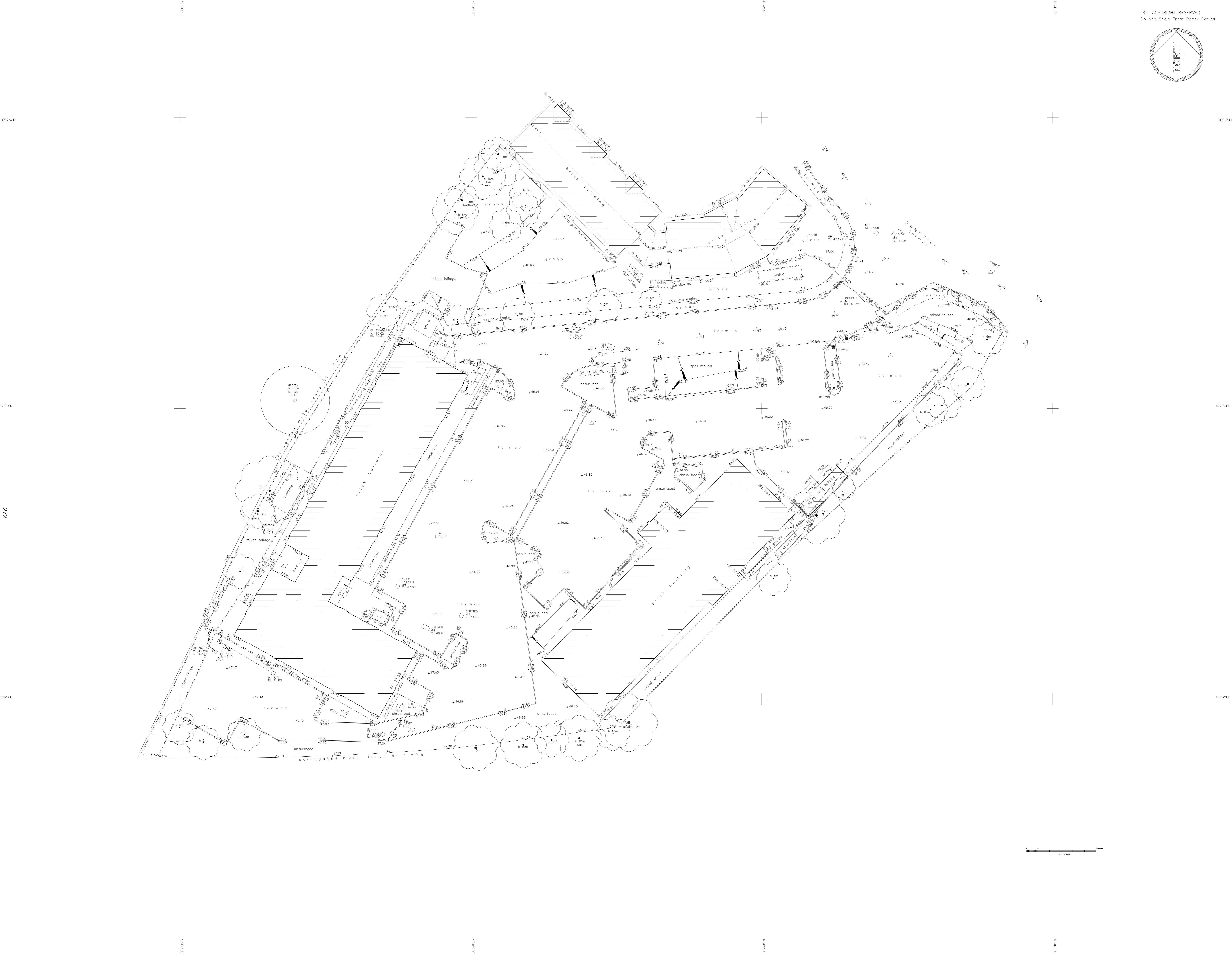
PROJECT No	STATUS	REVISION	SCALE @ A2
L2732	A3	C1	1:1000

DRAWING NUMBER
2732-RDJWL-XX-XX-DR-A-0016



Co-ordinate Table			
Station	Easting	Northing	Level
1	474589.306	169723.410	46.462
2	474571.105	169725.605	46.851
3	474572.129	169709.069	46.458
4	474520.799	169697.457	46.951
5	474489.393	169715.075	47.267
6	474483.271	169676.648	47.207
7	474467.788	169672.842	47.498
8	474456.677	169656.794	47.185
9	474489.651	169644.555	46.900

All levels related to Ordnance Survey active GPS network, of survey station 4.



DISCLAIMERS

This survey is related to OSGB36(15) coordinate system by GPS rapid static methods. No scale factor has been applied to the survey information. All horizontal distances taken from this drawing are ground distances.

Every effort has been made to confirm tree species on site, yet it is advised to confirm these details with an arborist before proceeding with any design.

Every effort has been made to confirm drainage run, type and size on site, yet it is advised to check these details against statutory authority records before proceeding with any design.

KEY

Survey control station	△ 5	Banking	
Gate (single)		Foliage Line	
Gate (subtle)		Drainage run, direction of flow and pipe size	
Tree Spread and Grit stream to scale		Approximate Position	

ABBREVIATIONS

Air Handling Unit	AHU	Water Meter	WM
Bellia Beacon	BB	Level Level	LL
Bellard	BD	Ridge Level	RL
Borehole	BH	Roof Level	RFL
BT Inspection Cover	BT	Staff Level	SFL
Cable Television Cover	CTV	Threshold Level	TL
Drainage Channel	DC	Parapet Wall Level	PWL
Electricity Cover	EC	Finished Floor Level	FFL
Electricity Fuse	EP	Head Level	HL
Earth Rod	ER	Silt Level	SL
Fire Hydrant	FH	Cover Level	CL
Gas Valve	GV	Invert Level	IL
Gate Post	GP	No Visible Pipes	NVP
Gully	GV	Unstable to Lift	UTL
Inspection Cover	IC	Foul Water	FW
Junction Box	JB	Sump Level	SLL
Key Outlet	KO	Surface Water	SW
Lamp Post	LP	Brick Pavings	BP
Manhole	MH	Concrete	CON
Marker Post	MP	Concrete Paving Slabs	CPS
Post	P	Flower Bed	FB
Pipe	PE	Shrub Bed	SB
Road Sign	RS	Tactile Paving	TAC
Roading Eye	RE	Unsurfaced	US
Marker Post	MP	Brick Wall	BW
Slip Post	SP	Retaining Wall	RW
Stop Valve	SV	Chainlink Fence	CLF
Stop Tap	ST	Chestnut Paving Fence	CPF
Telegraph Pole	TP	Iron Rolling Fence	IRF
Traffic Light	TL	Metal Security Fence	MSF
Vent Pipe	VP	Post and Chain Fence	PCF
Post and Rail Fence	PRF	Post and Wire Fence	PWF
Wooden Panel Fence	WPF		

Client
ROBERT DAVIES JOHN WEST LTD

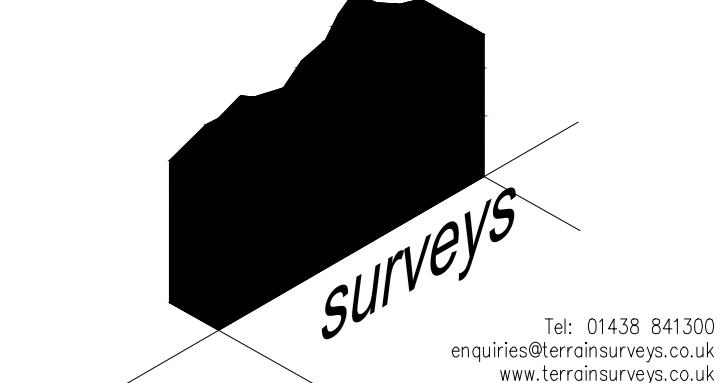
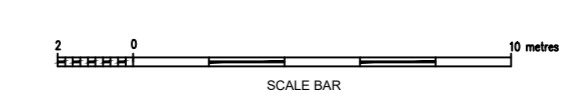
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AUTOTRADER HOUSE, HARTMAN HOUSE, EARLY, RG6 4UT

Title
TOPOGRAPHICAL SURVEY

Drawing Number
TB21-300-1

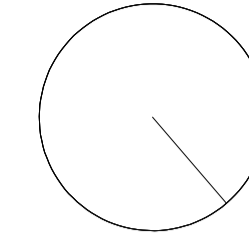
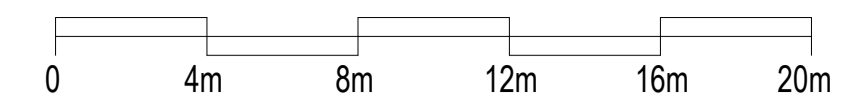
Revision	Description	Date

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Drawn by	RB	Checked by	PG
Date of Survey	JULY 2021		



Accreditations

Constructionline
Accredited Contractor
CHAS.co.uk

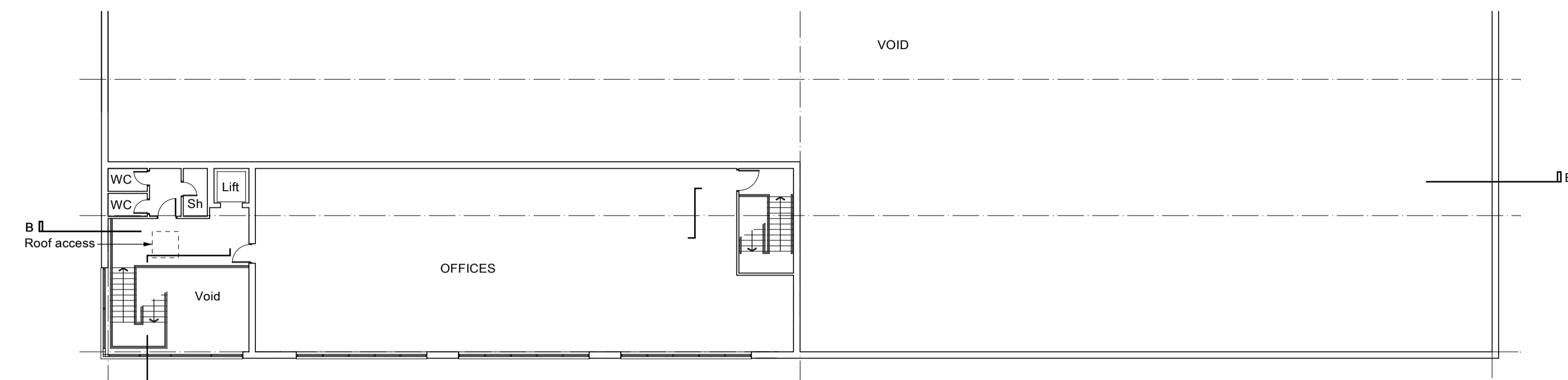


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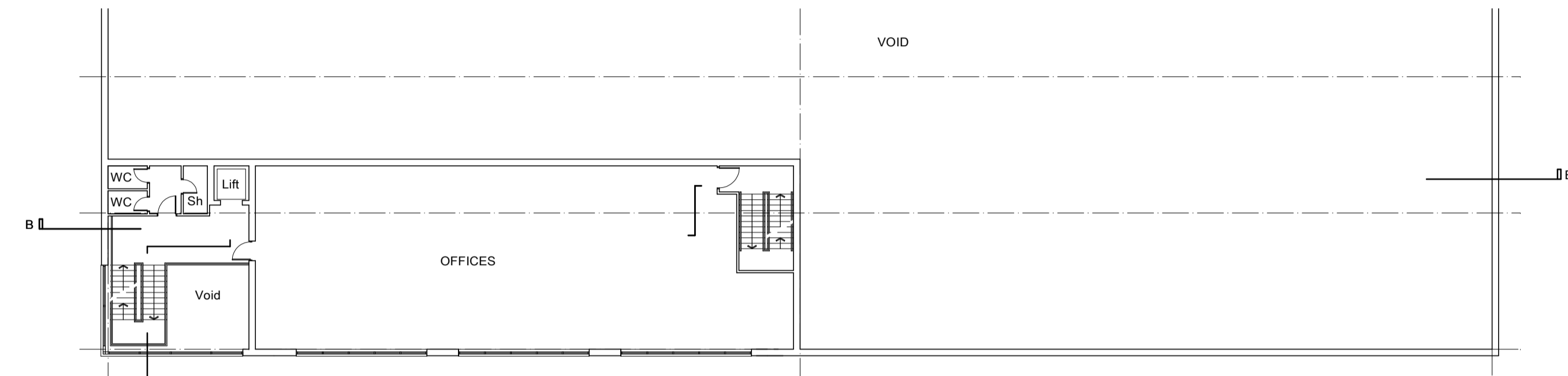
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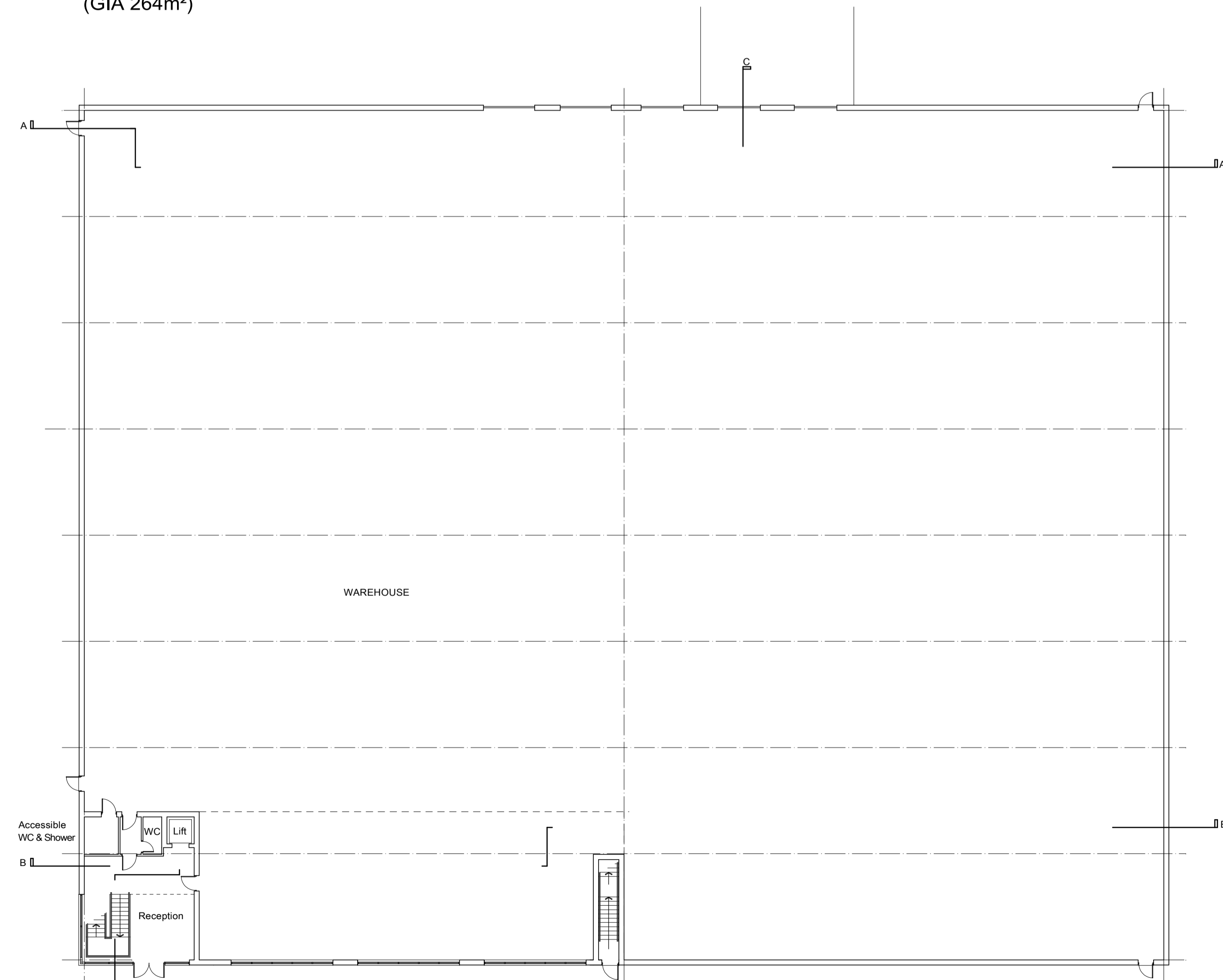
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SECOND FLOOR PLAN
(GIA 264m²)

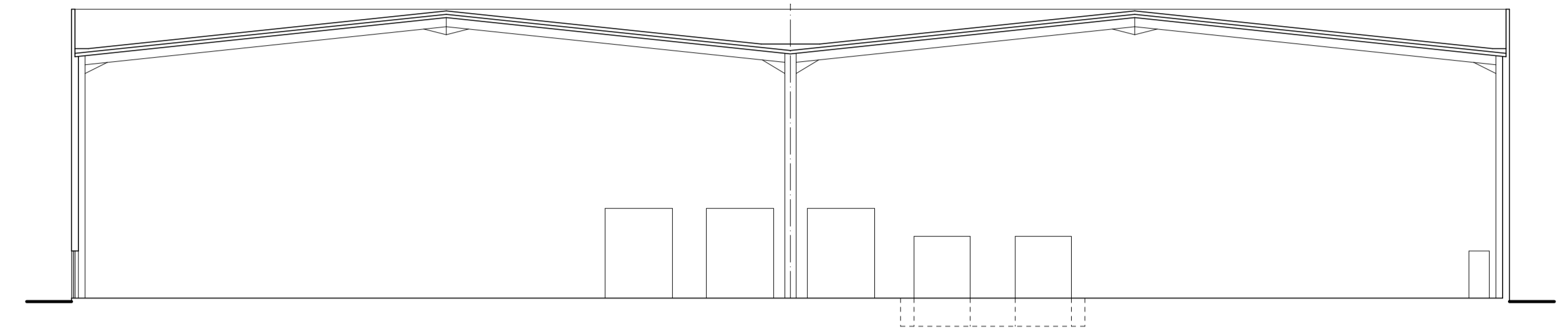


FIRST FLOOR PLAN
(GIA 264m²)

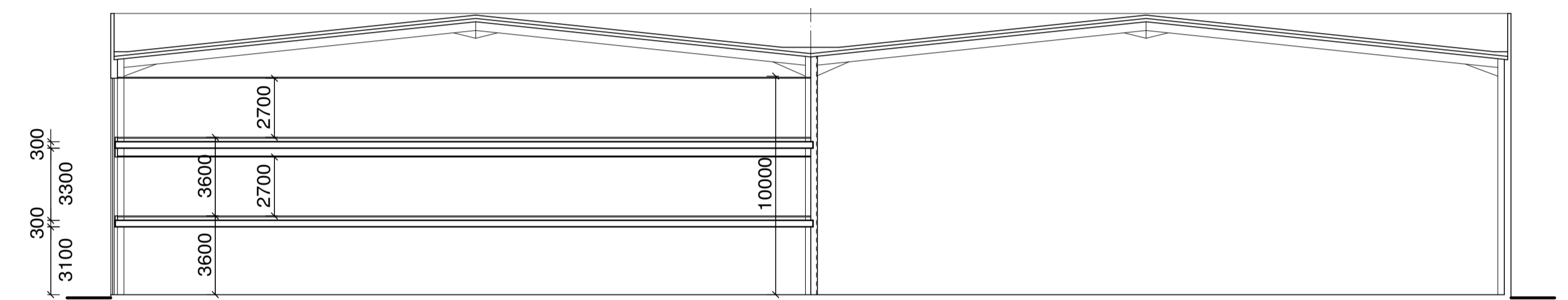


GROUND FLOOR PLAN
(GIA 3,164m²)

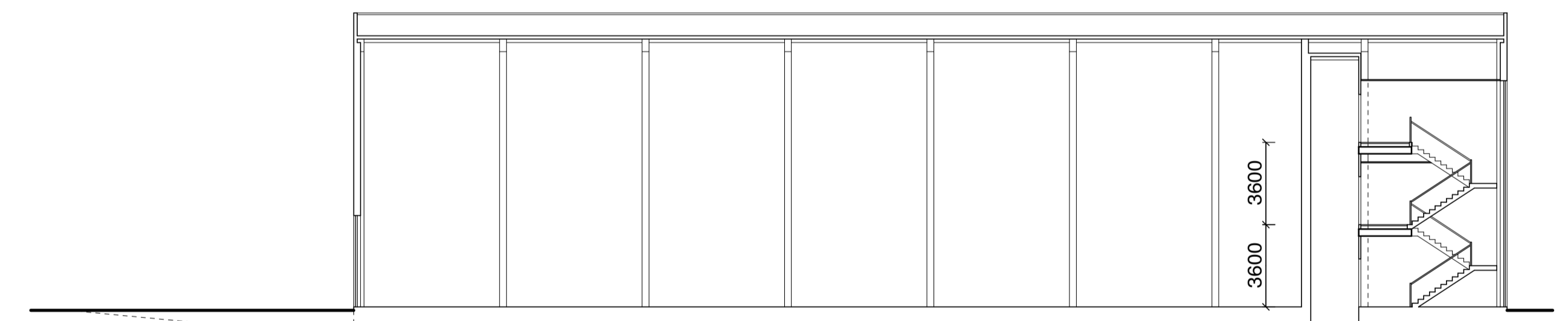
TOTAL GIA 3,692m²



SECTION A-A



SECTION B-B



SECTION C-C

C1	08/12/21	Issued for Planning	KMD - AM
P3	11/11/21	GIAs added/ section lines corrected	KMD - GS
P2	05/11/21	Amended post pre-app	KMD - GS
P1	09/07/21	For Comment	AOB - KMD

REV	DATE	COMMENT	CHK
			CHK - APD

Key - CHK = Checked by APD = Approved by

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PROJECT
Hartman & Autotrader House
Land at Lower Earley Way, Danehill, Reading
RG6 4UT

TITLE
Proposed Floor Plans & Sections

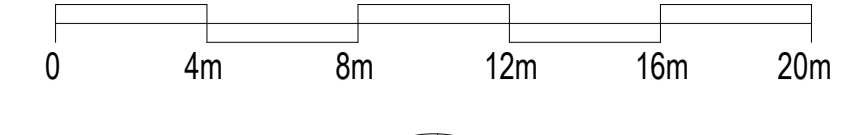
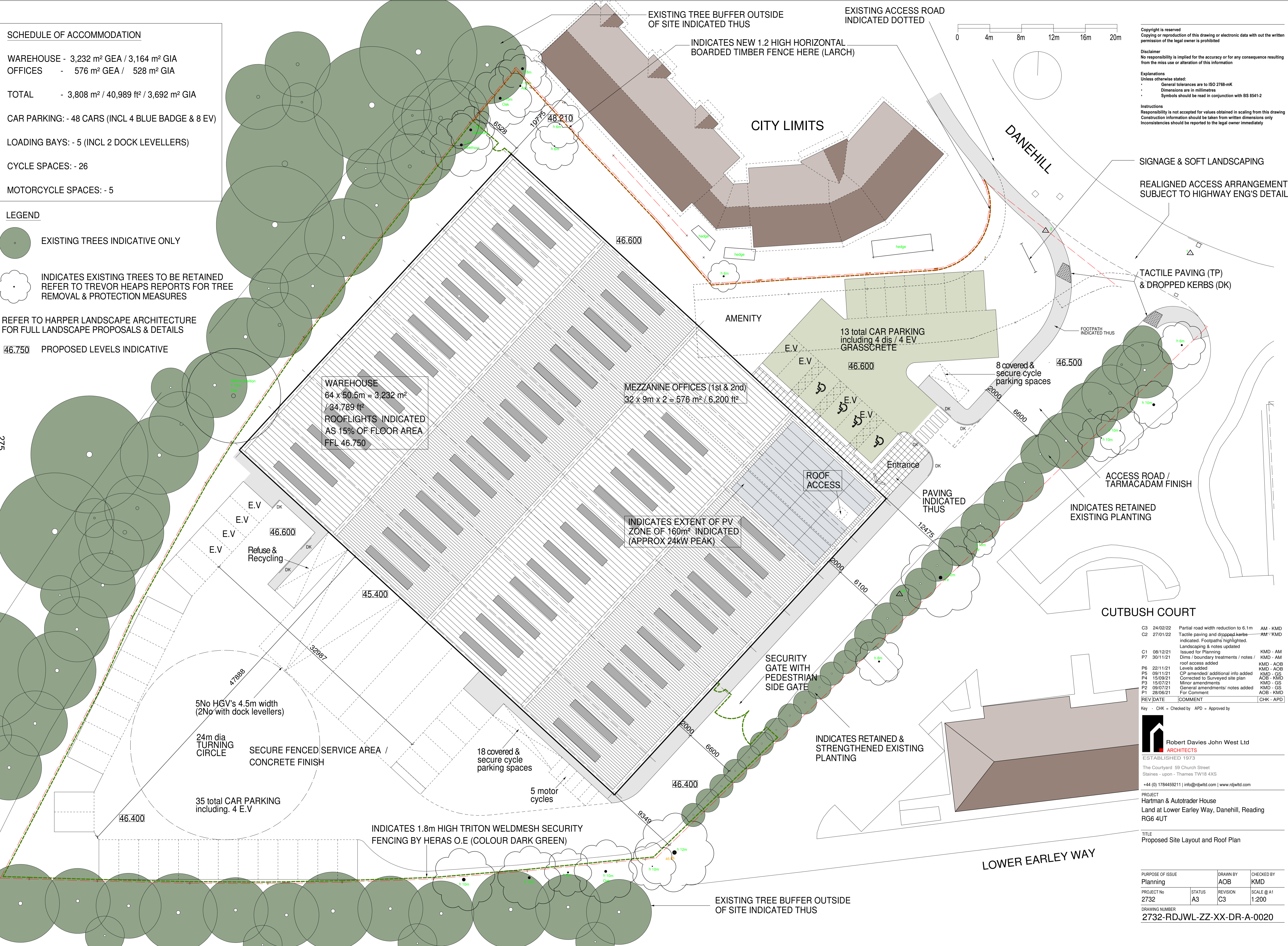
PURPOSE OF ISSUE Planning		DRAWN BY AOB	CHECKED BY KMD
PROJECT No 2732	STATUS A3	REVISION C1	SCALE @ A1 1:200

DRAWING NUMBER
2732-RDJWL-01-ZZ-DR-A-0025

SCHEDULE OF ACCOMMODATION

WAREHOUSE	- 3,232 m ² GEA / 3,164 m ² GIA
OFFICES	- 576 m ² GEA / 528 m ² GIA
TOTAL	- 3,808 m² / 40,989 ft² / 3,692 m² GIA
CAR PARKING:	- 48 CARS (INCL 4 BLUE BADGE & 8 EV)
LOADING BAYS:	- 5 (INCL 2 DOCK LEVELLERS)
CYCLE SPACES:	- 26
MOTORCYCLE SPACES:	- 5

- LEGEND**
- EXISTING TREES INDICATIVE ONLY
 - INDICATES EXISTING TREES TO BE RETAINED REFER TO TREVOR HEAPS REPORTS FOR TREE REMOVAL & PROTECTION MEASURES
 - REFER TO HARPER LANDSCAPE ARCHITECTURE FOR FULL LANDSCAPE PROPOSALS & DETAILS
 - 46.750 PROPOSED LEVELS INDICATIVE



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Explanations
Unless otherwise stated:
• General tolerances are to ISO 2768-mK
• Dimensions are in millimetres
• Symbols should be read in conjunction with BS 8541-2

Instructions
Responsibility is not accepted for values obtained in scaling from this drawing
Construction information should be taken from written dimensions only
Inconsistencies should be reported to the legal owner immediately

WAREHOUSE
64 x 50.5m = 3,232 m²
/ 34,789 ft²
ROOFLIGHTS INDICATED
AS 15% OF FLOOR AREA
FFL 46.750

MEZZANINE OFFICES (1st & 2nd)
32 x 9m x 2 = 576 m² / 6,200 ft²

INDICATES EXTENT OF PV
ZONE OF 160m² INDICATED
(APPROX 24kW PEAK)

5No HGV's 4.5m width
(2No with dock levellers)

24m dia
TURNING
CIRCLE

35 total CAR PARKING
including 4 E.V

SECURE FENCED SERVICE AREA /
CONCRETE FINISH

18 covered &
secure cycle
parking spaces

5 motor
cycles

INDICATES 1.8m HIGH TRITON WELDMESH SECURITY
FENCING BY HERAS O.E (COLOUR DARK GREEN)

EXISTING TREE BUFFER OUTSIDE
OF SITE INDICATED THUS

INDICATES RETAINED &
STRENGTHENED EXISTING
PLANTING

SIGNAGE & SOFT LANDSCAPING

REALIGNED ACCESS ARRANGEMENT
SUBJECT TO HIGHWAY ENG'S DETAIL

TACTILE PAVING (TP)
& DROPPED KERBS (DK)

ACCESS ROAD /
TARMACADAM FINISH

INDICATES RETAINED
EXISTING PLANTING

CUTBUSH COURT

C3	24/02/22	Partial road width reduction to 6.1m	AM - KMD
C2	27/01/22	Tactile paving and dropped kerbs indicated. Footpaths highlighted.	AM - KMD
P7	30/11/21	Landscaping & notes updated	KMD - AM
C1	08/12/21	Issued for Planning	KMD - AM
P7	30/11/21	Dims / boundary treatments / notes / roof access added	KMD - AM
P6	22/11/21	Levels added	KMD - AOB
P5	09/11/21	CP amended/ additional info added	KMD - GS
P4	15/09/21	Corrected to Surveyed site plan	AOB - KMD
P3	15/07/21	Minor amendments	KMD - GS
P2	09/07/21	General amendments/ notes added	KMD - GS
P1	28/06/21	For Comment	AOB - KMD

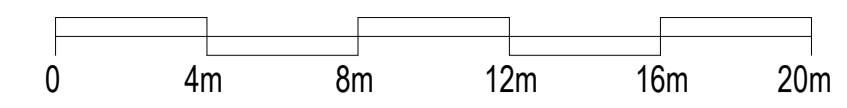
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PROJECT
Hartman & Autotrader House
Land at Lower Earley Way, Danehill, Reading
RG6 4UT

TITLE
Proposed Site Layout and Roof Plan

PURPOSE OF ISSUE Planning	STATUS A3	DRAWN BY AOB	CHECKED BY KMD
PROJECT No 2732	REVISION C3	SCALE @ A1 1:200	
DRAWING NUMBER 2732-RDJWL-ZZ-XX-DR-A-0020			



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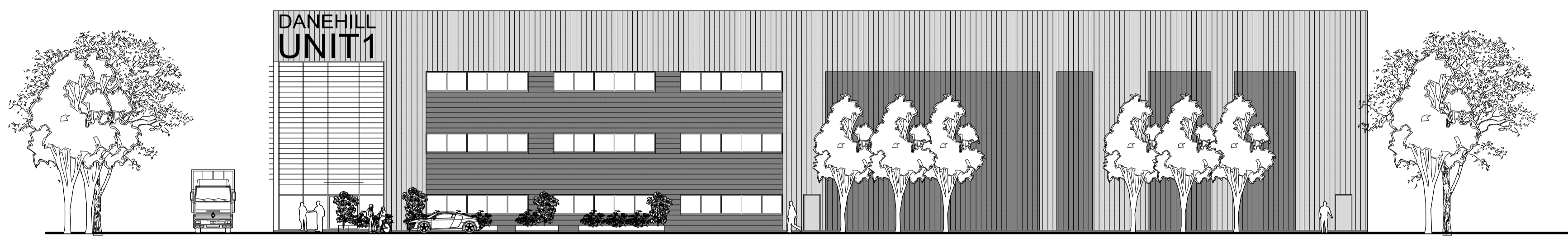
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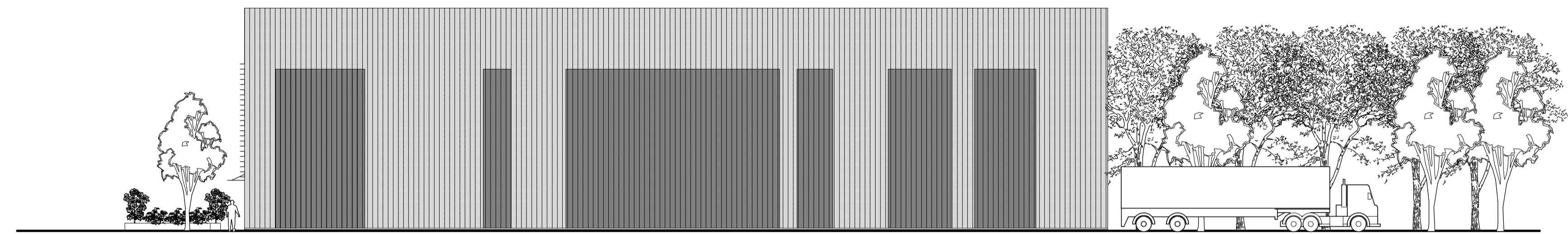


STREET ELEVATION (DANEHILL)

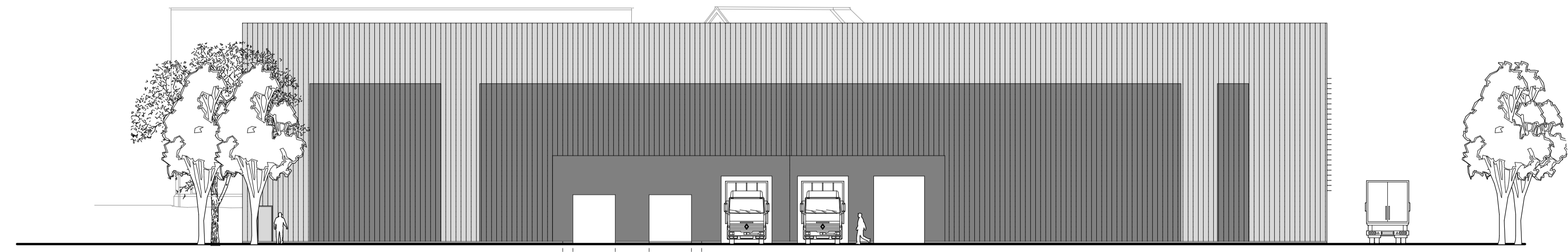


FRONT ELEVATION (EAST)

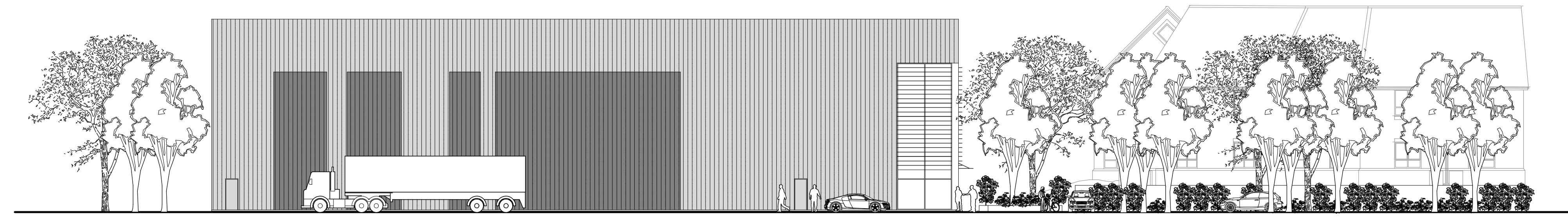
27/6



SIDE ELEVATION (NORTH)



REAR ELEVATION (WEST)



SIDE ELEVATION (SOUTH)



ILLUSTRATIVE 3D VIEW (ON APPROACH FROM DANEHILL)

SCHEDULE OF MATERIALS:

ROOF - 6° PITCHED ROOF BEHIND PARAPET WALLS - VERTICAL & HORIZONTAL MICRORIB CLADDING SILVER GREY & DARK GREY AS INDICATED
 WINDOWS/ DOORS - ALUMINIUM FRAMED WITH FULL HEIGHT TO ENTRANCE, DARK GREY.

ENTRANCE - SUSPENDED GLASS ENTRANCE CANOPY & BRISE SOLEIL TO FULL HEIGHT GLAZING PERSONEL DOORS - STEEL SILVER GREY

REV	DATE	COMMENT	CHK
C1	08/12/21	Issued for Planning	KMD - AM
P7	30/11/21	Context added	KMD - AM
P6	22/11/21	Image updated & planters added	KMD - AOB
P5	05/11/21	Amended post pre-app	KMD - GS
P4	11/08/21	Landscaping indicated	KMD - GS
P3	11/08/21	Street elevation added	KMD - GS
P2	15/07/21	Minor amendments	KMD - GS
P1	09/07/21	For Comment	KMD - GS

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PROJECT
 Hartman & Autotrader House
 Land at Lower Earley Way, Danehill, Reading
 RG6 4UT

TITLE
 Proposed Elevations & 3D View

PURPOSE OF ISSUE		DRAWN BY	CHECKED BY
Planning		KMD	GS
PROJECT No	STATUS	REVISION	SCALE @ A1
2732	A3	C1	1:200
DRAWING NUMBER			
2732-RDJWL-01-ZZ-DR-A-0030			

CYCLE PARKING SPACES

CAMBRIDGE BIKE SHELTER BY BIKEDOCKSOLUTIONS O.E.

MATERIALS:
FRAME OF ROBUST, GALVANISED STEEL TUBE AND SECTION.
ROOF MADE OF CLEAR SOLID POLYCARBONATE FINISH.

SHELTER FOR 8 BICYCLES PROVIDED TO THE FRONT OF THE SITE
AND FOR 18 BICYCLES AT THE REAR. (REFER TO PROPOSED SITE LAYOUT FOR LOCATION)

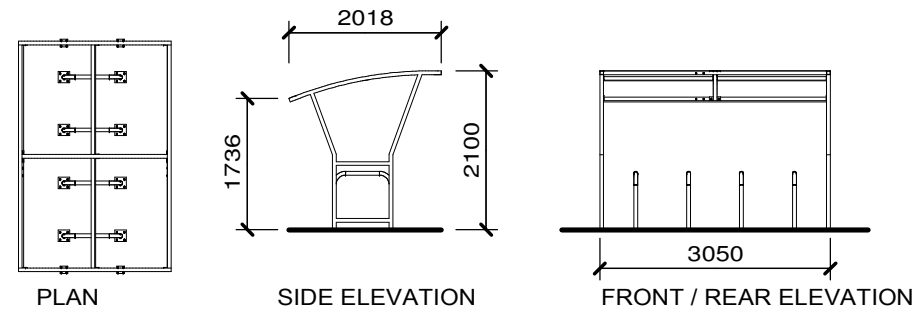


IMAGE SOURCE: <https://www.bikedocksolutions.com/cambridge-bike-shelter>

SERVICE AREA SECURITY FENCING

TRITON FENCING SYSTEM BY HERAS O.E.

MATERIALS:
MADE OF FULLY WELDED STEEL, COLOUR MOSS GREEN (RAL 6005).
FENCE MANUFACTURED FROM NOMINAL 5.0MM DIA. THICK STEEL WIRE
PROFILED HORIZONTAL SECTIONS. THE MESH ITSELF IS 55MM X 200MM.

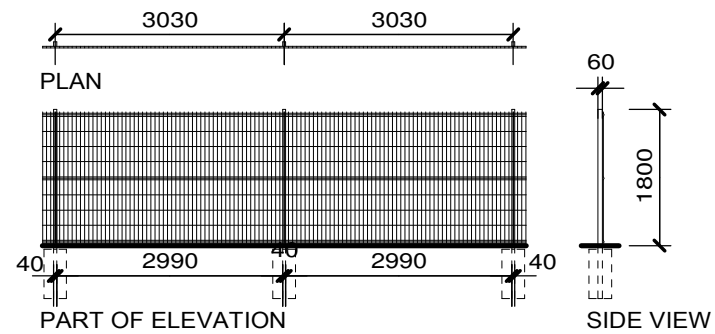


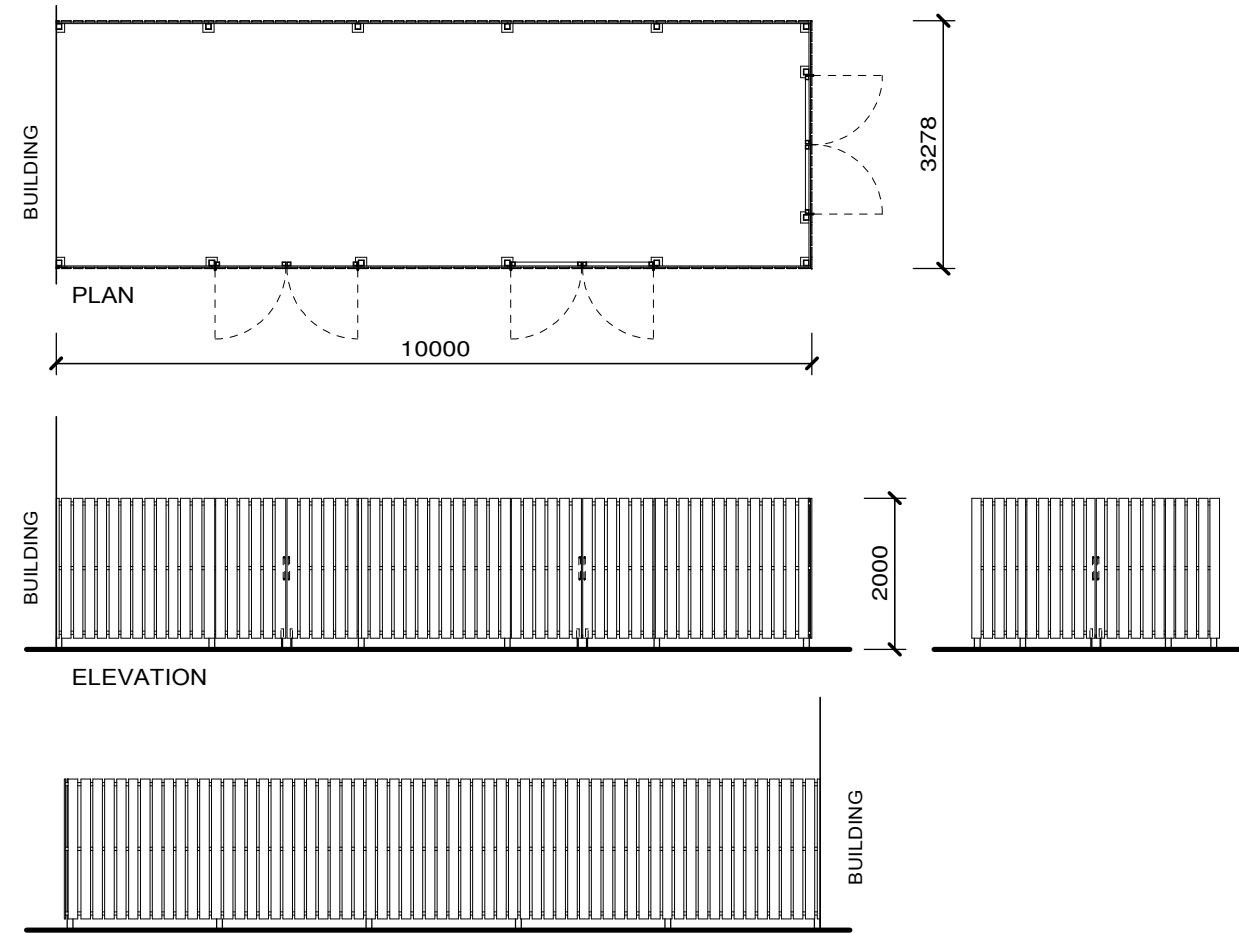
IMAGE SOURCE: <https://www.heras.co.uk/products/demarcation/welded-mesh-fence/mesh-fencing-triton/>

REFUSE & RECYCLING STORE

MATERIALS:

VERTICALLY TIMBER BOARDED (LARCH) OVER GALVANISED METAL FRAME
WITH ACCESS DOORS AS INDICATED.
(REFER TO PROPOSED SITE LAYOUT FOR LOCATION)

NOTE: NO ROOF. OPEN ENCLOSURE.



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C1	08.12.2021	Issued for Planning	AM - KMD
P1	01.12.2021	First Issue	AM - KMD
REV	DATE	COMMENT	CHK - APD

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PROJECT
Hartman & Autotrader House
Land at Lower Earley Way
Danehill, Reading

TITLE
Fence, Gate, Cycle Parking and Bin Store Details

PURPOSE OF ISSUE Planning	DRAWN BY AM	CHECKED BY KMD
PROJECT No L2732	STATUS A3	REVISION C1
SCALE @ A3 1:100		

DRAWING NUMBER
2732-RDJWL-ZZ-ZZ-DR-A-0035

SERVICE AREA SECURITY GATE

EURO 2 SWING GATE BY HERAS O.E.

1.8M HIGH DOUBLE LEAF METAL GATE (3030) WITH TAMPER-RESISTANT
LOCKING SYSTEM OFFERING FULL SECURITY.
PEDESTRIAN SINGLE LEAF METAL GATE (10).

COLOUR MOSS GREEN (RAL 6005) TO MATCH FENCING.

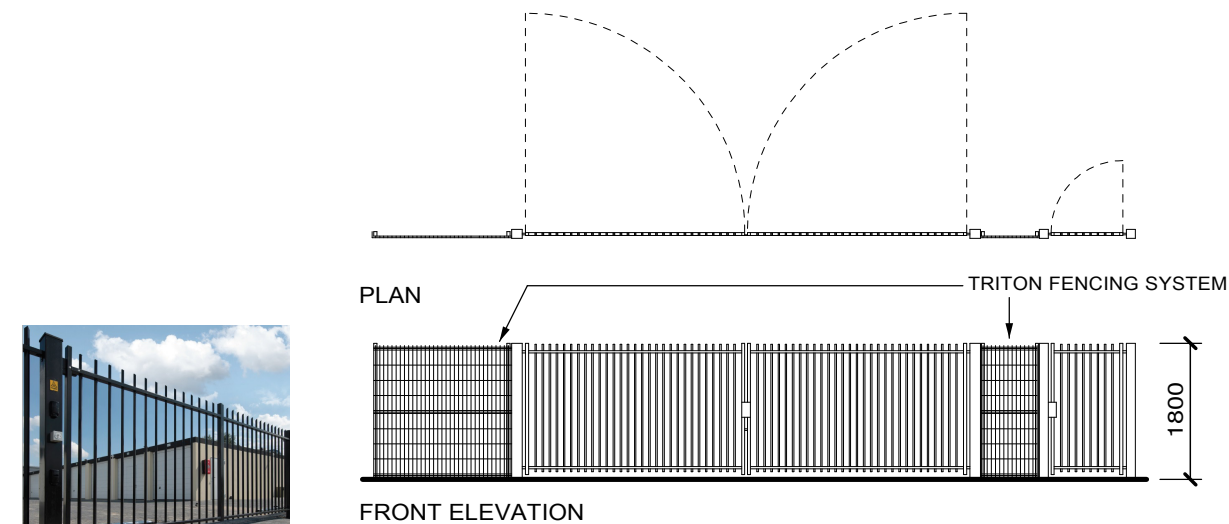


IMAGE & DIAGRAM SOURCE: BROCHURE AVAILABLE AT <https://www.heras.co.uk/products/entrance-control/swing-gates/swing-gate-euro-2/>

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Agenda Item 87.

Application Number	Expiry Date	Parish	Ward
213975	8 April 2022	Wokingham	Evendons

Applicant	Mr. D. Bolt
Site Address	Indigo House, Mulberry Business Park, Wokingham, RG41 2GY
Proposal	Full application for the proposed removal of the existing roof structure and the erection of a new second floor providing 11 No. 1 & 2 bedroom apartments, together with a cycle & refuse store.
Type	Full
Officer	Andrew Chugg
Reason for determination by committee	Major application

FOR CONSIDERATION BY	Planning Committee on Wednesday, 9 March 2022
REPORT PREPARED BY	Assistant Director – Place

SUMMARY
<p>The principle of converting this office building to residential flats has already been established by the extant prior approval decision 210166. This current application seeks approval to remove the building's roof and create a second floor to accommodate an additional 11 residential units over and above those granted under 182428. If approved, the applicant has advised that both schemes would be implemented simultaneously to deliver a total of 31 flats. The proposal would provide 4 affordable housing units (36%) that would be secured via a legal agreement under s106 and is therefore policy compliant in terms of delivering suitable housing for the borough.</p> <p>In design terms, the proposal would remove the existing shallow pitched roof and replace it with an additional floor that would be set back from the elevations and have a light-weight appearance being surrounded by glazed roof terraces. It would also be lower in height than that of roof ridge of the existing building. The additional floor proposed would not be out of character with the surrounding buildings in the area and would be reflective of other permissions in the area to add additional residential floors as part of office to residential conversions. There would be no impact on existing trees and a condition is recommended to secure a detail soft and hard landscaping scheme. Hence, overall, the proposal would enhance the character of the original building and the surrounding area.</p> <p>Adequate car and cycle parking facilities would be accommodated on site and the proposal would protect the amenities of future occupants within this and other neighbouring buildings. The proposed flats would comply with Nationally Described Space Standards (NDSS) and the proposal would not have an adverse impact on protected species.</p>

PLANNING STATUS
<ul style="list-style-type: none"> • Major Development Location • Core Employment Areas – Molly Millars Industrial Area • Wind turbine safeguarding zone • Farnborough Aerodrome consultation zone

- Sand and gravel extraction
- Thames Basin Heaths SPA Mitigation Zones – 7km zone
- Groundwater protection zone
- Landfill consultation zone
- Minerals consultation zone
- Nuclear Consultation Zone – AWE 12km zone
- Contaminated land consultation zone
- Bat Roost Habitat Suitability Zone
- Water Utility Consultation Zones

RECOMMENDATION

That the committee authorise the GRANT OF PLANNING PERMISSION subject to the following:

A. Completion of a legal agreement to secure four onsite affordable homes consisting of one First Home and three units for social rent

B. Conditions and informatives

Conditions:

1. Full Permission – 3yrs
The development hereby permitted shall be begun before the expiration of three years from the date of this permission.
Reason: In pursuance of s.91 of the Town and Country Planning Act 1990 (as amended by s.51 of the Planning and Compulsory Purchase Act 2004).
2. Approved details
This permission is in respect of the submitted application plans and drawings numbered: 18-085-20A – Location & site plan, 18-085-21 – Elevations – sheet 1, 18-085-22A – Elevations – sheet 2, 18-085-23 – Second floor plan and 18-085-24 – Cycle & refuse storage as received by the local planning authority on 03 December 2021. The development shall be carried out in accordance with the approved details unless other minor variations are agreed in writing after the date of this permission and before implementation with the Local Planning Authority.
Reason: For the avoidance of doubt and to ensure that the development is carried out in accordance with the application form and associated details hereby approved.
3. External materials
Before the development hereby permitted is commenced, samples and details of the materials to be used in the construction of the external surfaces of the building/s shall have first been submitted to and approved in writing by the local planning authority. Development shall not be carried out other than in accordance with the so-approved details.
Reason: To ensure that the external appearance of the building is satisfactory.
Relevant policy: Core Strategy policies CP1 and CP3
4. Noise attenuation measures
No development shall take place until a scheme for protecting the proposed dwellings of the approved development from external noise and internal noise

shall be submitted, for written approval, to the Local Planning Authority. The development shall not be occupied until the noise mitigation measure identified in the approved scheme, have been fully implemented. The noise mitigation measures shall be retained and maintained thereafter.

Reason: To protect future residents from noise from noise from industrial/commercial external sources and internally from the floor below.

Parking to be provided

5. No part of any building(s) hereby permitted shall be occupied or used until the vehicle parking space has been provided in accordance with the approved plans. The vehicle parking space shall be permanently maintained and remain available for the parking of vehicles at all times.

Reason: To ensure adequate on-site parking provision in the interests of highway safety, convenience and amenity. Relevant policy: Core Strategy policies CP3 & CP6 and Managing Development Delivery Local Plan policy CC07.

Cycle parking to be provided

6. No building shall be occupied until secure and covered parking for cycles has been provided in accordance with the approved drawing(s)/details. The cycle parking/ storage shall be permanently so-retained for the parking of bicycles and used for no other purpose.

Reason: In order to ensure that secure weather-proof bicycle parking facilities are provided so as to encourage the use of sustainable modes of travel. Relevant policy: NPPF Section 9 (Sustainable Transport) and Core Strategy policies CP1, CP3 & CP6 and Managing Development Delivery Local Plan policy CC07.

Surface Water Drainage

7. Before the development hereby permitted is commenced details of the proposed drainage and existing drainage for the site have been submitted to and approved in writing by the LPA. The details shall include how the site currently drains and will be drained after conversion with consideration to SuDS. Development should not increase flood risk within site or elsewhere and also employ SuDS Hierarchy.

Reason: To prevent increased flood risk from surface water run-off. Relevant policy: NPPF Section 10 (Meeting the Challenge of Climate Change, Flooding and Coastal Change), Core Strategy policy CP1 and Managing Development Delivery Local Plan policies CC09 and CC10.

Landscaping details

8. Prior to the commencement of the development, a Tree Condition Survey and full details of both hard and soft landscape proposals shall be submitted to and approved in writing by the local planning authority. These details shall include, as appropriate, proposed finished floor levels or contours, means of enclosure, car parking layouts, other vehicle and pedestrian access and circulation areas, hard surfacing materials and minor artefacts and structure (e.g. furniture/outdoor seating, planters, balcony screens, refuse or other storage units, signs, external lighting, external services, planting design etc). Soft landscaping details shall include planting plan, specification (including cultivation and other operations associated with plant and grass establishment), schedules of plants, noting species, planting sizes and proposed numbers/densities where appropriate, and implementation timetable. All hard and soft landscape works shall be carried out in accordance with the approved details prior to the occupation of any part of the development or in accordance with a timetable approved in writing by the local

planning authority. Any trees or plants which, within a period of five years after planting, are removed, die or become seriously damaged or defective, shall be replaced in the next planting season with others of species, size and number as originally approved and permanently retained.

Reason: In the interests of visual amenity. Relevant policy: Core Strategy policy CP3 and Managing Development Delivery Local Plan policies CC03 and TB21.

Decentralised energy

9. Prior to the commencement of development a scheme for generating 10% of the predicted energy requirement of the development from decentralised renewable and/or low carbon sources (as defined in the glossary of Planning Policy Statement: Planning and Climate Change (December 2007) or any subsequent version) shall be submitted to and approved in writing by the local planning authority. The approved scheme shall be implemented before the development is first occupied and shall remain operational for the lifetime of the development.

Reason: To ensure developments contribute to sustainable development.

Relevant policy: NPPF Section 14 (Meeting the Challenge of Climate Change, Flooding and Coastal Change), Core Strategy policy CP1, Managing Development Delivery Local Plan policy CC05 & the Sustainable Design and Construction Supplementary Planning Document.

10. Prior to occupation of the development hereby approved, detail for the biodiversity enhancements measures outlined in the Potential Roost Assessment report (Greenlink Ecology, ref: 21-1621-Report_MF-KV, September 2021) shall be provided to the local authority for its approval. Once approved the strategy shall be implemented in full unless otherwise agreed by the local authority in writing.

Reason: to ensure that the proposal is in accordance with Section 41 NERC Act re. UK Biodiversity Action Plan Priority Species (Species of Principal Importance) and complies with Planning Policies for Wildlife including CP7 of the Wokingham Borough Core Strategy (2010), and the National Planning Policy Framework which requires consideration of the potential biodiversity gains that can be secured within developments.

Informatives:

1. The development accords with the policies contained within the adopted / development plan and there are no material considerations that warrant a different decision being taken.
2. The Local Planning Authority has acted positively and proactively in determining this application by assessing the proposal against all material considerations, including planning policies and any representations that may have been received and subsequently determining to grant planning permission in accordance with the presumption in favour of sustainable development as set out in the NPPF.
3. Records indicate a public sewer beneath or in close proximity to the proposed development. In order to protect public sewers and to ensure that Thames Water can gain access to those sewers for future repair and maintenance, approval must be sought from Thames Water where the erection of a building or an extension to a building or underpinning work would be over the line of, or

would come within 3 metres of, a public sewer. Thames Water will usually refuse such approval in respect of the construction of new buildings, but approval may be granted in some cases for extensions to existing buildings. The applicant is advised to contact Thames Water Developer Services on 0845 850 2777 to discuss the options available at this site.

4. The development hereby permitted is liable to pay the Community Infrastructure Levy. As an affordable housing development, a claim for relief can be made. This is a matter for the developer. The Liability Notice issued by Wokingham Borough Council will state the current chargeable amount. Anyone can formally assume liability to pay, but if no one does so then liability will rest with the landowner. There are certain legal requirements that must be complied with. For instance, whoever will pay the levy must submit an Assumption of Liability form and a Commencement Notice to Wokingham Borough Council prior to commencement of development. For more information see - <http://www.wokingham.gov.uk/planning/developers/cil/cil-processes/>

PLANNING HISTORY		
Indigo House, Mulberry Business Park, Fishponds Road:		
Application Number	Proposal	Decision
210166	Prior approval submission for the proposed change of use from office (Class E(i)) to 30 no. residential dwellings (Class C3)	Approved 12/04/2021
200540	Prior Approval submission for the proposed change of use from office (Class B1) to 54no. residential dwellings (Class C3)	Refused 08.04.2020
193259	Prior Approval submission for a proposed change of use from Office Use (Class B1(a)) to 40no. residential dwellings (Class C3).	Refused 04.02.2020
193281	Prior Approval submission for a proposed change of use from Office Use (Class B1(a)) to 14no. residential dwellings (Class C3)	Refused 04.02.2020
192602	Prior Approval submission for the proposed change of use of existing office building (Use Class B1(a)) to 40no. residential flats (Use Class C3)	Withdrawn 26.11.2019
192688	Prior Approval submission for the proposed change of use of existing office building roof space (use class B1(a)) to 14no. residential flats (Use Class C3)	Withdrawn 26.11.2019
182428	Application for Prior Approval for the proposed change of use from office to 30no. residential dwellings	Approved 30.10.2018

Nigra Building, Mulberry Business Park:		
183174	Full planning application for the changes to fenestration, replacement of existing roof to flat roof together with provision of cycle, refuse stores and car parking.	Approved 18.01.2019
183084	Prior approval submission for the conversion of existing office to 43no residential flats.	Approved 11.01.2019
Rosa Building, Mulberry Business Park, Fishponds Road		
211508	Full planning application for the proposed raising of existing roof of Rosa Building to create 11no. apartments to the second floor.	Presently undetermined
210769	Prior approval submission for proposed change of use of the office building (Use Class E(i)) to 34 no. residential units (Use Class C3).	Approved 29.04.2021
Ilex House, Mulberry Business Park, Fishponds Road		
191907	Prior approval submission for the change of use of the Ground & First Floors from office use (Class B1a) to form 46 Apartments (Class C3).	Approved 03.09.2019
172091	Full application for proposed raising of roof to create a second floor extension for 14 residential flats and the provision of cycling and refuse facilities.	Refused – 30/04/2018 Appeal allowed – 06/03/2019

SUMMARY INFORMATION
<p>Site Area – 0.41 ha Existing units – Nil Proposed units – 11 Existing density – Nil Proposed density – 75.6 DPH (<i>inc. 20 unit conversion scheme 210166</i>). Number of affordable units proposed - 4 Previous land use – Offices Proposed Public Open Space - Nil Existing parking spaces – 86 Proposed parking spaces – 86</p>

CONSULTATION RESPONSES

Internal:

WBC Biodiversity	No objection subject to condition
WBC Drainage	No objection subject to condition
WBC Environmental Health	No objection subject to condition
WBC Highways	No objection subject to condition
WBC Tree & Landscape	No objection subject to condition

External:

Royal Berkshire Fire and Rescue No comments to make regarding fire service access or building separation.

Thames Water Utilities Ltd No objection with regard to waste water network and sewage treatment works. Measures for surface water drainage should follow NPPF advice (details forwarded to the applicant).

Southern Gas Networks No objection – Details of gas infrastructure network provided (details forwarded to the applicant).

SEE Power Distribution No objection - Details of high and low voltage mains provided (details forwarded to the applicant).

Cadent Gas No objection – Details of gas infrastructure network provided (details forwarded to the applicant).

REPRESENTATIONS

Wokingham Town Council:

Would like there to be access to green space for the residents. There is concern, regarding fire regulations, that there is only one central staircase for the whole building.

Local Members: No comments received

Neighbours: One objection received on the following grounds:

- Alternative premises will be needed for existing business occupants.
- The proposal will exacerbate existing traffic problems on Fishponds Road.
- Other office conversions with additional floors at Mulberry Business Park are derelict. These are prime office premises. A better option would be to and build housing.
- How many more flats and apartments are really needed in Wokingham? Neighbouring building already had extra floor added but seem to be slow in residential take up.

APPLICANT'S POINTS
<ul style="list-style-type: none"> • The development will make effective use of a previously developed site, in accordance with the NPPF. • The new second floor will be visually attractive, contemporary addition to the existing building. • The proposal will provide much needed new housing, including affordable housing. • The CIL contributions will generate additional investment in the local area. • The new dwellings are located in a sustainable location reducing on the dependency on cars. • The development provides more than adequate car parking & incorporates a number of vehicle charging points to support the transition to electric cars. • The new dwellings will comply with the Technical Space Standards & provide usable private roof terraces. • The development will be energy efficient, with high levels of insulation, PV panels & electric heating.

PLANNING POLICY		
National Policy	NPPF	National Planning Policy Framework
Adopted Core Strategy DPD 2010	CP1	Sustainable Development
	CP3	General Principles for Development
	CP4	Infrastructure Requirements
	CP5	Housing mix, density and affordability
	CP6	Managing Travel Demand
	CP7	Biodiversity
	CP8	Thames Basin Heaths Special Protection Area
	CP9	Scale and Location of Development Proposals
	CP15	Employment Development
Adopted Managing Development Delivery Local Plan 2014	CC01	Presumption in Favour of Sustainable Development
	CC02	Development Limits
	CC03	Green Infrastructure, Trees and Landscaping
	CC04	Sustainable Design and Construction
	CC05	Renewable energy and decentralised energy networks
	CC06	Noise
	CC07	Parking
	CC10	Sustainable Drainage
	TB05	Housing Mix
	TB07	Internal Space standards

	TB11	Core Employment Areas
	TB12	Employment Skills Plan
	TB21	Landscape Character
	TB23	Biodiversity and Development
Supplementary Planning Documents (SPD)	BDG	Borough Design Guide – Section 4
		DCLG – National Internal Space Standards

PLANNING ISSUES

Description of Development:

1. The application site relates to a two-storey 1980's office building with ancillary car parking and landscaping and is accessed off Fishponds Road (refer to Location Plan at Appendix A and Pic. 1.0 below). The site sits within Mulberry Business Park which was originally developed as a business park with a mix of offices and industrial units. The surrounding buildings are predominantly commercial/office buildings of either two or three storeys in height. However, following the introduction of Class O under the General Permitted Development Order, which allows conversions of offices to residential without the need for full planning permission, several buildings in the immediate area have been the subject of such conversion proposals. Most notably, these include Nigra Building, Rosa House and Ilex House (as outlined in the relevant planning history section of this report above).

Pic. 1.0: Indigo House viewed from within Mulberry Business Park



2. The proposal intends the creation of a new second floor above the existing ground and first floors to provide 11 residential units comprising of 8 no. 1 bedroom flats and 3 no. 2 bedroom flats. Four of these units would be secured via legal agreement under s106 as affordable homes. All the units would be accessed via the existing internal stair core to the building which is located centrally within the building.
3. The additional floor would be achieved by removing the existing shallow pitched roof and replacing it with a recessed additional floor utilising brickwork to match, glazed sections, zinc cladding and a single ply membrane flat roof. The proposed replacement flat roof would have a 1.5m reduction in height compared that of the existing roof ridge (refer to proposed elevations provided at Appendices C and D).
4. The applicant has advised that, if approved, this proposal would be implemented in tandem with the change of use of the building from offices to 30 apartments on the ground and first floors as approved under the prior approval application (210166).

Principle of Development:

5. The National Planning Policy Framework has an underlying presumption in favour of sustainable development which is carried through to the local Development Plan. The Managing Development Delivery Local Plan (MDD) Policy CC01 states that planning applications that accord with the policies in the Development Plan for Wokingham Borough will be approved without delay unless material considerations indicate otherwise.
6. Policy CC02 of the MDD sets out the development limits for each settlement as defined on the policies map and therefore replaces the proposals map adopted through the Core Strategy, as per the requirement of policy CP9. Policy CP9 sets out that development proposals located within development limits will be acceptable in principle, having regard to the service provisions associated with the major, modest and limited categories.
7. The site lies within a Major Development Location and while it also sits within a Core Employment Area the principle of the use of this building for residential accommodation has already been established by the extant prior approval application 210966.

Character of the Area:

8. Core Strategy Policy CP3 states that planning permission will be granted for proposals that are of an appropriate scale of activity, mass, layout, built form, height, materials and character, together with a high quality of design without detriment to the amenities of adjoining land users, including open spaces or occupiers and their quality of life. Policy CP1 of the Core Strategy requires developments to maintain or enhance the high quality of the environment. R9 of the Council's Borough Design Guide states that the height of residential buildings should respond to the prevailing heights in the local context, the scale and importance of the space that the building will define and the position of the building line in relation to the street; i.e. how far back the building is from the street frontage.
9. The existing office building includes a shallow pitched roof (refer to Pic. 1.0 above) that is somewhat tired in its appearance. The proposed additional floor would incorporate a 1.0m set-back from the edge of the existing external building envelope thereby avoid appearing as an overly dominant or bulky building. This set-back also helps better articulate the elevations of the building in addition to the proposed use of

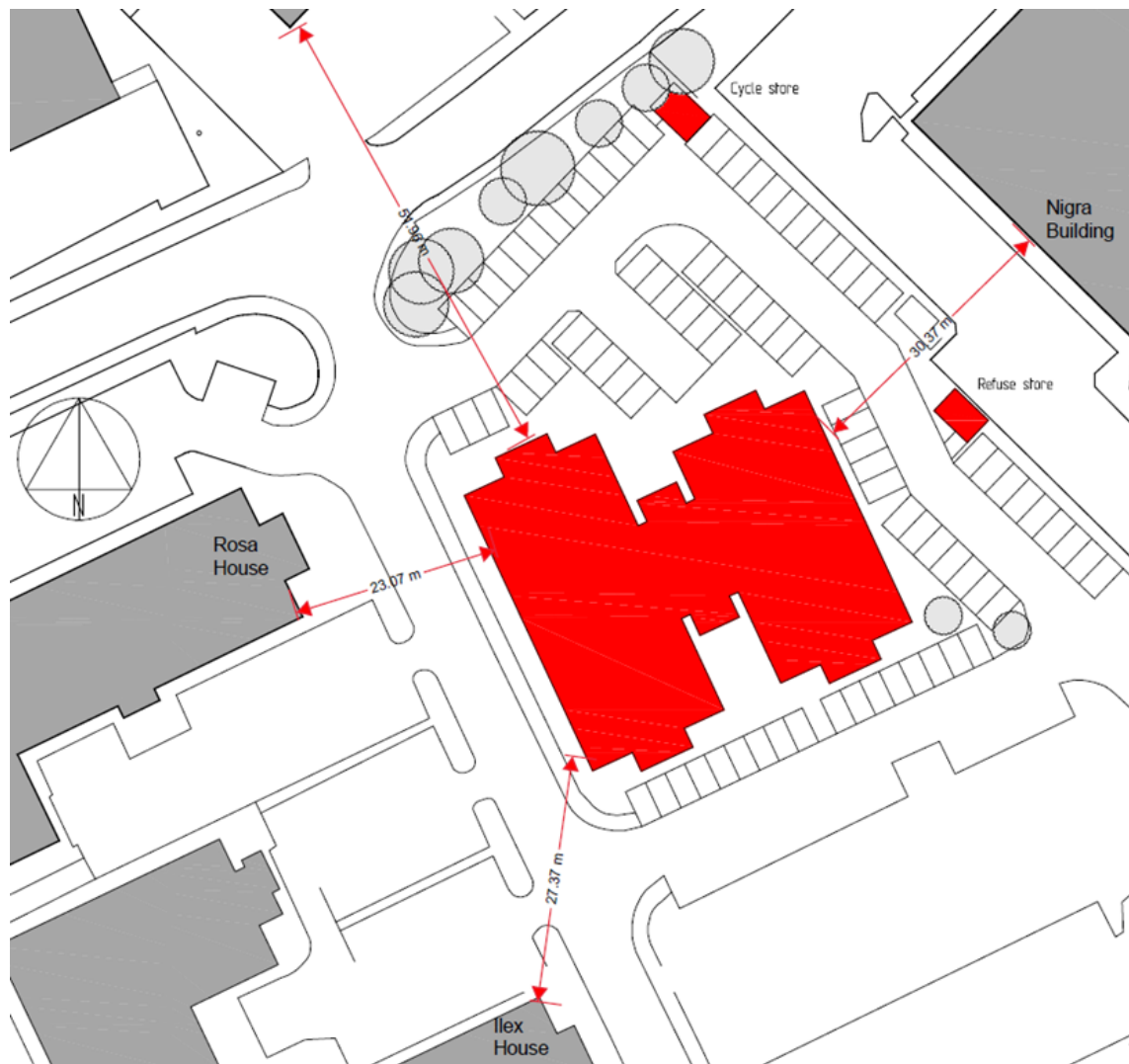
lightweight glazed panels and glazed roof terrace areas (refer to proposed second floor plan at Appendix B) provided around to the edge of the upper floor of the building.

10. Hence, the proposed second floor would provide a holistic replacement roof design for the existing building that would enhance its overall external appearance. The proposal would also be reflective of the mix of differing rooflines in the area which include both flat and shallow pitched versions. The additional resultant bulk at second floor would be tempered by that fact that the overall resultant roof height would be less than that of existing pitched roof and would therefore not appear out of keeping with the overall heights and massing of other buildings in the vicinity of the application site. Therefore, the proposal complies with the above design-based policies and guidance and is acceptable in this respect.

Residential Amenities:

11. Core Strategy Policy CP3 states that development proposals should not result in a detriment to the amenities of adjoining land users including open spaces or occupiers and their quality of life. The Council's Borough Design Guide (BDG) stipulates that a minimum back-to-back distance of 30m should be provided between new flatted developments and 15m back-to-flank for developments over 2 storeys to maintain privacy and limit the sense of enclosure.
12. The proposal would introduce second floor habitable room windows that would face towards the Nigra Building with a back-to-back distance of 30m; and towards Rosa House (currently the subject of planning application 211508 – refer to the Planning History section of this report above) with a back-to-flank distance of 23m. The proposal would retain a 27m distance between Ilex House with windows between the two buildings be very much at an oblique angle (refer to Fig. 1.0 below).
13. These distances would help avoid any significant overlooking between buildings and thereby protect the privacy and amenities of existing or future dwellings in accordance with Core Strategy Policy CP3 and guidance within the Council's Borough Design Guide.

14. Fig.1.0: Extract of proposed Site Plan (refer to Appendix A for full plan)



Access and Movement:

15. MDD Local Plan Policy CC07 and Appendix 2 of the MDD Local Plan stipulates minimum off street parking standards. Core Strategy Policy CP6 requires developments to provide appropriate vehicular parking, having regard to car ownership. P3 of the Borough Design Guide SPD notes that parking spaces should be safe and convenient, close to the dwelling and sited to minimise impact upon safety.

Highway Safety & Traffic Impact:

16. The Council's Highway's Officer raises no concerns with regards to highway safety or traffic impact issues and confirms that the proposal (even when combined with the extant prior approval 210166) would generate less vehicle trips than the use of the site as offices. The objection received regarding a perceived exacerbation of parking problems along Fishponds Road has been considered, however, given that overall vehicle trips would be less than that of the existing use there would be no adverse impact on highway safety or convenience. Therefore, the proposal is acceptable in this respect.

Parking:

17. The site has a total of 86 parking spaces, which would be shared between the 30 apartments on the ground and first floor (as approved under the extant prior approval 210166) and the 11 proposed apartments on the second floor as part of this application. Given the town & fringe location of the site, the proposed dwellings would require no more than 1 parking space per unit. The overall scheme, if approved, would have a total requirement of 41 spaces. The provision of 86 parking spaces would therefore be in excess of the maximum parking standards for studios and 1 and 2-bedroom apartments.
18. Hence, the Council's Highways Officer raises no objection in respect of proposed parking arrangements but has recommended that conditions requiring that car and cycle parking is provided in accordance with the approved plans.

Sustainability:

19. The site is located in a sustainable location within walking distance of the town centre of Wokingham. The site is located approximately 1 mile from Wokingham train station and close to a number of bus stops providing access to the train station and Crowthorne, Reading & Wokingham.

Flooding and Drainage:

20. MMD Local Plan policy CC10 states that all development proposals must reproduce greenfield runoff characteristics and return run-off rates and volumes back to the original greenfield levels, for greenfield sites and for brownfield sites both run-off rates and volumes be reduced to as near greenfield as practicably possible.
21. The site is located within Flood Zone 1 and no increase in the footprint of the building is proposed as part of this application. Hence, the Council's Drainage Officer raises no objection to this proposal but has requested details of the existing site drainage to the site and a drainage strategy drawing, which indicates the location and sizing of all drainage features with pipe connections from the building to the drainage features, is secured via condition if the application is approved.
22. In respect of foul water, this matter would be dealt with by Thames Water or by local sewerage authority and applicant has been advised to check whether there is enough capacity to accommodate 11 new apartments. An informative is also recommended in this respect.
23. Subject to compliance with the above surface water drainage condition, the proposal accords with MDD Local Plan Policy CC10 in respect of flooding and drainage issues.

Landscape and Trees:

24. MDD Local Plan Policy CC03 aims to protect and retain existing trees, hedges and other landscape features. Core Strategy Policy CP3 aims to create a sense of place in the way buildings integrate with their surroundings including the use of appropriate landscaping.
25. The site is located within the Wokingham Settlement Area and within the Mulberry Business Park, which has been a core employment area of the town. The Council's Tree & Landscape Officer is aware that in recent years, interest has been shown in reusing the commercial office spaces as residential apartments and has identified

that while the trees and shrubs around the site edges, to Fishponds Road frontage and around the building, have been poorly managed they still contribute to the character of the area.

26. No landscape information, in the form of a Tree Survey or Landscape Strategy, has been submitted to support this application. The submitted Design and Access Statement indicates that no trees would be removed as part of the proposal and that planting areas will be retained as existing.
27. While the Council's Tree & Landscape Officer raises no objection to the application in principle, she has suggested a review of the existing landscaping including the existing trees along Fishponds Road adjacent to the application site in addition to considering whether there is an opportunity to provide for improvements to the existing landscape and potential new amenity space (dealt with in the Amenity Space for Future Occupiers section of this report below) by replacing some of the existing parking spaces around the site with additional soft landscaping.
28. In line with the above comments, a condition is recommended to secure a Tree Condition Survey and full details of both hard and soft landscape proposals for the development. Subject to compliance with the above condition, the proposal would both mitigate the impact of the development and enhance the green infrastructure of site in accordance with Core Strategy Policies CP1 and CP3 and MDD Local Plan policy CC03.

Environmental Health:

29. Policy CC06 and Appendix 1 of the MDD Local Plan requires that development protect noise sensitive receptors from noise impact. Policy CP3 of the Core Strategy aims to protect neighbouring amenity. Core Strategy Policy CP3 states that development proposals should not result in a detriment to the amenities of adjoining land users including open spaces or occupiers and their quality of life.
30. WBC Environmental Health have assessed the application and raise no objection in principle. However, they have advised that the information submitted does not address noise from existing industrial / commercial external sources nor insulation protection from the first floor below for the future residents. As such, they have recommended that a condition is imposed to secure details of noise attenuation measures to protect future residents from excessive noise to comply with Policy CC06.

Amenity Space for Future Occupiers:

31. Core Strategy Policy CP3 states that planning permission will be granted for proposals that provide functional amenity space. The Borough Design Guide states that balconies in areas with an urban character may be suitable to compensate for the loss of rear garden space by providing roof terraces, balconies or wintergardens, so long as they do not overlook existing properties.
32. The Wokingham Town Council suggestion that the proposed development include additional green space is noted. However, the site is highly constrained in the respect that the existing building sits largely within an area of hardstanding for car parking within limited soft landscaping provided on the frontage of the site facing Fishponds Road.

33. While there is an overprovision of car parking for the proposal, it is dispersed around the application site meaning that converting some of these spaces to any meaningful outdoor amenity space would be difficult to achieve. Moreover, the extant prior approval (210166) for conversion of the existing building requires no provision of outdoor amenity space making it more difficult in planning terms to justify the requirement new outdoor amenity space for the 11 new units especially given that they would benefit from generous private roof terrace areas (refer to Appendix B) as outlined below:

Proposed roof terrace areas:

- Plot 1 - 6.5m²
 - Plot 2 - 42.9m²
 - Plot 3 - 10.0m²
 - Plot 4 - 25.8m²
 - Plot 5 - 22.3m²
 - Plot 6 - 6.5m²
 - Plot 7 - 42.9m²
 - Plot 8 - 9.9m²
 - Plot 9 - 9.9m²
 - Plot 10 - 75.6m²
 - Plot 11 - 6.5m²
34. In addition, Leslie Sears Playing Ground located to the south of the application site is a 15min walk from the application site which would provide future occupants with public outdoor open space.
35. Hence, in this instance, a combination of all the above factors means that an adequate arrangement of private and communal open space would be provided for the development in accordance with Policy CP3 and with guidance contained within the Council's Borough Design Guide.

Internal Space Standards:

36. Policy TB07 of the MDD and R17 of the Borough Design Guide SPD require adequate internal space to ensure the layout and size achieves good internal amenity. The Technical housing standards – Nationally Described Space Standards (NDSS) set out minimum internal floor areas.
37. Table 1.0 below demonstrates that the proposal meets all the NDSS requirements; the right-hand (green) column shows respectively by how much each flat exceeds the minimum standard. In summary, all the units proposed meet the following floorspace criteria and demonstrate:
- GIA above the NDSS standard
 - Single room width over and above NDSS standard
 - Single room floor space over and above NDSS standard
 - Double room width over and above NDSS standard
 - Double room floor space over and above NDSS standard

Table 1.0: Gross Internal Floorspace comparison (Proposed units and NDSS)

Plot no	House Type	House Details			Proposed	National Standards	Difference
Unit#		Floors	Beds	Person	GIA sqm	GIA sqm	GIA sqm
1	Flat	1	1	2	52.7	50	+2.7
2	Flat	1	2	4	76.7	70	+6.7
3	Flat	1	1	2	50.3	50	+0.3
4	Flat	1	1	2	58.9	50	+8.9
5	Flat	1	1	2	60.6	50	+0.6
6	Flat	1	1	2	50.6	50	+0.6
7	Flat	1	2	4	74.1	70	+4.1
8	Flat	1	1	2	50.0	50	+0.0
9	Flat	1	1	2	50.0	50	+0.0
10	Flat	1	2	4	75.2	70	+5.2
11	Flat	1	1	2	50.8	50	+0.8

n.b. the areas given above exclude the proposed roof terraces as referred to in the Amenity Space for Future Occupiers section of this report (see above).

Ecology:

38. Policy TB23 of the MDD Local Plan requires the incorporation of new biodiversity features, buffers between habitats and species of importance and integration with the wider green infrastructure network. Policy CP7 states that sites designated as of importance for nature conservation at an international or national level will be conserved and enhanced and inappropriate development will be resisted. The NPPF paragraph 174 is clear that the local planning authority should seek biodiversity net gain wherever possible through development.
39. The submitted bat survey found no evidence of use of the existing building and trees for roosting by bats and no potential access points for bats have been identified. The Council's Ecologist therefore agrees that the building can be classified as having negligible potential and there is no need for any follow-up surveys and that the local planning authority can be confident that the proposed development will not adversely affect the local bat population.
40. In terms of biodiversity net gain, the site predominately has a sealed surface with only a few shrubs and urban trees present and its baseline value would therefore be very low in ecological terms. Hence, the Council's Ecologist has advised that it would be reasonable and proportionate to not seek submission of a Defra metric calculator in this instance.
41. However, Section 6.2 of the submitted ecological report makes recommendations for ecological enhancements – 6 wall mounted bird boxes installed on a variety of aspects and the Council's Ecologist recommends that these species enhancements are secured via condition to enhance biodiversity in line with guidance contained within the NPPF.

Sustainable Design/Construction:

42. Policy CC05 states that on proposals of 10 dwellings or more, planning permission will only be granted for proposals that deliver a minimum 10% reduction in carbon emissions through renewable energy or low carbon technology.
43. The applicant has advised that all apartments would feature large areas of glazing to provide natural daylight & ventilation and in relation to energy efficiency measures and the conservation of fuel and power, the proposed apartments would be highly

insulated and would incorporate other measures, including smart heating controls, low energy light fittings and low water use WC's. The applicant has also advised that 4 unallocated parking spaces will be designated as electric vehicle charging points the ongoing maintenance of which would be the responsibility of the appointed management company.

44. While the application has not been supported by a detailed Sustainability Statement or Energy Statement, the applicant has agreed to a condition to deliver a minimum 10% reduction in carbon emissions through renewable energy or low carbon technology for this development in accordance with Policy CC05.

Community Infrastructure Levy (CIL), Special Protection Area (SPA) & Affordable Housing:

Community Infrastructure Levy:

45. The application is liable for CIL payments because it involves a net increase of 11 new dwellings, payable at a rate of £365/m², index linked.

Special Protection Area:

46. The application site falls outside of the Thames Basin Heaths Special Protection Area 5KM zone and hence does not trigger a requirement for an Appropriate Assessment for an avoidance strategy under the Conservation of Habitats and Species Regulations 2017.

Affordable Housing:

47. Policy CP5 of the Core Strategy, Policy TB05 of the MDD Local Plan and the Affordable Housing SPD specify an affordable housing rate of 20% for any development involving five dwellings or more on previously developed land with a total area of 0.16 hectares or more within development limits.
48. The threshold for requiring affordable housing relates to planning applications of 5 dwellings or more or residential sites of 0.16 hectares or larger. The site exceeds this threshold and therefore there is a requirement for the provision of affordable housing. The Council's Housing Officer has advised that to meet the requirements of Policy CP5 of the Core Strategy, a minimum of 30% of the total number of units (net) should be provided as affordable housing. This equates to 3.3 units in this instance.
49. Following negotiations with the applicant, they have agreed to provide four onsite affordable homes consisting of 1 First Home and 3 affordable units for social rent which would be secured via a legal agreement under s106.

Timing of development:

50. In addition to the above, a clause has been included in the draft legal agreement to ensure that the development would be implemented 'in tandem' with the office-to-residential conversion prior approval permission 210166. This would ensure a holistic development is achieved and protected the amenities of future residents within the flats.

Employment Skills:

51. Policy TB12 of the Wokingham Borough Council MDD, requires planning applications for all major development (both commercial and residential) in Wokingham Borough to submit an employment skills plan (ESP) with a supporting method statement.

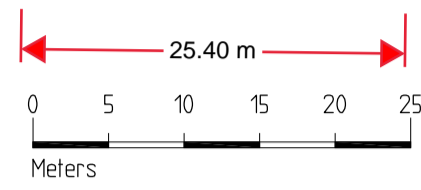
52. The Council's Employment Skills Officer has been consulted on this proposal and advises that it falls below the threshold of 1000 SQM. Hence, no ESP or contribution in lieu of an ESP is required in this instance.

CONCLUSION:

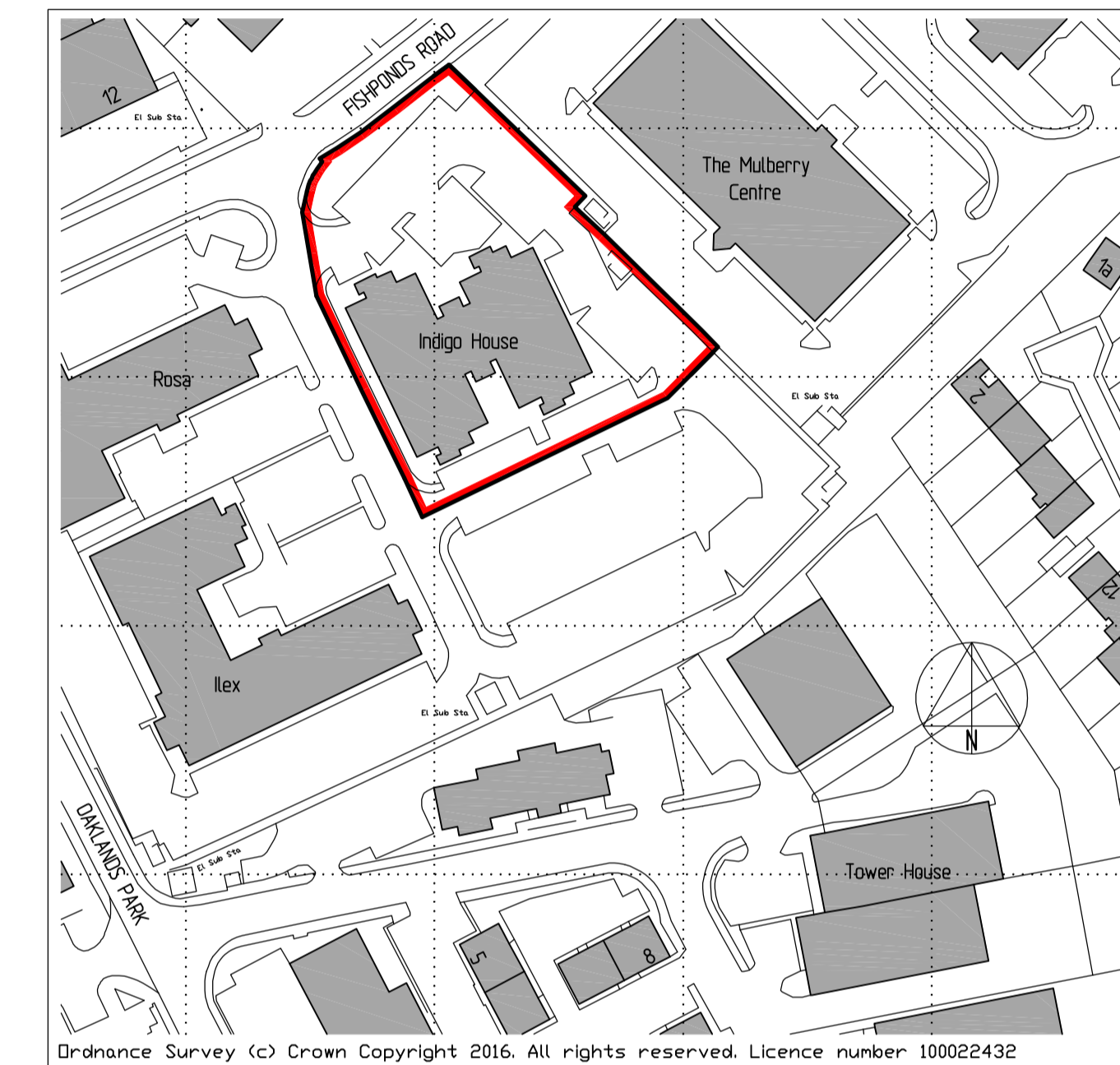
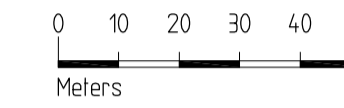
53. The principle of converting this office building to residential flats is already established and the application would provide 11 good quality flats with a policy compliant percentage of affordable housing on site. The proposal would enhance the appearance of the building and additional improvements to soft landscaping measures would be secured via condition. The residential amenity of future occupants in the host building and neighbouring buildings would be protected and adequate car and cycle parking facilities would be provided. As such, the proposal represents a suitable brownfield redevelopment proposal that would be policy compliant subject to adherence to the recommended conditions and obligations in the draft s106 legal agreement.

The Public Sector Equality Duty (Equality Act 2010)

In determining this application the Council is required to have due regard to its obligations under the Equality Act 2010. The key equalities protected characteristics include age, disability, gender, gender reassignment, marriage and civil partnership, pregnancy and maternity, race, religion or belief. There is no indication or evidence (including from consultation on the application) that the protected groups identified by the Act have or will have different needs, experiences, issues and priorities in relation to this particular planning application and there would be no significant adverse impacts upon protected groups as a result of the development.



Site Plan 1 : 500



Site Location Plan 1 : 1250

PROPOSED DEVELOPMENT
INDIGO HOUSE FISHPONDS ROAD
WOKINGHAM BERKS RG41 2GY

For :
Mr D BOLT

SCHEME PROPOSALS
Location & Site Plan

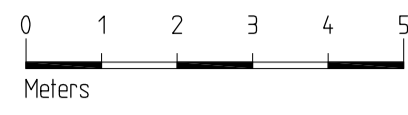
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Second Floor Plan

299

PROPOSED DEVELOPMENT
INDIGO HOUSE FISHPONDS ROAD
WOKINGHAM BERKS RG41 2GY

For :
Mr D BOLT

SCHEME PROPOSALS
Second Floor Plan

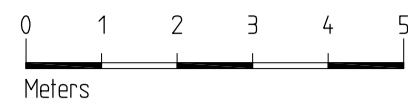
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Front (NW) Elevation



Side (NE) Elevation

PROPOSED DEVELOPMENT
INDIGO HOUSE FISHPONDS ROAD
WOKINGHAM BERKS RG41 2GY

For :
Mr D BOLT

SCHEME PROPOSALS
Elevations - Sheet 1

SCALE:	DATE:	REF:
1 : 100	Nov. 2021	18-085-21

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0 1 2 3 4 5
Meters



Rear (SE) Elevation



Side (SW) Elevation

303

PROPOSED DEVELOPMENT
INDIGO HOUSE FISHPONDS ROAD
WOKINGHAM BERKS RG41 2GY

For :
Mr D BOLT

SCHEME PROPOSALS
Elevations - Sheet 2

SCALE:	DATE:	REF:
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